

Council Assessment Panel Agenda & Reports

16 October 2023

Our Vision

*A City which values its heritage, cultural diversity,
sense of place and natural environment.*

*A progressive City which is prosperous, sustainable
and socially cohesive, with a strong community spirit.*

City of Norwood Payneham & St Peters
175 The Parade, Norwood SA 5067

Telephone 8366 4555
Email townhall@npsp.sa.gov.au
Website www.npsp.sa.gov.au
Socials  /cityofnpsp  @cityofnpsp



City of
Norwood
Payneham
& St Peters

11 October 2023

To all Members of the Council Assessment Panel:

- Mr Terry Mosel (Presiding Member)
- Ms Jenny Newman
- Mr Mark Adcock
- Mr Ross Bateup
- Cr Christel Mex

NOTICE OF MEETING

I wish to advise that pursuant to Clause 1.5 of the Meeting Procedures, the next Ordinary Meeting of the Norwood Payneham & St Peters Council Assessment Panel, will be held in the Council Chambers, Norwood Town Hall, 175 The Parade, Norwood, on:

Monday 16 October 2023, commencing at 7.00pm.

Please advise Kate Talbot on 8366 4562 or email ktalbot@npsp.sa.gov.au if you are unable to attend this meeting or will be late.

Yours faithfully



Geoff Parsons
ASSESSMENT MANAGER

City of Norwood Payneham & St Peters
175 The Parade, Norwood SA 5067

Telephone 8366 4555
Email townhall@npsp.sa.gov.au
Website www.npsp.sa.gov.au
Socials  /cityofnpsp  @cityofnpsp



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Norwood
Payneham
& St Peters

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VENUE Council Chambers, Norwood Town Hall

HOUR

PRESENT

Panel Members

Staff

APOLOGIES Mr Terry Mosel

ABSENT

1. **COMMENCEMENT AND WELCOME**
2. **APOLOGIES**
3. **CONFIRMATION OF THE MINUTES OF THE MEETING OF THE COUNCIL ASSESSMENT PANEL HELD ON 18 SEPTEMBER 2023**
4. **DECLARATION OF INTERESTS**

5. DEVELOPMENT APPLICATIONS – PDI ACT

5.1 DEVELOPMENT NUMBER 22033399 – ALISON BROOKMAN & STEPHEN BROOKMAN –
 23 HARRIS STREET, NORWOOD

DEVELOPMENT NO.:	22033399
APPLICANT:	Alison Brookman Stephen Brookman
ADDRESS:	23 HARRIS ST NORWOOD SA 5067
NATURE OF DEVELOPMENT:	Construction of a two-storey detached dwelling with a roof-top garden, and construction of a swimming pool
ZONING INFORMATION:	<p>Zones:</p> <ul style="list-style-type: none"> • Established Neighbourhood <p>Overlays:</p> <ul style="list-style-type: none"> • Airport Building Heights (Regulated) • Prescribed Wells Area • Regulated and Significant Tree • Stormwater Management • Traffic Generating Development • Urban Tree Canopy <p>Technical Numeric Variations (TNVs):</p> <ul style="list-style-type: none"> • Minimum Frontage (Minimum frontage for a detached dwelling is 9m; semi-detached dwelling is 8m; row dwelling is 6m; group dwelling is 18m; residential flat building is 18m) • Minimum Site Area (Minimum site area for a detached dwelling is 300 sqm; semi-detached dwelling is 300 sqm; row dwelling is 300 sqm; group dwelling is 300 sqm) • Maximum Building Height (Levels) (Maximum building height is 2 levels)
LODGEMENT DATE:	7 Oct 2022
RELEVANT AUTHORITY:	Assessment panel/Assessment manager at City of Norwood Payneham & St. Peters
PLANNING & DESIGN CODE VERSION:	7 Oct 2022
CATEGORY OF DEVELOPMENT:	Code Assessed - Performance Assessed
NOTIFICATION:	Yes
RECOMMENDING OFFICER:	Kieran Fairbrother Senior Urban Planner
REFERRALS STATUTORY:	Nil
REFERRALS NON-STATUTORY:	Nil

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ATTACHMENT 2:	Subject Land Map	ATTACHMENT 6:	Response to Representations
ATTACHMENT 3:	Zoning Map	ATTACHMENT 7:	Applicant's Responses

DETAILED DESCRIPTION OF PROPOSAL:

This application seeks to construct a two-storey detached dwelling on an existing, small 152m² allotment, together with a roof-top garden and a swimming pool. The proposal involves the partial demolition of an existing four-car garage that currently straddles both the north and west boundaries of the site, retaining these walls but removing the south and east elevations to facilitate the construction of the proposed dwelling on the eastern boundary. Thus, the proposed dwelling is to be constructed across both side boundaries and the rear boundary.

BACKGROUND:

This application was first submitted more than 12 months ago, and in that time extensive discussions have taken place between the Applicant and Council staff which has resulted in the proposal that is currently before the Panel for assessment. The original submission and all subsequent correspondence are contained in **Attachment 7** for the Panel's benefit, to assist in providing context to the recommendation set out at the end of this Report.

SUBJECT LAND & LOCALITY:

Site Description:

Location reference: 23 HARRIS ST NORWOOD SA 5067

Title ref.: CT 5395/185 **Plan Parcel:** F139337 AL57 **Council:** THE CITY OF NORWOOD PAYNEHAM AND ST PETERS

Shape:	regular
Frontage width:	12.50 metres
Area:	approx. 152m ²
Topography:	relatively flat
Existing Structures:	a freestanding four-car garage (outbuilding), disassociated with any other nearby dwelling/building
Existing Vegetation:	nil

Locality

The locality can broadly be described as the area comprising George Street, Harris Street and a small portion of Wall Street that are bound by Beulah Road to the north and Webbe Street to the south. For the Panel's benefit, this is represented in **Attachment 2**.

This locality contains a mix of single- and two-storey dwellings and two commercial car parks. With respect to Harris Street specifically, a two-storey car park dominates the south side of the streetscape, whereas the north side of the streetscape contains a mix of single- and two-storey dwellings of various forms.

CONSENT TYPE REQUIRED:

Planning Consent

CATEGORY OF DEVELOPMENT:

- **PER ELEMENT:**
Swimming pool, spa pool or associated
safety features: Code Assessed - Performance Assessed
Detached dwelling: Code Assessed - Performance Assessed
Partial demolition of a building or structure: Accepted
- **OVERALL APPLICATION CATEGORY:**
Code Assessed - Performance Assessed
- **REASON**
P&D Code

PUBLIC NOTIFICATION

- **REASON**
Boundary walls exceed the relevant height and length in Table 5 of the Established Neighbourhood Zone
- **LIST OF REPRESENTATIONS**

First Name	Family Name	Address	Position	Wishes to be heard?
Oren	Klemich	2C Macklin St, Parkside	Support	No
Elizabeth	Slatter	37 George St, Norwood	Opposed	Yes
Deborah	Skelly	2/31 George St, Norwood	Opposed	No

- **SUMMARY**

The one representor in support of the application simply stated that this is “a good design which suits the allotment”. The concerns raised by the two representors in opposition to the proposal can be summarised as follows:

- Potential overlooking opportunities from the roof-top garden;
- Visual outlook issues caused by the eastern boundary wall;
- Insufficient setbacks from boundaries;
- An overdevelopment of the site.

AGENCY REFERRALS

Nil

INTERNAL REFERRALS

Nil

PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Planning & Design Code, which are contained in **Appendix One**.

Land Use

Desired Outcome 1 of the Established Neighbourhood Zone states:

“A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.”

Performance Outcome 1.1 of the Established Neighbourhood Zone states:

“Predominantly residential development with complementary non-residential activities compatible with the established development pattern of the neighbourhood.”

The corresponding Designated Performance Feature (DPF 1.1) specifically seeks dwellings as an envisaged land use within the Established Neighbourhood Zone, and therefore the proposed land use is acceptable.

Building Height

Performance Outcome 4.1 of the Established Neighbourhood Zone states:

“Buildings contribute to the prevailing character of the neighbourhood and complements the height of nearby buildings.”

A maximum building Technical and Numeric Variation applies to this site of 2 building levels, which is expressed in the corresponding Designated Performance Feature (DPF 4.1).

Part 8 of the Planning & Design Code defines “building level” as:

“Means that portion of a building which is situated between the top of any floor and the top of the next floor above it, and if there is no floor above it, that portion between the top of the floor and the ceiling above it. It does not include any mezzanine or any building level having a floor than is located 1.5m or more below finished ground level.”

With respect to this proposal, the roof-top garden is not covered by any roof or ceiling and therefore does not constitute a building level for the purposes of this definition. Thus, the proposal is for a two-storey dwelling which accords with the TNV applicable to this site and therefore satisfies Performance Outcome 4.1 (above).

Side and Rear Setbacks, Visual Impact, Overshadowing & Overlooking

Performance Outcome 3.1 of the Established Neighbourhood Zone states:

“Building footprints are consistent with the character and pattern of the neighbourhood and provide sufficient space around buildings to limit visual impact, provide an attractive outlook and access to light and ventilation.”

The corresponding Designated Performance Feature (DPF) of the above PO states that site coverages should not exceed 50%. It is wise to consider this DPF in the context of the minimum site areas also sought by DPF 2.1 of the Zone, which in respect of a detached dwelling is 300m². Hence, generally speaking, a 300m² allotment could support a dwelling footprint of 150m².

In this case, however, the proposed dwelling is to be sited on an existing allotment of only 152m² – approximately half of the desired site area for this Zone – and has a site coverage of 132m², or 86.8%. While this is far beyond what DPF 3.1 of the Zone seeks, it is a modest footprint that would be acceptable in principle on most other surrounding allotments. For context, both adjoining sites with frontages to Harris Street have site areas over 300m² and an approximate site coverage of 50% or above.

Performance Outcome 3.1 requires building footprints to be “consistent” with surrounding development, and this proposal achieves that. Considerations regarding visual impact and access to light and ventilation are discussed further below.

Performance Outcome 7.1 of the Established Neighbourhood Zone states:

“Dwelling boundary walls are limited in height and length to manage visual and overshadowing impacts on adjoining properties.”

Performance Outcome 8.1 of the Established Neighbourhood Zone states:

“Buildings are set back from side boundaries to provide:

- (a) Separation between buildings in a way that complements the established character of the locality*
- (b) Access to natural light and ventilation for neighbours”*

Performance Outcome 9.1 of the Established Neighbourhood Zone states:

“Buildings are set back from rear boundaries to provide:

- (a) Separation between buildings in a way that complements the established character of the locality*
- (b) Access to natural light and ventilation for neighbours*
- (c) Private open space*
- (d) Space for landscaping and vegetation.”*

The proposed dwelling features development along all of the west, north and east boundaries of the site. More specifically, this comprises:

- A 7.2m high wall along the western boundary for a length of 10.1m;
- A 7.2m high wall along the northern boundary for a length of 6.4m, followed by a 6m-long wall ranging in height from 2.4m to 2.2m;
- Boundary walling along the eastern boundary that is 2.3m at its lowest and 9.1m at its highest (where it accommodates an internal lift).

It is relevant to note that the site currently contains an outbuilding that has 3m high walls built along both the western and northern boundary for lengths of 8.1m and 9.7m, respectively. The applicant seeks to retain and add to these walls for their dwelling construction.

The abovementioned Performance Outcomes essentially seek similar outcomes: that boundary development, or development close to a boundary, provides sufficient separation between buildings to complement the character of the locality and not unreasonably impact neighbours by way of visual impact or overshadowing. Each of the neighbouring sites to this development will be dealt with in turn.

21 Harris Street (western neighbour)

The dwelling at 21 Harris Street has a setback of approximately 2.7m from the shared side boundary and has one window facing this boundary. This window faces onto the existing garage boundary wall on the subject land and is adjacent to a mature, deciduous tree on that site and therefore enjoys limited solar access and visual outlook. The addition of a second storey wall on this boundary is not considered to unreasonably exacerbate this issue and it is worth noting that this neighbour did not submit a representation during public notification.

With respect to this boundary in particular, the proposal sufficiently accords with Performance Outcome 7.1 (above).

35 George Street (northern neighbour)

The dwelling at 35 George Street is setback approximately 7.5m north of the shared boundary, separated from the boundary by a driveway and landscaping. Further, a large outbuilding is located in the southwest corner of 35 George Street, that extends for a length of approximately 6.6m along the shared boundary with the subject land. The proposed second storey boundary wall on the subject land does not extend laterally any further than this outbuilding and so its visual impacts are not considered to be unreasonable – the relationship between the development and this outbuilding is demonstrated on the north elevation of **Attachment 1**. The balance of the proposed dwelling is setback 2.7m from the rear boundary providing area for private open space at ground level. A privacy screen for the upper level will be attached to the dwelling, and setback 0.5m from the rear boundary. These are not considered to pose any unreasonable visual outlook issues to the northern neighbour because they are adjacent to the driveway and the occupants are unlikely to have any direct views onto this development from proximate private open space or internal windows.

Due to the subject land being south of 35 George Street, the proposed development will not impact solar access or ventilation to this site and thus no overshadowing impacts will arise. Again, it is worth highlighting that this neighbour did not submit a representation during public notification.

With respect to this boundary in particular, the proposal sufficiently accords with Performance Outcome 9.1 (above).

37 George Street (eastern neighbour)

The rear verandah attached to the dwelling at 37 George Street is setback approximately 9.5m east from the shared side boundary. This setback area forms the rear yard and private open space of 37 George Street, and so the occupants of this dwelling will have a direct outlook onto the proposed development; and their concerns about such are expressed in their representation in **Attachment 5**.

The east elevation of the proposed dwelling incorporates a mix of solid walling (in 'Evening Haze' colour or similar), a Colorbond cladding feature wall (in 'Deep Ocean' colour or similar) and privacy screens. This variation in materials and colours has arisen as a result of extensive discussions between the Applicant and council staff (see **Attachment 7**) as a means of trying to minimise, as much as possible, the impact of the development on 37 George Street. The result is a reduction in

the originally-proposed monochromatic, monotonous two-storey boundary walling, and increased articulation and setbacks throughout the elevation. Notwithstanding these changes, it is likely that this development will pose an unreasonable outlook for the eastern neighbour.

Performance Outcome 7.1 of the Zone, however, seeks for boundary walls to be “limited in height and length to manage visual and overshadowing impacts on adjoining properties” (my emphasis).

Further, Performance Outcome 20.3 of the Design in Urban Areas module states:

“The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.”

As earlier mentioned, the subject land is only 152m² and so the reasonable development of the land for a modest two- or three-bedroom dwelling is likely going to necessitate some form of boundary development. The proposed dwelling contains only two bedrooms (although a redesign could incorporate a third), a single garage, a small plunge pool, a rooftop garden for private open space and otherwise modest living and bathroom areas. It is fair to say the dwelling is modest in proportions and is not an overdevelopment of the site.

The Applicant has made favourable changes to their design to reduce the visual impact on the eastern neighbour. Further, it is reasonable to expect this visual impact to be largely the same, albeit slightly reduced, if the dwelling was removed entirely from the eastern boundary and setback, say, 900mm. In this context, the proposed boundary walls might be considered to be limited in height and length to *manage* visual impacts on 37 George Street despite not completely removing those impacts.

With respect to overshadowing, Performance Outcome 3.2 of the Interface Between Land Uses module states:

“Overshadowing of the primary area of private open space... of adjacent residential land uses in... a neighbourhood-type zone is minimised to maintain access to direct winter sunlight”.

The corresponding Designated Performance Feature states:

“Development maintains 2 hours of direct sunlight between 9.00am and 3.00pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following:

(a) For ground level private open space, the smaller of the following:

i. Half the existing ground level open space

Or

ii. 35m² of the existing ground level open space (with at least one of the area’s dimensions measuring 2.5m)

The Applicant has provided shadow diagrams that demonstrate the impact of the development on the private open space of 37 George Street on 21 June. These diagrams show that at 12.00pm the proposed development does not result in any additional overshadowing into the rear yard of 37 George Street. At 3.00pm, the proposed development imposes an additional, approximate 16.6m² of overshadowing into their ground level open space, leaving approximately 32m² of ground level open space not shadowed. Thus, it is reasonable to deduce that between 12.00pm and 3.00pm at least 35m² of existing ground level open space at 37 George Street will maintain 2 hours of direct winter sunlight, satisfying the abovementioned Designated Performance Feature and consequently, in this case, satisfying Performance Outcome 3.2 above.

In this context, and with respect to 37 George Street in particular, the proposal is considered to sufficiently accord with Performance Outcomes 7.1 and 8.1 of the Zone.

To briefly summarise all of the above, the proposed dwelling will have two storey walls along the western, northern and eastern boundaries. The impacts posed by these walls to the western and northern neighbours are acceptable. The impacts posed by the eastern boundary development to 37 George Street, however, will be noticeable by the occupants of that site, but those impacts are considered acceptable given the constraints of the subject land and the design elements of the elevation.

With respect to the rooftop garden, and concerns about overlooking raised by one representor, Performance Outcome 10.2 of the Interface Between Land Uses module states:

“Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.”

The corresponding Designated Performance Feature states that except where a balcony faces a public road or reserve:

“all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of:

- (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land*
- Or*
- (ii) 1.7m above finished floor level in all other cases.”*

Although the rooftop garden is not a balcony per se, it can be considered a terrace and therefore this is the most relevant policy in respect of overlooking concerns.

To address overlooking concerns, and to break up the visual bulk of the building, the application proposes pool fence style fencing along the perimeter of the rooftop garden behind which will be translucent polycarbonate screens. The fencing and polycarbonate screens will extend to 1.7m above the floor level of the rooftop garden. The plans do not specify the level of transparency/obscurity of the polycarbonate screens, so Condition 2 contained within the recommendation at the end of this report has been worded to ensure that these screens have a maximum 25% transparency in accordance with DPF 10.2 above.

This condition also applies to the front second level balcony which would otherwise provide views into the rear yard of 37 George Street. Notwithstanding, the east elevation in **Attachment 1** does demonstrate floor-to-ceiling screens with a maximum transparency of 25% and so even in the absence of this condition this aspect of the development satisfies Performance Outcome 10.2 (above).

Performance Outcome 10.1 of the Interface Between Land Uses module states:

“Development mitigates direct overlooking from upper-level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.”

The proposed dwelling incorporates upper-level windows on the north and east elevations for the living and dining area. Rather than use obscure glazing, the Applicant has opted to install a 5.7m x 2.7m long perforated metal privacy screen external to these windows as a means of mitigating direct overlooking into neighbouring windows and private open space. This screen is 2m tall, extending from 500mm below the second storey internal floor level to 1.5m above.

The Applicant has provided 3D sightline images that show the extent of views able to be obtained of neighbouring properties from different points within the second level living space (see **Attachment 1**). These demonstrate that any views into neighbouring properties are very minimal and will not unduly compromise the privacy that the northern and eastern neighbour expect to enjoy. Accordingly, this screen is considered sufficient for satisfying Performance Outcome 10.1 above, and Condition 2 reiterates the requirement to permanently maintain the screen at a maximum 25% transparency.

Front setback, Design & Appearance

Performance Outcome 5.1 of the Established Neighbourhood Zone states:

“Buildings are set back from primary street boundaries consistent with the existing streetscape.”

The adjoining dwelling at 21 Harris Street has a front setback of approximately 1.5m and 37 George Street has a side setback to Harris Street of approximately 1m. Other dwellings along Harris Street have varying front setbacks but all are relatively close to the front boundary. The proposed dwelling will have a front setback of 1.5m to the ground level and 1m to the projecting upper floor, which is considered consistent with the existing streetscape.

Performance Outcome 10.1 of the Established Neighbourhood Zone states:

“Garages and carports are designed and sited to be discrete and not dominate the appearance of the associated dwelling when viewed from the street.”

The garage will be setback 5.5m from the street and 3.5m behind the closest street-facing wall of the dwelling which includes the second storey above, resulting in a barely visible garage in the streetscape in accordance with this Performance Outcome.

Performance Outcome 10.2 of the Established Neighbourhood Zone states:

“The appearance of development as viewed from public roads is sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality.”

The predominant housing stock in the locality is one of single- and two-storey dwellings with hipped roofs of a traditionally low pitch, along with some villas that incorporate gable ends. The proposed dwelling does not incorporate a pitched roof, but instead the rooftop garden, which is inconsistent with the predominant housing stock. Slightly west of the subject land are five two-storey row dwellings that have two storey walls, as does the proposed dwelling. But this does not represent the predominant housing stock in the locality, which is one of mainly single storey dwellings. Accordingly, it cannot be said that the proposed development is “sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality”. However, it is compatible with surrounding development (for reasons outlined above and below) and will make a positive contribution to the streetscape, as explained below. Accordingly, the failure of the proposal to meet this Performance Outcome should not condemn the application.

Desired Outcome 1 of the Design in Urban Areas module states:

“Development is:

- (a) Contextual – by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality*
- (b) Durable – fit for purpose, adaptable and long lasting*
- (c) ...*
- (d) Sustainable – by integrating sustainable techniques in to the design and siting of development and landscaping.”*

Performance Outcome 17.1 of the Design in Urban Areas module states:

“Dwellings incorporate windows facing primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.”

Performance Outcome 17.2 of the Design in Urban Areas module states:

“Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.”

Performance Outcome 20.2 of the Design in Urban Areas module states:

“Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape...”

The proposed dwelling incorporates the following features into its façade, which together will make a positive contribution to the streetscape in accordance with the above Performance Outcomes:

- A visible, legible entry way;
- A complementary mix of colours and materials throughout the elevation, including the use of corten steel for a feature window on the second level;
- Articulation both within and between building levels, including the slight cantilevering of the second level above the ground level and the use of a balcony to break up the visual bulk of the building; and
- A good level of fenestration, including windows on both building levels that provide opportunities for passive surveillance.

Additionally, the implementation of soft landscaping at both ground level and on the rooftop garden will help soften the appearance and bulk of the building and provide visual interest to the public realm.

Private Open Space and Soft Landscaping

Performance Outcome 21.1 of the Design in Urban Areas module states:

“Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.”

The corresponding Designated Performance Feature suggests that a minimum 24m² of private open space is sufficient for a dwelling on a site with an area of 300m² or less.

The proposed dwelling incorporates private open space in three separate areas. At ground level, there is a small courtyard and plunge pool in the rear yard of approximately 15.5m². There is also a small 4m² balcony area on the second level and the 89.5m² rooftop garden above (58m² excluding areas of soft landscaping). With approximately 77m² of usable private open space available to the occupants of this dwelling, the proposal adequately satisfies PO 21.1 above.

Performance Outcome 22.1 of the Design in Urban Areas module states:

- “Soft landscaping is incorporated into development to:*
- (a) Minimise heat absorption and reflection*
 - (b) Contribute shade and shelter*
 - (c) Provide for stormwater infiltration and biodiversity*
 - (d) Enhance the appearance of land and streetscapes.”*

The corresponding Designated Performance Feature suggests that sites under 150m² should be 10% comprised of soft landscaping and sites between 150-200m² should be 15% comprised of soft landscaping. Accordingly, an amount of soft landscaping over 10% of the site area is considered sufficient for this site given its constraints. Notwithstanding, the proposal includes 29m² of soft landscaping on the rooftop garden and a further 17.5m² at ground level (of which 14.5m² is between the dwelling and the front boundary), totalling 30.6% of the site. This exceeds the expectations of DPF 22.1 and is considered to therefore meet the Performance Outcome (above).

Access and Parking

Performance Outcome 23.3 of the Design in Urban Areas module states:

- “Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, domestic waste collection, landscaped street frontages and on-street parking.”*

The subject land is currently serviced by a 9.6m wide crossover, part of which will be reused by the proposed development. The portion of this crossover that is not going to be required by the development is noted on the Site Plan (**Attachment 1**) as being reinstated to upright kerb & gutter. This kerb reinstatement will provide room for an additional on-street car parking space (in an area where on-street car parking is in high demand), street tree planting and room for domestic waste collection to take place to service this dwelling. Accordingly, Condition 5 set out in the recommendation below requires the Applicant to undertake this work prior to occupation of the dwelling.

In respect of car parking requirements, Performance Outcome 5.1 of the Transport, Access and Parking module states:

- “Sufficient on-site vehicle parking... places are provided to meet the needs of the development...”*

The corresponding Designated Performance Features suggests that a dwelling with 2 or more bedrooms should be provided with 2 car parking spaces, 1 of which is to be covered.

The proposed dwelling will provide 2 car parking spaces, in the form of a single garage and a 5.5m driveway. Both of these car parking spaces meet the minimum dimensions set out in Performance Outcomes 23.1 and 23.2 of the Design in Urban Areas module (see **Appendix 1**). Consequently, the proposed development satisfies the Code's expectations in respect of off-street car parking spaces, as well as providing an additional car parking space to the public realm.

Interface Issues

Performance Outcome 4.3 of the Interface Between Land Uses module states:

“Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).”

The application documentation does not show the location of the swimming pool pump and equipment and so an assessment against this Performance Outcome cannot be done. Consequently, Condition 6 (below) has been recommended to ensure the suitable enclosure of the swimming pool equipment takes effect so as to protect the amenity of adjacent sensitive receivers.

CONCLUSION

The proposed dwelling represents a well-balanced response to the development of a small parcel of land in an established neighbourhood. Existing site constraints prevent the more-sought-after 3- to 4-bedroom dwelling from being constructed on this site, whereas the Applicant’s 2-bedroom dwelling is a more modest and appropriate response. The good level of articulation and mix of materials and colours provide visual interest to benefit the Harris Street streetscape. The eastern boundary wall has been modified to mitigate visual impacts on the adjacent neighbour through changes in materials, colours and setbacks, whereas both the northern and western neighbours suffer no additional unreasonable visual outlook impacts as a result of the two-storey boundary development. Sufficient car parking spaces have been provided on site to cater for the needs of the occupants, and an additional, valuable on-street car parking space will be provided within close proximity to The Parade. Finally, the rooftop garden provides opportunity for additional soft landscaping and will assist in improving the environmental performance of the building while providing the occupants with some additional private open space.

Overall, the proposed development sufficiently satisfies the provisions of the Planning and Design Code to warrant support. There will be some inevitable visual impacts caused to the eastern neighbour by virtue of its close proximity to their private open space, but the relevant policy (PO 7.1 of the Zone) talks about boundary walls being *managed* to limit their impact. As evidenced in **Attachment 7**, the various iterations that this proposal has gone through to get to the point it is at now sufficiently demonstrates the management of this issue. Any further mitigation of these impacts would require a wholesale redesign of the dwelling, which is not considered necessary when the proposal as a whole is balanced against the provisions of the Planning & Design Code. It is considered worthy of consent.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107(2)(c) of the Planning, Development and Infrastructure Act 2016, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code; and
2. Development Application Number 22033399, by Alison Brookman and Stephen Brookman is granted Planning Consent subject to the following conditions:

CONDITIONS

Planning Consent

Condition 1

The development granted Planning Consent shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below (if any).

Condition 2

All balustrades/screens around the rooftop garden and second level front balcony, with the exception of the southern elevation, shall be permanently treated with a maximum 25% transparency to a height of 1700mm above floor level, prior to occupation of the building, in a manner that restricts views being obtained by a person occupying the balcony, to the reasonable satisfaction of the Assessment Manager and such treatment shall be maintained in good condition at all times.

Similarly, the second level privacy screen on the east and north elevations shall be permanently treated with a maximum 25% transparency, prior to occupation of the building, in a manner that restricts views being obtained by a person within the building, to the reasonable satisfaction of the Assessment Manager and such treatment shall be maintained in good condition at all times.

Condition 3

All areas nominated as landscaping or garden areas on the approved plans shall be planted with a suitable mix and density of trees, shrubs and groundcovers within the next available planting season after the occupation of the premises to the reasonable satisfaction of the Assessment Manager and such plants, as well as any existing plants which are shown to be retained, shall be nurtured and maintained in good health and condition at all times, with any diseased or dying plants being replaced, to the reasonable satisfaction of the Council or its delegate.

Condition 4

Either:

1. Tree(s) must be planted and/or retained in accordance with DTS/DPF 1.1 of the Urban Tree Canopy Overlay in the Planning and Design Code (as at the date of lodgement of the application). New trees must be planted within 12 months of occupation of the dwelling(s) and maintained.
2. Where provided for by any relevant off-set scheme established under section 197 of the Planning, Development and Infrastructure Act 2016 (as at the date of lodgement of the application), payment of an amount calculated in accordance with the off-set scheme may be made in lieu of planting/retaining 1 or more trees as set out in the Urban Tree Canopy Overlay in the Planning and Design Code (as at the date of lodgement of the application). Payment must be made prior to the issue of development approval.

Condition 5

The portion of the existing crossover invert on Harris Street that is made redundant as a result of this development shall be reinstated to upright kerb & gutter in accordance with Council specifications prior to occupation of the dwelling, and to the reasonable satisfaction of the Assessment Manager or its delegate.

Condition 6

That the associated swimming pool filter pump be enclosed in such a way that noise levels do not exceed 45db(a) measured at adjoining property boundaries.

Condition 7

All stormwater from buildings and paved areas shall be disposed of in accordance with recognised engineering practices in a manner and with materials that does not result in the entry of water onto any adjoining property or any building, and does not affect the stability of any building and in all instances the stormwater drainage system shall be directly connected into either the adjacent street kerb & water table or a Council underground pipe drainage system.

Condition 8

The approved development must include rainwater tank storage which is:

1. connected to at least 60% of the roof area;
2. connected to either a toilet, laundry cold water outlets or hot water service;
3. with a minimum retention capacity of 1000 litres;
4. with a minimum detention capacity of 1000 litres; and
5. includes a 20-25 mm diameter slow release orifice at the bottom of the detention component of the tank

within 12 months of occupation of the dwelling(s).

ADVISORY NOTES

Planning Consent

Advisory Note 1

Appeal Rights - General rights of review and appeal exist in relation to any assessment, request, direction or act of a relevant authority in relation to the determination of this application, including conditions.

Advisory Note 2

Consents issued for this Development Application will remain valid for the following periods of time:

1. Planning Consent is valid for 24 months following the date of issue, within which time Development Approval must be obtained;
2. Development Approval is valid for 24 months following the date of issue, within which time works must have substantially commenced on site;
3. Works must be substantially completed within 3 years of the date on which Development Approval is issued.

If an extension is required to any of the above-mentioned timeframes a request can be made for an extension of time by emailing the Planning Department at townhall@npsp.sa.gov.au. Whether or not an extension of time will be granted will be at the discretion of the relevant authority.

Advisory Note 3

No work can commence on this development unless a Development Approval has been obtained. If one or more Consents have been granted on this Decision Notification Form, you must not start any site works or building work or change of use of the land until you have received notification that Development Approval has been granted.

Advisory Note 4

To assist in the interpretation of the Urban Tree Canopy condition noted above, where payment into a relevant off-set scheme is not possible or chosen, tree(s) must be planted in accordance with the requirements set out below. Further guidance and information can be obtained by visiting the Landscaping and Development webpage on the Council's website (https://www.npsp.sa.gov.au/planning_and_development/landscaping-and-development) or contacting the Council's Planning Department on (08) 8366 4555.

Lot Size Per Dwelling (m2) // Tree Size and Number Required

<450 // 1 small tree

450-800 // 1 medium tree or 2 small trees

>800 // 1 large tree or 2 medium trees or 4 small trees

Tree Size// Mature Height (minimum) // Mature Spread (minimum) // Soil Area Around Tree Within Development Site (minimum)

Small // 4m // 2m // 10m² and min. dimension of 1.5m

Medium // 6m // 4m // 30m² and min. dimension of 2m

Large // 12m // 8m // 60m² and min. dimension of 4m

Advisory Note 5

The granting of this consent does not remove the need for the beneficiary to obtain all other consents which may be required by any other legislation.

The Applicant's attention is particularly drawn to the requirements of the *Fences Act 1975* regarding notification of any neighbours affected by new boundary development or boundary fencing. Further information is available in the 'Fences and the Law' booklet available through the Legal Services Commission.

Advisory Note 6

The Applicant is reminded of its responsibilities under the *Environment Protection Act 1993*, to not harm the environment. Specifically, paint, plaster, concrete, brick wastes and wash waters should not be discharged into the stormwater system, litter should be appropriately stored on site pending removal, excavation and site disturbance should be limited, entry/exit points to the site should be managed to prevent soil being carried off site by vehicles, sediment barriers should be used (particularly on sloping sites), and material stockpiles should all be placed on site and not on the footpath or public roads or reserves. Further information is available by contacting the EPA.

Advisory Note 7

The Applicant is advised that construction noise is not allowed:

1. on any Sunday or public holiday; or
2. after 7pm or before 7am on any other day

Advisory Note 8

The Applicant is advised that any works undertaken on Council owned land (including but not limited to works relating to crossovers, driveways, footpaths, street trees and stormwater connections) will require the approval of the Council pursuant to the *Local Government Act 1999* prior to any works being undertaken. Further information may be obtained by contacting Council's Public Realm Compliance Officer on 8366 4513.

Advisory Note 9

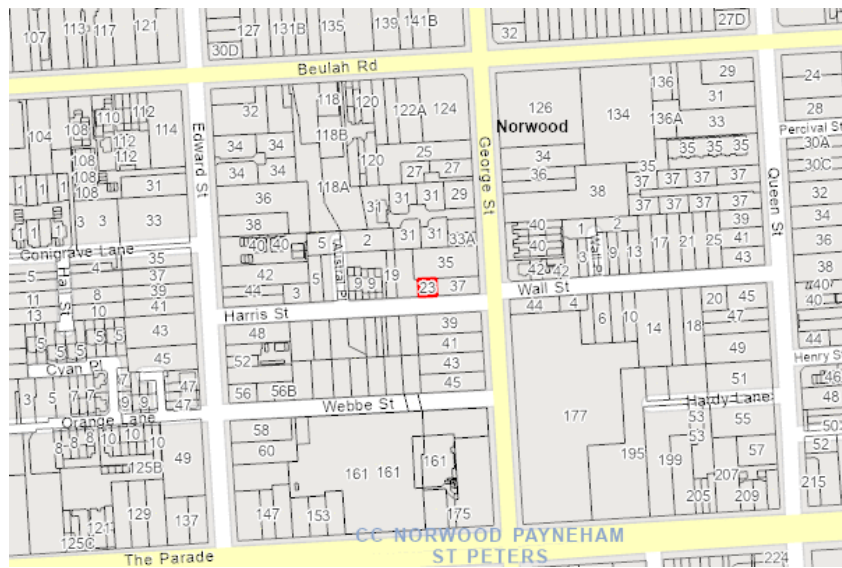
The Applicant is advised that the condition of the footpath, kerbing, vehicular crossing point, street tree(s) and any other Council infrastructure located adjacent to the subject land will be inspected by the Council prior to the commencement of building work and at the completion of building work. Any damage to Council infrastructure that occurs during construction must be rectified as soon as practicable and in any event, no later than four (4) weeks after substantial completion of the building work. The Council reserves its right to recover all costs associated with remedying any damage that has not been repaired in a timely manner from the appropriate person.

Advisory Note 10

The Council has not surveyed the subject land and has, for the purpose of its assessment, assumed that all dimensions and other details provided by the Applicant are correct and accurate.

23 HARRIS ST NORWOOD SA 5067**Address:**Click to view a detailed interactive [SAILIS](#) in SAILIS

To view a detailed interactive property map in SAPPA click on the map below

**Property Zoning Details****Local Variation (TNV)**

Minimum Frontage (*Minimum frontage for a detached dwelling is 9m; semi-detached dwelling is 8m; row dwelling is 6m; group dwelling is 18m; residential flat building is 18m*)

Minimum Site Area (*Minimum site area for a detached dwelling is 300 sqm; semi-detached dwelling is 300 sqm; row dwelling is 300 sqm; group dwelling is 300 sqm*)

Maximum Building Height (Levels) (*Maximum building height is 2 levels*)

Overlay

Airport Building Heights (Regulated) (*All structures over 45 metres*)

Prescribed Wells Area

Regulated and Significant Tree

Stormwater Management

Traffic Generating Development

Urban Tree Canopy

Zone

Established Neighbourhood

Selected Development(s)

Detached dwelling

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards.

If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

Detached dwelling - Code Assessed - Performance Assessed

Part 2 - Zones and Sub Zones

Established Neighbourhood Zone

Assessment Provisions (AP)

Desired Outcome	
DO 1	A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.
DO 2	Maintain the predominant streetscape character, having regard to key features such as roadside plantings, footpaths, front yards, and space between crossovers.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature		
Land Use and Intensity			
PO 1.1 Predominantly residential development with complementary non-residential activities compatible with the established development pattern of the neighbourhood.	DTS/DPF 1.1 Development comprises one or more of the following: <ul style="list-style-type: none"> (a) Ancillary accommodation (b) Community facility (c) Consulting room (d) Dwelling (e) Office (f) Recreation area (g) Shop. 		
Site Dimensions and Land Division			
PO 2.1 Allotments/sites for residential purposes are of suitable size and dimension to accommodate the anticipated dwelling form and are compatible with the prevailing development pattern in the locality.	DTS/DPF 2.1 Development will not result in more than 1 dwelling on an existing allotment or Development involves the conversion of an existing dwelling into two or more dwellings and the existing dwelling retains its original external appearance to the public road or Allotments/sites for residential purposes accord with the following: <ul style="list-style-type: none"> (a) site areas (or allotment areas in the case of land division) are not less than the following (average site area per dwelling, including common areas, applies for group dwellings or dwellings within a residential flat building): <table border="1" style="width: 100%; margin-top: 10px;"> <thead> <tr> <th style="text-align: center;">Minimum Site Area</th> </tr> </thead> <tbody> <tr> <td>Minimum site area for a detached dwelling is 300 sqm; semi-detached dwelling is 300 sqm; row dwelling is 300 sqm; group dwelling is 300 sqm</td> </tr> </tbody> </table>	Minimum Site Area	Minimum site area for a detached dwelling is 300 sqm; semi-detached dwelling is 300 sqm; row dwelling is 300 sqm; group dwelling is 300 sqm
Minimum Site Area			
Minimum site area for a detached dwelling is 300 sqm; semi-detached dwelling is 300 sqm; row dwelling is 300 sqm; group dwelling is 300 sqm			

	<p>and</p> <p>(b) site frontages (or allotment frontages in the case of land division) are not less than:</p> <table border="1" style="margin-left: auto; margin-right: auto; border-collapse: collapse;"> <thead> <tr> <th style="text-align: center; padding: 2px;">Minimum Frontage</th> </tr> </thead> <tbody> <tr> <td style="padding: 2px;">Minimum frontage for a detached dwelling is 9m; semi-detached dwelling is 8m; row dwelling is 6m; group dwelling is 18m; residential flat building is 18m</td> </tr> </tbody> </table> <p>In relation to DTS/DPF 2.1, in instances where:</p> <p>(c) more than one value is returned in the same field, refer to the <i>Minimum Frontage Technical and Numeric Variation</i> layer or <i>Minimum Site Area Technical and Numeric Variation</i> layer in the SA planning database to determine the applicable value relevant to the site of the proposed development</p> <p>(d) no value is returned in (a) or (b) (i.e. there is a blank field or the relevant dwelling type is not listed), then none are applicable and the relevant development cannot be classified as deemed-to-satisfy.</p>	Minimum Frontage	Minimum frontage for a detached dwelling is 9m; semi-detached dwelling is 8m; row dwelling is 6m; group dwelling is 18m; residential flat building is 18m
Minimum Frontage			
Minimum frontage for a detached dwelling is 9m; semi-detached dwelling is 8m; row dwelling is 6m; group dwelling is 18m; residential flat building is 18m			
<p>PO 2.2</p> <p>Development creating new allotments/sites in conjunction with retention of an existing dwelling ensures the site of the existing dwelling remains fit for purpose.</p>	<p>DTS/DPF 2.2</p> <p>Where the site of a dwelling does not comprise an entire allotment:</p> <p>(a) the balance of the allotment accords with the requirements specified in Established Neighbourhood Zone DTS/DPF 2.1, with 10% reduction in minimum site area where located in a Character Area Overlay or Historic Area Overlay</p> <p>(b) if there is an existing dwelling on the allotment that will remain on the allotment after completion of the development it will not contravene:</p> <ul style="list-style-type: none"> (i) private open space requirements specified in Design in Urban Areas Table 1 - Private Open Space (ii) car parking requirements specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas to the nearest whole number. 		
Site coverage			
<p>PO 3.1</p> <p>Building footprints are consistent with the character and pattern of the neighbourhood and provide sufficient space around buildings to limit visual impact, provide an attractive outlook and access to light and ventilation.</p>	<p>DTS/DPF 3.1</p> <p>Development does not result in site coverage exceeding:</p> <p>In instances where:</p> <p>(a) no value is returned (i.e. there is a blank field), then a maximum 50% site coverage applies</p> <p>(b) more than one value is returned in the same field, refer to the Site Coverage Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development.</p>		

Building Height			
<p>PO 4.1</p> <p>Buildings contribute to the prevailing character of the neighbourhood and complements the height of nearby buildings.</p>	<p>DTS/DPF 4.1</p> <p>Building height (excluding garages, carports and outbuildings) is no greater than:</p> <p>(a) the following:</p> <table border="1" data-bbox="829 369 1516 448"> <thead> <tr> <th>Maximum Building Height (Levels)</th> </tr> </thead> <tbody> <tr> <td>Maximum building height is 2 levels</td> </tr> </tbody> </table> <p>(b) in all other cases (i.e. there are blank fields for both maximum building height (metres) and maximum building height (levels)) - 2 building levels up to a height of 9m.</p> <p>In relation to DTS/DPF 4.1, in instances where:</p> <p>(c) more than one value is returned in the same field, refer to the <i>Maximum Building Height (Levels) Technical and Numeric Variation layer</i> or <i>Maximum Building Height (Meters) Technical and Numeric Variation layer</i> in the SA planning database to determine the applicable value relevant to the site of the proposed development.</p> <p>(d) only one value is returned for DTS/DPF 4.1(a) (i.e. there is one blank field), then the relevant height in metres or building levels applies with no criteria for the other.</p>	Maximum Building Height (Levels)	Maximum building height is 2 levels
Maximum Building Height (Levels)			
Maximum building height is 2 levels			
Primary Street Setback			
<p>PO 5.1</p> <p>Buildings are set back from primary street boundaries consistent with the existing streetscape.</p>	<p>DTS/DPF 5.1</p> <p>The building line of a building is set back from the primary street boundary:</p> <p>(a) at least the average setback to the building line of existing buildings on adjoining sites which face the same primary street (including those buildings that would adjoin the site if not separated by a public road or a vacant allotment)</p> <p>(b) where there is only one existing building on adjoining sites which face the same primary street (including those that would adjoin if not separated by a public road or a vacant allotment), not less than the setback to the building line of that building</p> <p>or</p> <p>(c) in all other cases, no DTS/DPF is applicable.</p>		
Secondary Street Setback			
<p>PO 6.1</p> <p>Buildings are set back from secondary street boundaries (not being a rear laneway) to maintain the established pattern of separation between buildings and public streets and reinforce streetscape character.</p>	<p>DTS/DPF 6.1</p> <p>Building walls are set back from the secondary street boundary (other than a rear laneway):</p> <p>(a) no less than:</p> <p>or</p> <p>(b) 900mm, whichever is greater</p> <p>or</p>		

	<p>(c) if a dwelling on any adjoining allotment is closer to the secondary street, the distance of that dwelling from the boundary with the secondary street.</p> <p>In instances where no value is returned in DTS/DPF 6.1(a) (i.e. there is a blank field), then it is taken that the value for DTS/DPF 6.1(a) is zero.</p>
Boundary Walls	
<p>PO 7.1</p> <p>Dwelling boundary walls are limited in height and length to manage visual and overshadowing impacts on adjoining properties.</p>	<p>DTS/DPF 7.1</p> <p>Dwellings do not incorporate side boundary walls where a side boundary setback value is returned in (a) below:</p> <p>(a)</p> <p>or</p> <p>(b) where no side boundary setback value is returned in (a) above, and except where the dwelling is located on a central site within a row dwelling or terrace arrangement, side boundary walls occur only on one side boundary and satisfy (i) or (ii) below:</p> <p>(i) side boundary walls adjoin or abut a boundary wall of a building on adjoining land for the same or lesser length and height</p> <p>(ii) side boundary walls do not:</p> <p>A. exceed 3.2m in height from the lower of the natural or finished ground level</p> <p>B. exceed 8m in length</p> <p>C. when combined with other walls on the boundary of the subject development site, exceed a maximum 45% of the length of the boundary</p> <p>D. encroach within 3m of any other existing or proposed boundary walls on the subject land.</p>
Side Boundary Setback	
<p>PO 8.1</p> <p>Buildings are set back from side boundaries to provide:</p> <p>(a) separation between buildings in a way that complements the established character of the locality</p> <p>(b) access to natural light and ventilation for neighbours.</p>	<p>DTS/DPF 8.1</p> <p>Other than walls located on a side boundary in accordance with Established Neighbourhood Zone DTS/DPF 7.1, building walls are set back from the side boundary:</p> <p>(a) no less than:</p> <p>(b) in all other cases (i.e. there is a blank field), then:</p> <p>(i) at least 900mm where the wall is up to 3m</p> <p>(ii) other than for a south facing wall, at least 900mm plus 1/3 of the wall height above 3m</p> <p>(iii) at least 1.9m plus 1/3 of the wall height above 3m for south facing walls.</p>
Rear Boundary Setback	
<p>PO 9.1</p>	<p>DTS/DPF 9.1</p>

Buildings are set back from rear boundaries to provide:	Other than in relation to an access lane way, buildings are set back from the rear boundary at least:
<ul style="list-style-type: none"> (a) separation between dwellings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours (c) private open space (d) space for landscaping and vegetation. 	<ul style="list-style-type: none"> (a) 4m for the first building level (b) 6m for any second building level.
Appearance	
PO 10.1 Garages and carports are designed and sited to be discrete and not dominate the appearance of the associated dwelling when viewed from the street.	DTS/DPF 10.1 Garages and carports facing a street (other than an access lane way): <ul style="list-style-type: none"> (a) are set back at least 0.5m behind the building line of the associated dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a total garage door / opening width not exceeding 30% of the allotment or site frontage, to a maximum width of 7m.
PO 10.2 The appearance of development as viewed from public roads is sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality.	DTS/DPF 10.2 None are applicable.

Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

Notification tables exclude the classes of development listed in Column A from notification provided that they do not fall within a corresponding exclusion prescribed in Column B.

Where a development or an element of a development falls within more than one class of development listed in Column A, it will be excluded from notification if it is excluded (in its entirety) under any of those classes of development. It need not be excluded under all applicable classes of development.

Where a development involves multiple performance assessed elements, all performance assessed elements will require notification (regardless of whether one or more elements are excluded in the applicable notification table) unless every performance assessed element of the application is excluded in the applicable notification table, in which case the application will not require notification.

Class of Development (Column A)	Exceptions (Column B)
1. Development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. All development undertaken by: <ul style="list-style-type: none"> (a) the South Australian Housing Trust either 	Except development involving any of the following:

<p>individually or jointly with other persons or bodies or (b) a provider registered under the Community Housing National Law participating in a program relating to the renewal of housing endorsed by the South Australian Housing Trust.</p>	<ol style="list-style-type: none"> 1. residential flat building(s) of 3 or more building levels 2. the demolition of a State or Local Heritage Place 3. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
<p>3. Any development involving any of the following (or of any combination of any of the following):</p> <ol style="list-style-type: none"> (a) air handling unit, air conditioning system or exhaust fan (b) ancillary accommodation (c) building work on railway land (d) carport (e) deck (f) dwelling (g) dwelling addition (h) fence (i) outbuilding (j) pergola (k) private bushfire shelter (l) residential flat building (m) retaining wall (n) shade sail (o) solar photovoltaic panels (roof mounted) (p) swimming pool or spa pool (q) verandah (r) water tank. 	<p>Except development that:</p> <ol style="list-style-type: none"> 1. exceeds the maximum building height specified in Established Neighbourhood Zone DTS/DPF 4.1 or 2. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ol style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 8m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
<p>4. Any development involving any of the following (or of any combination of any of the following):</p> <ol style="list-style-type: none"> (a) consulting room (b) office (c) shop. 	<p>Except development that:</p> <ol style="list-style-type: none"> 1. does not satisfy Established Neighbourhood Zone DTS/DPF 1.2 or 2. exceeds the maximum building height specified in Established Neighbourhood Zone DTS/DPF 4.1 or 3. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ol style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 8m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
<p>5. Any of the following (or of any combination of any of the following):</p>	<p>None specified.</p>

<ul style="list-style-type: none"> (a) internal building works (b) land division (c) recreation area (d) replacement building (e) temporary accommodation in an area affected by bushfire (f) tree damaging activity. 	
6. Demolition.	<p>Except any of the following:</p> <ul style="list-style-type: none"> 1. the demolition of a State or Local Heritage Place 2. the demolition of a building (except an ancillary building) in a Historic Area Overlay.
Placement of Notices - Exemptions for Performance Assessed Development	
None specified.	
Placement of Notices - Exemptions for Restricted Development	
None specified.	

Part 3 - Overlays

Airport Building Heights (Regulated) Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Management of potential impacts of buildings and generated emissions to maintain operational and safety requirements of registered and certified commercial and military airfields, airports, airstrips and helicopter landing sites.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Building height does not pose a hazard to the operation of a certified or registered aerodrome.	DTS/DPF 1.1 Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas. In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It

sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Any of the following classes of development:</p> <p>(a) building located in an area identified as 'All structures' (no height limit is prescribed) or will exceed the height specified in the <i>Airport Building Heights (Regulated) Overlay</i></p> <p>(b) building comprising exhaust stacks that generates plumes, or may cause plumes to be generated, above a height specified in the <i>Airport Building Heights (Regulated) Overlay</i>.</p>	<p>The airport-operator company for the relevant airport within the meaning of the <i>Airports Act 1996</i> of the Commonwealth or, if there is no airport-operator company, the Secretary of the Minister responsible for the administration of the <i>Airports Act 1996</i> of the Commonwealth.</p>	<p>To provide expert assessment and direction to the relevant authority on potential impacts on the safety and operation of aviation activities.</p>	<p>Development of a class to which Schedule 9 clause 3 item 1 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>

Stormwater Management Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development incorporates water sensitive urban design techniques to capture and re-use stormwater.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Residential development is designed to capture and re-use stormwater to:</p> <p>(a) maximise conservation of water resources</p> <p>(b) manage peak stormwater runoff flows and volume to ensure the carrying capacities of downstream systems are not overloaded</p> <p>(c) manage stormwater runoff quality.</p>	<p>DTS/DPF 1.1</p> <p>Residential development comprising detached, semi-detached or row dwellings, or less than 5 group dwellings or dwellings within a residential flat building:</p> <p>(a) includes rainwater tank storage:</p> <p>(i) connected to at least:</p> <p>A. in relation to a detached dwelling (not in a battle-axe arrangement), semi-detached dwelling or row dwelling, 60% of the roof area</p> <p>B. in all other cases, 80% of the roof area</p> <p>(ii) connected to either a toilet, laundry cold water outlets or hot water service for sites less than 200m²</p> <p>(iii) connected to one toilet and either the laundry cold water outlets or hot water service for sites of 200m² or greater</p> <p>(iv) with a minimum total capacity in accordance with Table 1</p> <p>(v) where detention is required, includes a 20-25 mm diameter slow release orifice at the bottom</p>

	<p style="text-align: center;">of the detention component of the tank</p> <p>(b) incorporates dwelling roof area comprising at least 80% of the site's impervious area</p> <p>Table 1: Rainwater Tank</p> <table border="1"> <thead> <tr> <th>Site size (m²)</th> <th>Minimum retention volume (Litres)</th> <th>Minimum detention volume (Litres)</th> </tr> </thead> <tbody> <tr> <td><200</td> <td>1000</td> <td>1000</td> </tr> <tr> <td>200-400</td> <td>2000</td> <td>Site perviousness <30%: 1000 Site perviousness ≥30%: N/A</td> </tr> <tr> <td>>401</td> <td>4000</td> <td>Site perviousness <35%: 1000 Site perviousness ≥35%: N/A</td> </tr> </tbody> </table>	Site size (m ²)	Minimum retention volume (Litres)	Minimum detention volume (Litres)	<200	1000	1000	200-400	2000	Site perviousness <30%: 1000 Site perviousness ≥30%: N/A	>401	4000	Site perviousness <35%: 1000 Site perviousness ≥35%: N/A
Site size (m ²)	Minimum retention volume (Litres)	Minimum detention volume (Litres)											
<200	1000	1000											
200-400	2000	Site perviousness <30%: 1000 Site perviousness ≥30%: N/A											
>401	4000	Site perviousness <35%: 1000 Site perviousness ≥35%: N/A											

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Traffic Generating Development Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes and Major Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from urban transport routes and major urban transport routes.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance
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		Feature
Traffic Generating Development		
<p>PO 1.1</p> <p>Development designed to minimise its potential impact on the safety, efficiency and functional performance of the State Maintained Road network.</p>	<p>DTS/DPF 1.1</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more. 	
<p>PO 1.2</p> <p>Access points sited and designed to accommodate the type and volume of traffic likely to be generated by development.</p>	<p>DTS/DPF 1.2</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more. 	
<p>PO 1.3</p> <p>Sufficient accessible on-site queuing provided to meet the needs of the development so that queues do not impact on the State Maintained Road network.</p>	<p>DTS/DPF 1.3</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more. 	

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
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<p>Except where all of the relevant deemed-to-satisfy criteria are met, any of the following classes of development that are proposed within 250m of a State Maintained Road:</p> <ul style="list-style-type: none"> (a) land division creating 50 or more additional allotments (b) commercial development with a gross floor area of 10,000m² or more (c) retail development with a gross floor area of 2,000m² or more (d) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (e) industry with a gross floor area of 20,000m² or more (f) educational facilities with a capacity of 250 students or more. 	<p>Commissioner of Highways.</p>	<p>To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.</p>	<p>Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>
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Urban Tree Canopy Overlay

Assessment Provisions (AP)

Desired Outcome	
DO 1	Residential development preserves and enhances urban tree canopy through the planting of new trees and retention of existing mature trees where practicable.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature																				
<p>PO 1.1</p> <p>Trees are planted or retained to contribute to an urban tree canopy.</p>	<p>DTS/DPF 1.1</p> <p>Tree planting is provided in accordance with the following:</p> <table border="1" data-bbox="833 1451 1520 1888"> <thead> <tr> <th>Site size per dwelling (m²)</th> <th>Tree size* and number required per dwelling</th> </tr> </thead> <tbody> <tr> <td><450</td> <td>1 small tree</td> </tr> <tr> <td>450-800</td> <td>1 medium tree or 2 small trees</td> </tr> <tr> <td>>800</td> <td>1 large tree or 2 medium trees or 4 small trees</td> </tr> </tbody> </table> <p>*refer Table 1 Tree Size</p> <table border="1" data-bbox="833 1973 1520 2112"> <thead> <tr> <th colspan="4">Table 1 Tree Size</th> </tr> <tr> <th>Tree size</th> <th>Mature</th> <th>Mature</th> <th>Soil area around tree</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	Site size per dwelling (m ²)	Tree size* and number required per dwelling	<450	1 small tree	450-800	1 medium tree or 2 small trees	>800	1 large tree or 2 medium trees or 4 small trees	Table 1 Tree Size				Tree size	Mature	Mature	Soil area around tree				
Site size per dwelling (m ²)	Tree size* and number required per dwelling																				
<450	1 small tree																				
450-800	1 medium tree or 2 small trees																				
>800	1 large tree or 2 medium trees or 4 small trees																				
Table 1 Tree Size																					
Tree size	Mature	Mature	Soil area around tree																		

	height (minimum)	spread (minimum)	within development site (minimum)
Small	4 m	2m	10m ² and min. dimension of 1.5m
Medium	6 m	4 m	30m ² and min. dimension of 2m
Large	12 m	8m	60m ² and min. dimension of 4m

The discount in Column D of Table 2 discounts the number of trees required to be planted in DTS/DPF 1.1 where existing tree(s) are retained on the subject land that meet the criteria in Columns A, B and C of Table 2, and are not a species identified in Regulation 3F(4)(b) of the Planning Development and Infrastructure (General) Regulations 2017.

Table 2 Tree Discounts			
Retained tree height (Column A)	Retained tree spread (Column B)	Retained soil area around tree within development site (Column C)	Discount applied (Column D)
4-6m	2-4m	10m ² and min. dimension of 1.5m	2 small trees (or 1 medium tree)
6-12m	4-8m	30m ² and min. dimension of 3m	2 medium trees (or 4 small trees)
>12m	>8m	60m ² and min. dimension of 6m	2 large trees (or 4 medium trees, or 8 small trees)

Note: In order to satisfy DTS/DPF 1.1, payment may be made in accordance with a relevant off-set scheme established by the Minister under section 197 of the Planning, Development and Infrastructure Act 2016, provided the provisions and requirements of that scheme are satisfied. For the purposes of section 102(4) of the Planning, Development and Infrastructure Act 2016, an applicant may elect for any of the matters in DTS/DPF 1.1 to be reserved.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Part 4 - General Development Policies**Clearance from Overhead Powerlines****Assessment Provisions (AP)**

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.	DTS/DPF 1.1 One of the following is satisfied: (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i> (b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.

Design in Urban Areas**Assessment Provisions (AP)**

Desired Outcome	
DO 1	Development is: (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping

to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
On-site Waste Treatment Systems	
<p>PO 6.1</p> <p>Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.</p>	<p>DTS/DPF 6.1</p> <p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> (a) encroach within an area used as private open space or result in less private open space than that specified in Design in Urban Areas Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
Car parking appearance	
<p>PO 7.1</p> <p>Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on streetscapes through techniques such as:</p> <ul style="list-style-type: none"> (a) limiting protrusion above finished ground level (b) screening through appropriate planting, fencing and mounding (c) limiting the width of openings and integrating them into the building structure. 	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
Earthworks and sloping land	
<p>PO 8.1</p> <p>Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.</p>	<p>DTS/DPF 8.1</p> <p>Development does not involve any of the following:</p> <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
<p>PO 8.2</p> <p>Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.</p>	<p>DTS/DPF 8.2</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):</p> <ul style="list-style-type: none"> (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
<p>PO 8.3</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>

<ul style="list-style-type: none"> (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development (c) are designed to integrate with the natural topography of the land. 	
<p>PO 8.4</p> <p>Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.</p>	<p>DTS/DPF 8.4</p> <p>None are applicable.</p>
<p>PO 8.5</p> <p>Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.</p>	<p>DTS/DPF 8.5</p> <p>None are applicable.</p>
Overlooking / Visual Privacy (low rise buildings)	
<p>PO 10.1</p> <p>Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.</p>	<p>DTS/DPF 10.1</p> <p>Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone:</p> <ul style="list-style-type: none"> (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.
<p>PO 10.2</p> <p>Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.</p>	<p>DTS/DPF 10.2</p> <p>One of the following is satisfied:</p> <ul style="list-style-type: none"> (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases
All residential development	
Front elevations and passive surveillance	
<p>PO 17.1</p> <p>Dwellings incorporate windows facing primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 17.1</p> <p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> (a) includes at least one window facing the primary street

	<p>from a habitable room that has a minimum internal room dimension of 2.4m</p> <p>(b) has an aggregate window area of at least 2m² facing the primary street.</p>
PO 17.2 Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.	DTS/DPF 17.2 Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.
Outlook and Amenity	
PO 18.1 Living rooms have an external outlook to provide a high standard of amenity for occupants.	DTS/DPF 18.1 A living room of a dwelling incorporates a window with an external outlook of the street frontage, private open space, public open space, or waterfront areas.
Residential Development - Low Rise	
External appearance	
PO 20.2 Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and the appearance of common driveway areas.	DTS/DPF 20.2 Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway: <ul style="list-style-type: none"> (a) a minimum of 30% of the building wall is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building wall (c) a balcony projects from the building wall (d) a verandah projects at least 1m from the building wall (e) eaves of a minimum 400mm width extend along the width of the front elevation (f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm (g) a minimum of two different materials or finishes are incorporated on the walls of the front building elevation, with a maximum of 80% of the building elevation in a single material or finish.
PO 20.3 The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.	DTS/DPF 20.3 None are applicable
Private Open Space	
PO 21.1 Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 21.1 Private open space is provided in accordance with Design in Urban Areas Table 1 - Private Open Space.

<p>PO 21.2</p> <p>Private open space is positioned to provide convenient access from internal living areas.</p>	<p>DTS/DPF 21.2</p> <p>Private open space is directly accessible from a habitable room.</p>										
Landscaping											
<p>PO 22.1</p> <p>Soft landscaping is incorporated into development to:</p> <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) contribute shade and shelter (c) provide for stormwater infiltration and biodiversity (d) enhance the appearance of land and streetscapes. 	<p>DTS/DPF 22.1</p> <p>Residential development incorporates soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b):</p> <ul style="list-style-type: none"> (a) a total area as determined by the following table: <table border="1" data-bbox="922 607 1522 1061"> <thead> <tr> <th style="background-color: #1a3d54; color: white;">Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="background-color: #1a3d54; color: white;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td><150</td> <td>10%</td> </tr> <tr> <td>150-200</td> <td>15%</td> </tr> <tr> <td>>200-450</td> <td>20%</td> </tr> <tr> <td>>450</td> <td>25%</td> </tr> </tbody> </table> (b) at least 30% of any land between the primary street boundary and the primary building line. 	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
>200-450	20%										
>450	25%										
Car parking, access and manoeuvrability											
<p>PO 23.1</p> <p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single width car parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m per space (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double width car parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.4m (iii) minimum garage door width of 2.4m per space. 										
<p>PO 23.2</p> <p>Uncovered car parking space are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m. 										
<p>PO 23.3</p>	<p>DTS/DPF 23.3</p>										

<p>Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, domestic waste collection, landscaped street frontages and on-street parking.</p>	<p>Driveways and access points satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) sites with a frontage to a public road of 10m or less, have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site (b) sites with a frontage to a public road greater than 10m: <ul style="list-style-type: none"> (i) have a maximum width of 5m measured at the property boundary and are the only access point provided on the site; (ii) have a width between 3.0 metres and 3.2 metres measured at the property boundary and no more than two access points are provided on site, separated by no less than 1m.
<p>PO 23.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 23.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: <ul style="list-style-type: none"> (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
<p>PO 23.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 23.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient from the place of access on the boundary of the allotment to the finished floor level at the front of the garage or carport is not steeper than 1-in-4 on average (b) they are aligned relative to the street so that there is no more than a 20 degree deviation from 90 degrees between the centreline of any dedicated car parking space to which it provides access (measured from the front of that space) and the road boundary. (c) if located so as to provide access from an alley, lane or right of way - the alley, lane or right of way is at least 6.2m wide along the boundary of the allotment / site
<p>PO 23.6</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 23.6</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> (a) minimum 0.33 on-street spaces per dwelling on the site

	<p>(rounded up to the nearest whole number)</p> <p>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</p> <p>(c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.</p>
Waste storage	
<p>PO 24.1</p> <p>Provision is made for the convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 24.1</p> <p>Where dwellings abut both side boundaries a waste bin storage area is provided behind the building line of each dwelling that:</p> <p>(a) has a minimum area of 2m² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space); and</p> <p>(b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.</p>
Design of Transportable Buildings	
<p>PO 25.1</p> <p>The sub-floor space beneath transportable buildings is enclosed to give the appearance of a permanent structure.</p>	<p>DTS/DPF 25.1</p> <p>Buildings satisfy (a) or (b):</p> <p>(a) are not transportable</p> <p>(b) the sub-floor space between the building and ground level is clad in a material and finish consistent with the building.</p>
Group Dwellings, Residential Flat Buildings and Battle axe Development	
Amenity	
<p>PO 31.2</p> <p>The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.</p>	<p>DTS/DPF 31.2</p> <p>None are applicable.</p>
<p>PO 31.3</p> <p>Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.</p>	<p>DTS/DPF 31.3</p> <p>None are applicable.</p>
<p>PO 31.4</p> <p>Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.</p>	<p>DTS/DPF 31.4</p> <p>Dwelling sites/allotments are not in the form of a battle-axe arrangement.</p>
Car parking, access and manoeuvrability	
<p>PO 33.1</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 33.1</p> <p>Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements:</p> <p>(a) minimum 0.33 on-street car parks per proposed dwelling (rounded up to the nearest whole number)</p> <p>(b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly</p> <p>(c) minimum carpark length of 6m for an intermediate</p>

	space located between two other parking spaces or to an end obstruction where the parking is indented.
PO 33.4 Residential driveways that service more than one dwelling or a dwelling on a battle-axe site are designed to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 33.4 Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.
PO 33.5 Dwellings are adequately separated from common driveways and manoeuvring areas.	DTS/DPF 33.5 Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.
Soft landscaping	
PO 34.2 Battle-axe or common driveways incorporate landscaping and permeability to improve appearance and assist in stormwater management.	DTS/DPF 34.2 Battle-axe or common driveways satisfy (a) and (b): (a) are constructed of a minimum of 50% permeable or porous material (b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).
Laneway Development	
Infrastructure and Access	
PO 44.1 Development with a primary street comprising a laneway, alley, lane, right of way or similar minor thoroughfare only occurs where: (a) existing utility infrastructure and services are capable of accommodating the development (b) the primary street can support access by emergency and regular service vehicles (such as waste collection) (c) it does not require the provision or upgrading of infrastructure on public land (such as footpaths and stormwater management systems) (d) safety of pedestrians or vehicle movement is maintained (e) any necessary grade transition is accommodated within the site of the development to support an appropriate development intensity and orderly development of land fronting minor thoroughfares.	DTS/DPF 44.1 Development with a primary street frontage that is not an alley, lane, right of way or similar public thoroughfare.

Table 1 - Private Open Space

Dwelling Type	Dwelling / Site Configuration	Minimum Rate
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Dwelling (at ground level, other than a residential flat building that includes above ground dwellings)		<p>Total private open space area:</p> <p>(a) Site area <301m²: 24m² located behind the building line.</p> <p>(b) Site area ≥ 301m²: 60m² located behind the building line.</p> <p>Minimum directly accessible from a living room: 16m² / with a minimum dimension 3m.</p>
Cabin or caravan (permanently fixed to the ground) in a residential park or caravan and tourist park		Total area: 16m ² , which may be uses as second car parking space, provided on each site intended for residential occupation.
Dwelling in a residential flat building or mixed use building which incorporate above ground level dwellings	Dwellings at ground level:	15m ² / minimum dimension 3m
	Dwellings above ground level:	
	Studio (no separate bedroom)	4m ² / minimum dimension 1.8m
	One bedroom dwelling	8m ² / minimum dimension 2.1m
	Two bedroom dwelling	11m ² / minimum dimension 2.4m
	Three + bedroom dwelling	15 m ² / minimum dimension 2.6m

Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Overshadowing	
PO 3.1 Overshadowing of habitable room windows of adjacent residential land uses in: <ol style="list-style-type: none"> a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight. 	DTS/DPF 3.1 North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.
PO 3.2 Overshadowing of the primary area of private open space or	DTS/DPF 3.2 Development maintains 2 hours of direct sunlight between 9.00

<p>communal open space of adjacent residential land uses in:</p> <p>a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following:</p> <p>a. for ground level private open space, the smaller of the following:</p> <p>i. half the existing ground level open space</p> <p>or</p> <p>ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m)</p> <p>b. for ground level communal open space, at least half of the existing ground level open space.</p>
<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <p>(a) the form of development contemplated in the zone</p> <p>(b) the orientation of the solar energy facilities</p> <p>(c) the extent to which the solar energy facilities are already overshadowed.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>

Activities Generating Noise or Vibration
--

<p>PO 4.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.3</p> <p>The pump and/or filtration system ancillary to a dwelling erected on the same site is:</p> <ul style="list-style-type: none"> (a) enclosed in a solid acoustic structure located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.
--	---

	<p style="text-align: center;">or</p> <p>B. the land is suitable for the proposed use or range of uses (without the need for any further remediation)</p> <p style="text-align: center;">or</p> <p>C. where remediation is, or remains, necessary for the proposed use (or range of uses), remediation work has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development)</p> <p style="text-align: center;">and</p> <p>(ii) no other class 1 activity or class 2 activity has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a site contamination declaration form).</p>
--	---

Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Vehicle Parking Rates	
<p>PO 5.1</p> <p>Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:</p> <ul style="list-style-type: none"> (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place. 	<p>DTS/DPF 5.1</p> <p>Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:</p> <ul style="list-style-type: none"> (a) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements (b) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.
Corner Cut-Offs	
PO 10.1	DTS/DPF 10.1

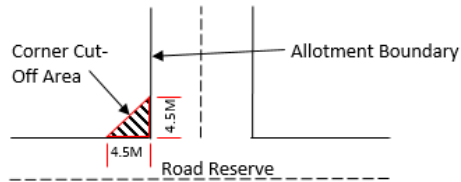
<p>Development is located and designed to ensure drivers can safely turn into and out of public road junctions.</p>	<p>Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:</p> 
---	--

Table 1 - General Off-Street Car Parking Requirements

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate (unless varied by Table 2 onwards)
<p>Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.</p>	
<p>Residential Development</p>	
<p>Detached Dwelling</p>	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>



DRAWING LIST

Sheet #	Sheet Name	Current Revision	Current Revision Date	Current Revision Description
A000	COVER PAGE	1	31/07/2023	Colours amended
A011	PROPOSED SITE PLAN	0	31/05/2023	Issued for Planning Consent
A100	GROUND AND FIRST FLOOR PLANS	0	31/05/2023	Issued for Planning Consent
A120	ROOF DECK FLOOR PLAN	0	31/05/2023	Issued for Planning Consent
A200	ELEVATIONS	1	31/07/2023	Colours amended
A201	ELEVATIONS	1	31/07/2023	Colours amended
A700	3D VIEWS 1	1	31/07/2023	Colours amended
A701	3D VIEWS 2	1	31/07/2023	Colours amended
A702	SHADOW DIAGRAMS - SUMMER SOLSTICE	0	31/05/2023	Issued for Planning Consent
A703	SHADOW DIAGRAMS - WINTER SOLSTICE	0	31/05/2023	Issued for Planning Consent
A704	SIGHTLINES 3D VIEWS	0	31/05/2023	Issued for Planning Consent
A705	SIGHTLINES SECTIONS	0	31/05/2023	Issued for Planning Consent

ISSUED FOR PLANNING CONSENT



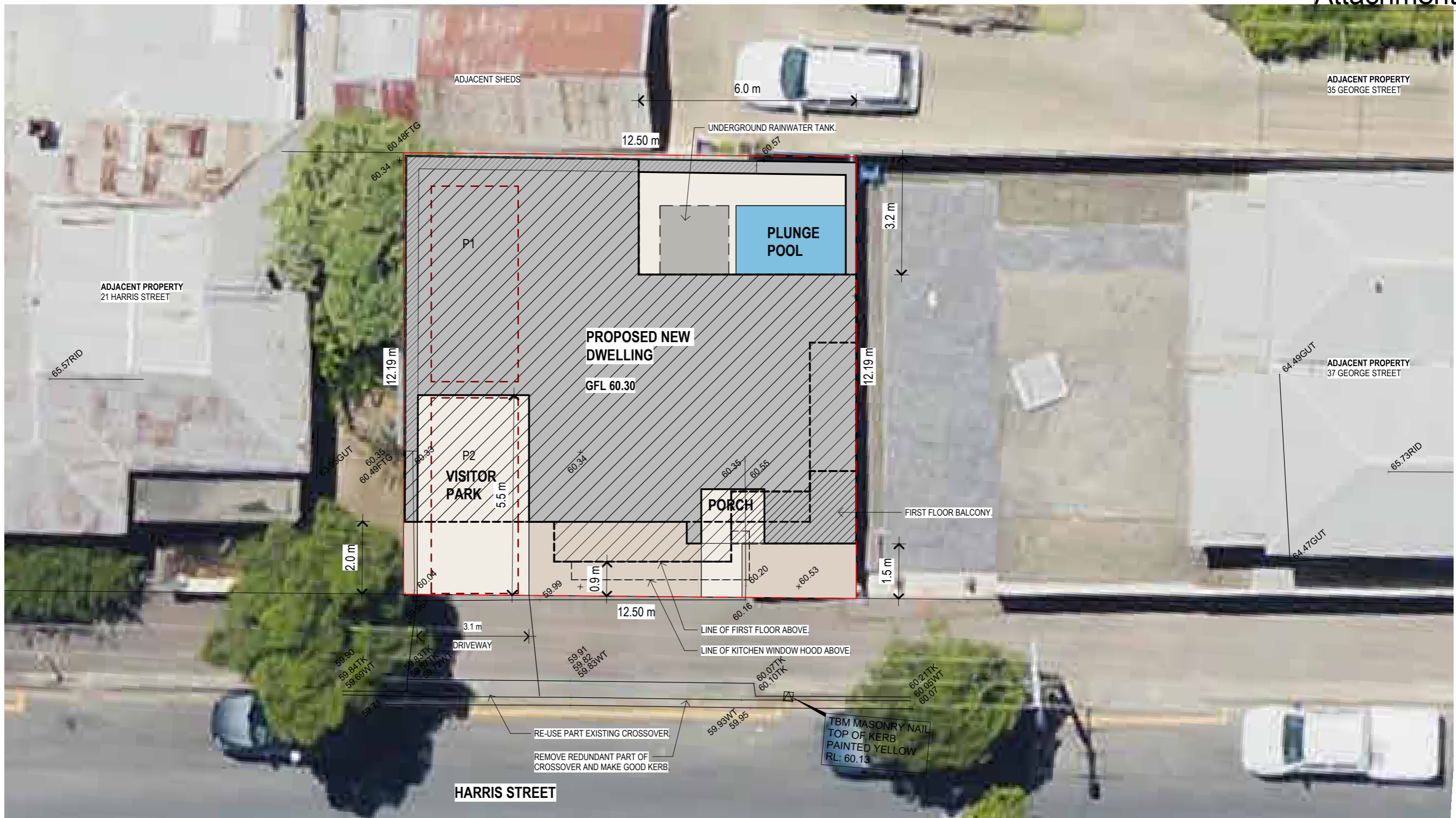
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No.	Date	Amendment
1	31/07/2023	Colours amended
0	31/05/2023	Issued for Planning Consent

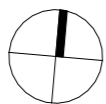
Project
NEW RESIDENCE
For
STEVE & ALISON BROOKMAN
At
23 HARRIS ST, NORWOOD SA

Drawing Title:
COVER PAGE

Drawn: AJD © Copyright Drawing Scale:
Project Number: Issue Date: Sheet Number: Revision: Sheet Size:
2307 31/07/2023 A000 1 A3



1 SITE PLAN
A200 1:100

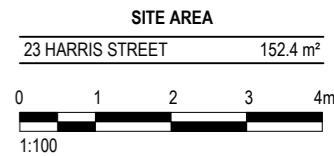


STORMWATER

IN ACCORDANCE WITH THE NATIONAL CONSTRUCTION CODE AND THE SA PLANNING AND DESIGN CODE DIRECT MINIMUM 60% OF ROOF RUN OFF TO A MINIMUM 2000 LITRE RAINWATER TANK WHERE:

- MINIMUM 1000 LITRES SHALL BE DETAINED AND RELEASED THROUGH A 20-25mm OUTLET AND MINIMUM 1000 LITRES RETAINED AND CONNECTED TO EITHER A TOILET, LAUNDRY COLD WATER OUTLETS OR HOT WATER SERVICE.

ADDITIONAL RUN OFF, OVERFLOW AND DETAINED WATER SHALL BE DIRECTED TO STREET WATER TABLE IN ACCORDANCE WITH COUNCIL REQUIREMENTS. ALL WORKS BEYOND THE PROPERTY BOUNDARY ARE TO BE IN ACCORDANCE WITH COUNCIL REQUIREMENTS. DIRECT PAVING RUNOFF TO LANDSCAPED AREAS.



A.D. DESIGN
building design & drafting
E admin@adesign.com.au
T 0410 818 518
ABN 40 660 247 043

ISSUED FOR PLANNING CONSENT

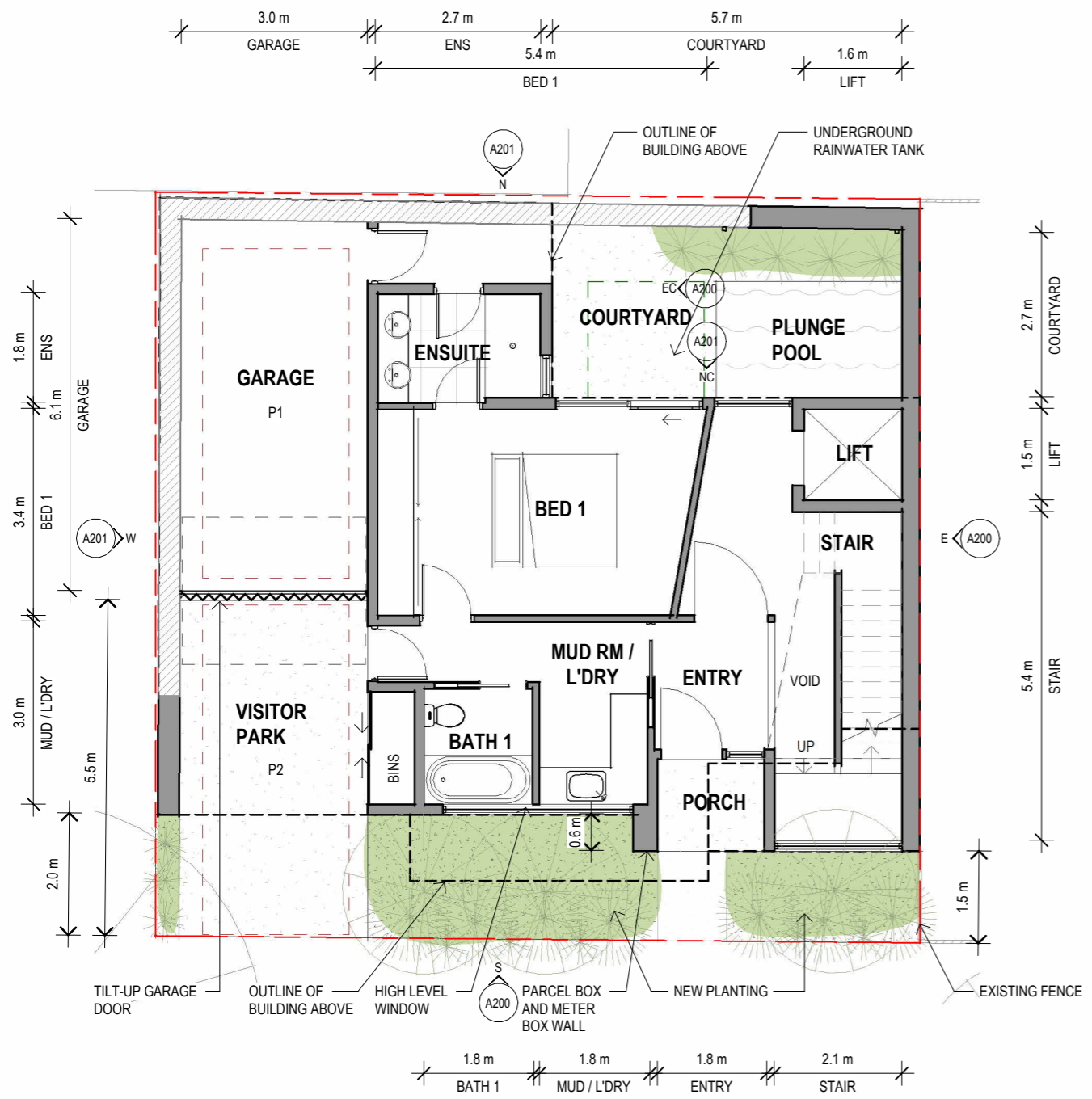
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No.	Date	Amendment
0	31/05/2023	Issued for Planning Consent

Project
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At
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Drawing Title:
PROPOSED SITE PLAN

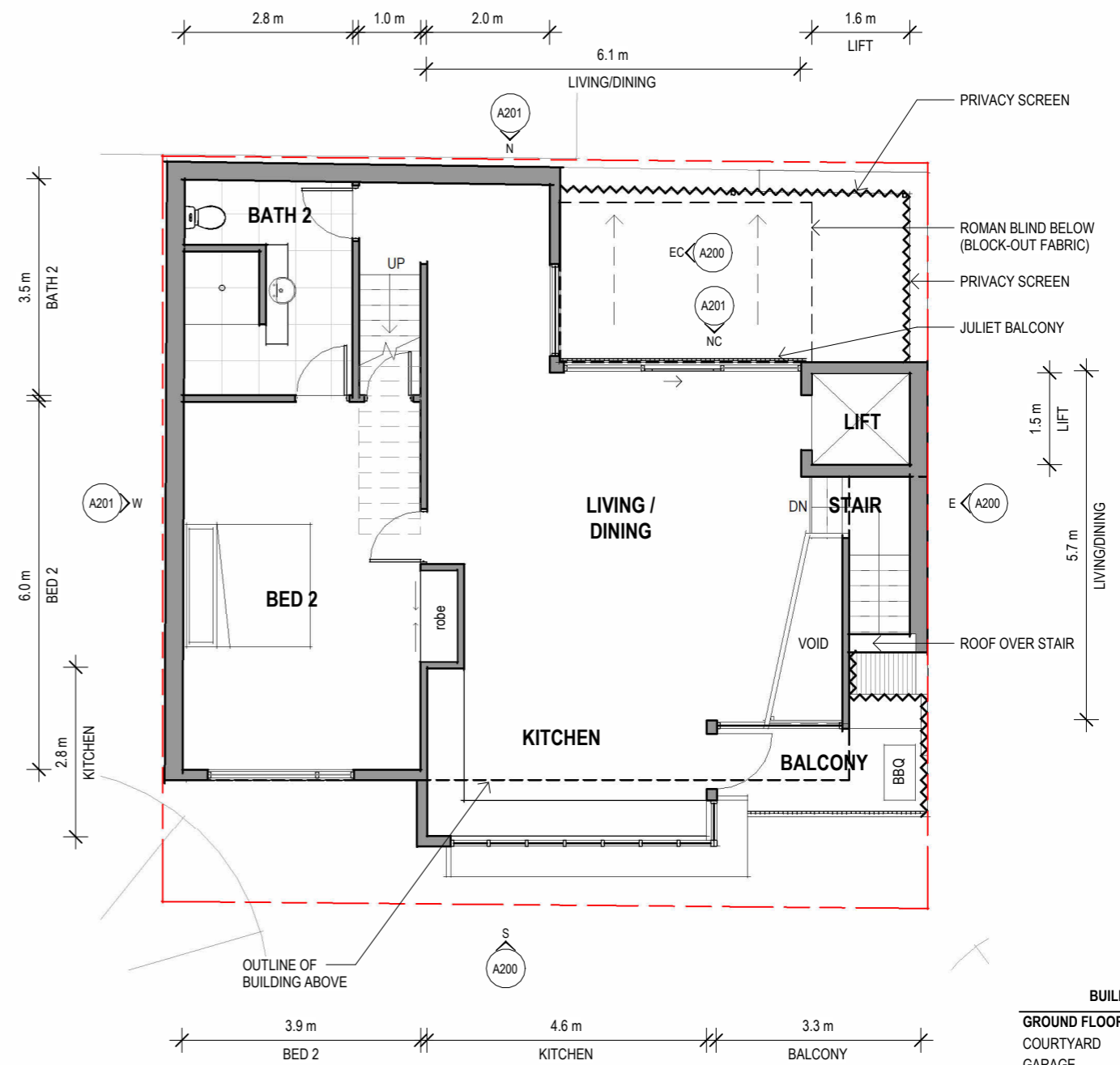
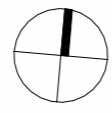
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Project Number: 2307	Issue Date: 31/05/2023	Sheet Number: A011
		Revision: 0
		Sheet Size: A3



1 GROUND FLOOR PLAN
A200 1:100

WALL LEGEND

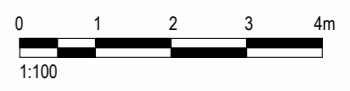
	EXISTING WALLS
	NEW WALLS



2 FIRST FLOOR PLAN
A200 1:100

BUILDING AREAS

GROUND FLOOR LEVEL	
COURTYARD	22.7 m ²
GARAGE	23.9 m ²
LIVING	67.5 m ²
PORCH	2.6 m ²
TOTAL	116.7 m²
FIRST FLOOR LEVEL	
BALCONY	5.7 m ²
LIVING	103.0 m ²
TOTAL	108.7 m²
ROOF DECK LEVEL	
ROOF DECK	55.7 m ²
ROOF GARDEN	32.8 m ²
STAIR/LIFT	10.9 m ²
TOTAL	99.4 m²
TOTAL	324.8 m²



A.D. DESIGN
building design & drafting
E admin@addesign.com.au
T 0410 818 518
ABN 40 660 247 043

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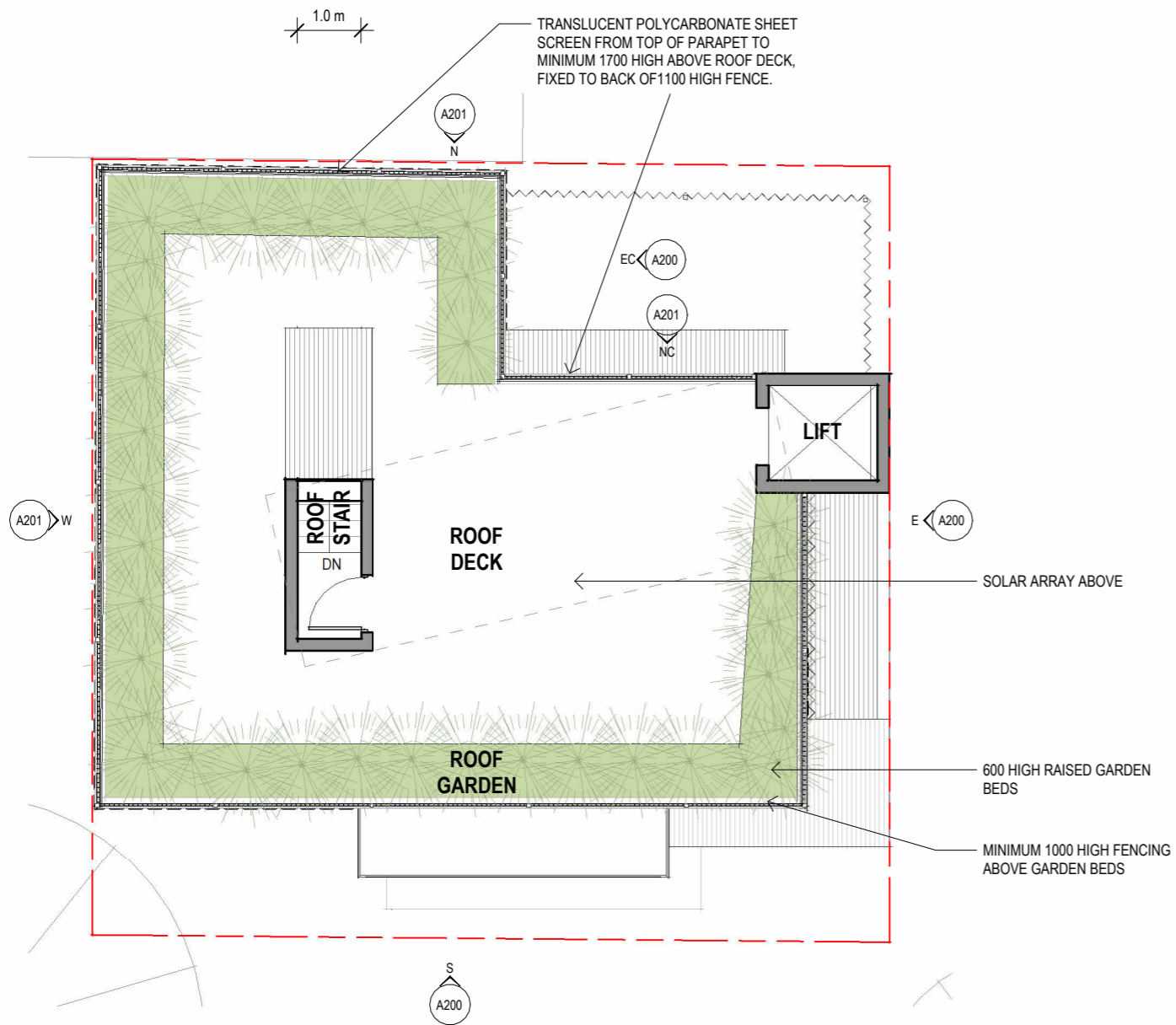
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Drawing Title:
GROUND AND FIRST FLOOR PLANS

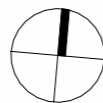
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Revision: 0	Sheet Size: A3	



1 ROOF DECK FLOOR PLAN
A200 1 : 100

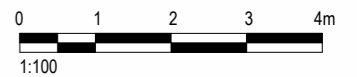
WALL LEGEND

	EXISTING WALLS
	NEW WALLS



BUILDING AREAS

GROUND FLOOR LEVEL	
COURTYARD	22.7 m ²
GARAGE	23.9 m ²
LIVING	67.5 m ²
PORCH	2.6 m ²
TOTAL	116.7 m²
FIRST FLOOR LEVEL	
BALCONY	5.7 m ²
LIVING	103.0 m ²
TOTAL	108.7 m²
ROOF DECK LEVEL	
ROOF DECK	55.7 m ²
ROOF GARDEN	32.8 m ²
STAIR/LIFT	10.9 m ²
TOTAL	99.4 m²
TOTAL	324.8 m²



A.D. DESIGN
building design & drafting
E admin@addesign.com.au
T 0410 818 518
ABN 40 660 247 043

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No.	Date	Amendment

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Drawing Title:
ROOF DECK FLOOR PLAN

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Project Number: 2307	Issue Date: 31/05/2023	Sheet Number: A120
Revision: 0	Sheet Size: A3	

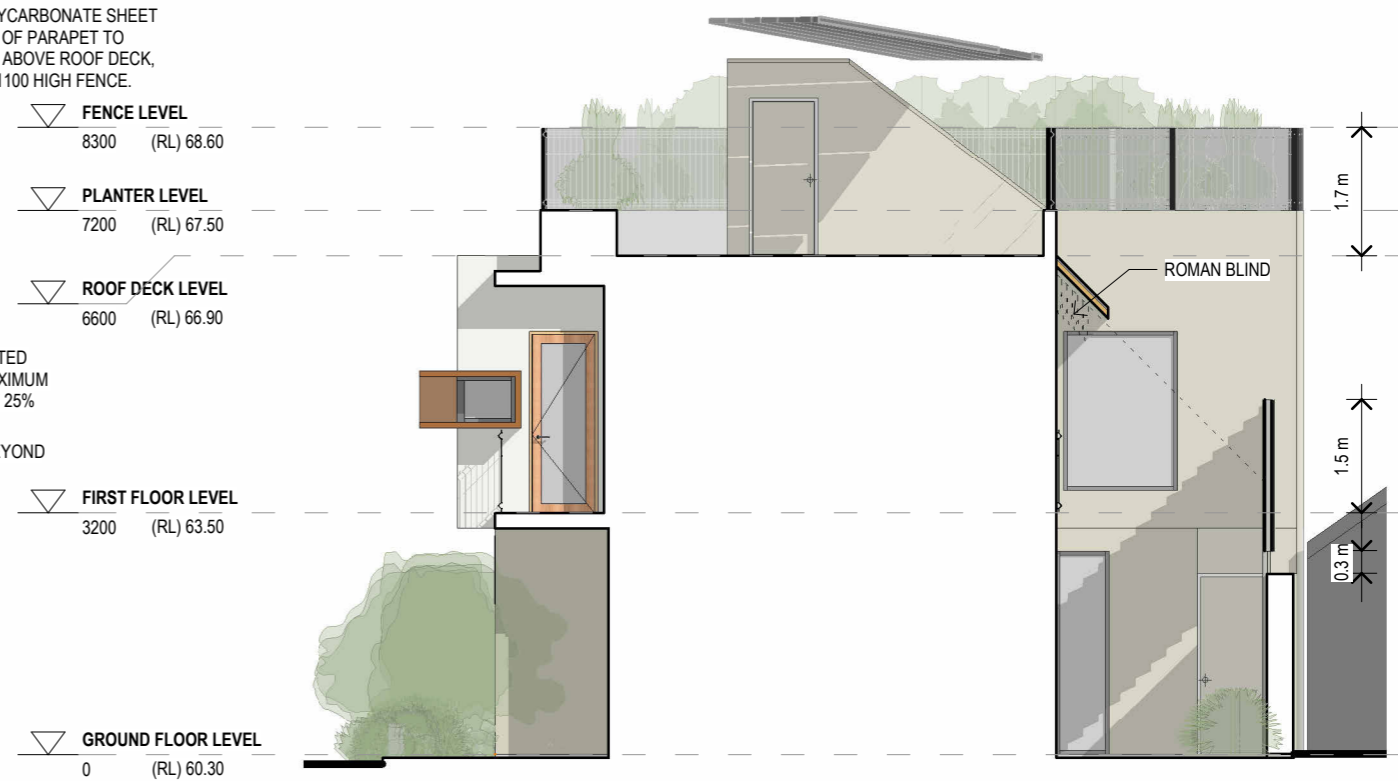
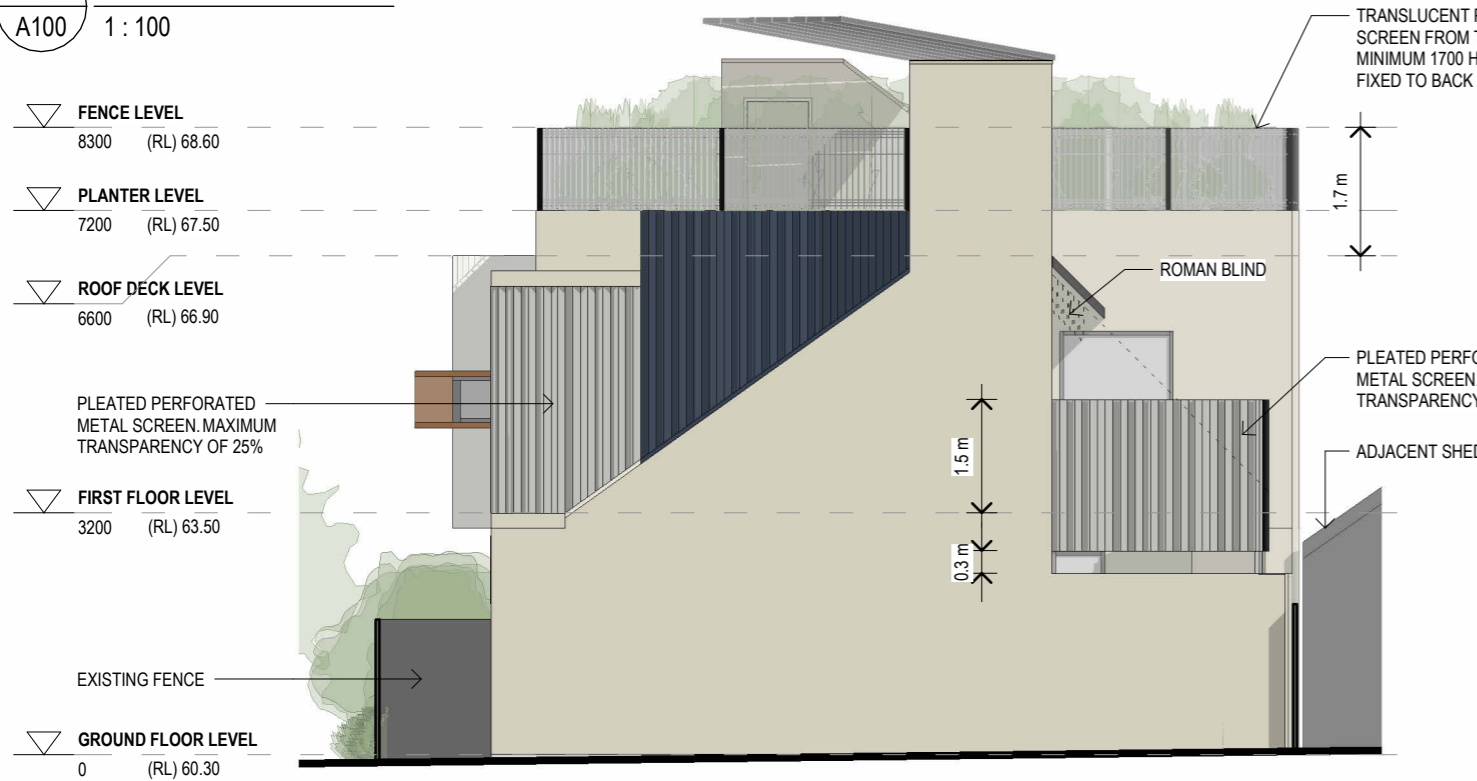


WALL AND ROOF AREAS

-  COLORBOND 'DEEP OCEAN' OR SIMILAR
-  COLORBOND 'SHALE GREY' OR SIMILAR
-  COLORBOND 'EVENING HAZE' OR SIMILAR
-  COLORBOND 'SURFMIST' OR SIMILAR
-  COR-TEN STEEL

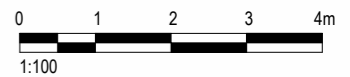
COLOUR PALETTE

S SOUTH ELEVATION
A100 1:100



E EAST ELEVATION
A100 1:100

EC EAST ELEVATION COURTYARD
A100 1:100




A.D. DESIGN
building design & drafting
E admin@addesign.com.au
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ABN 40 660 247 043

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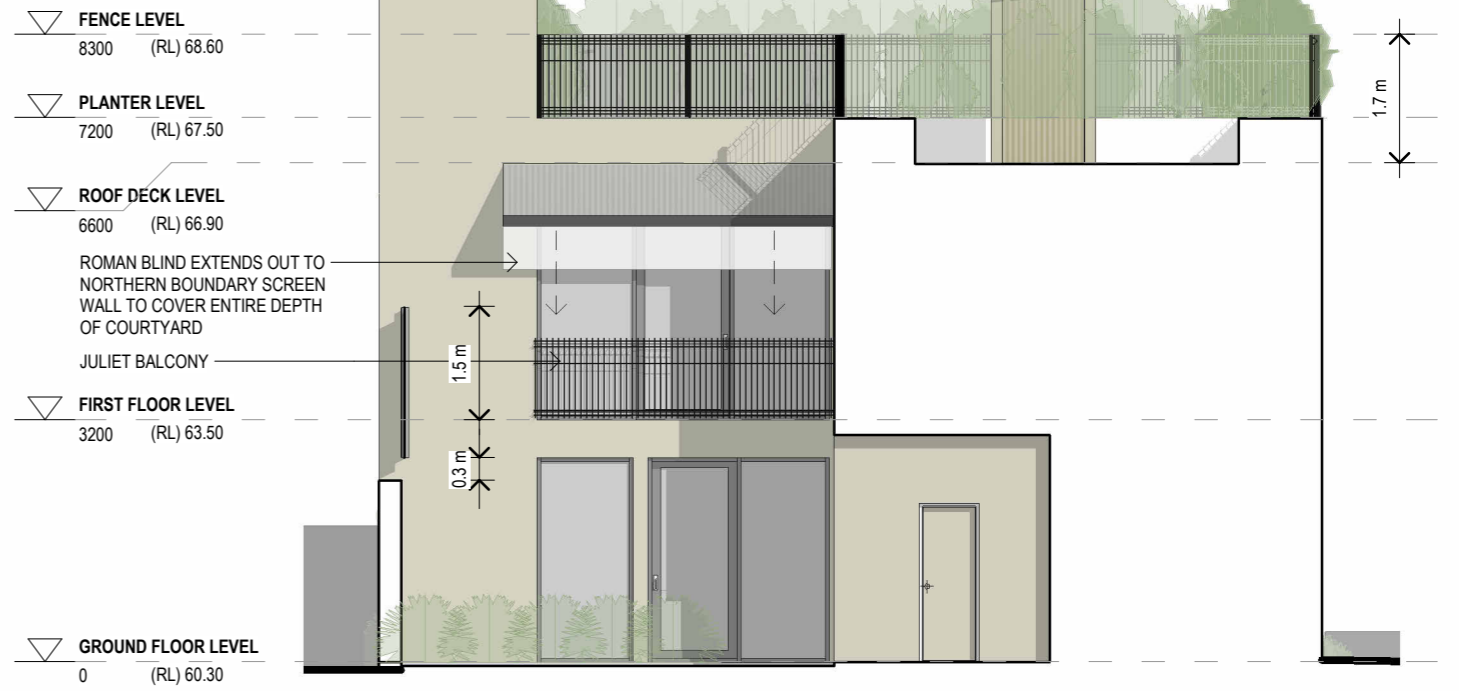
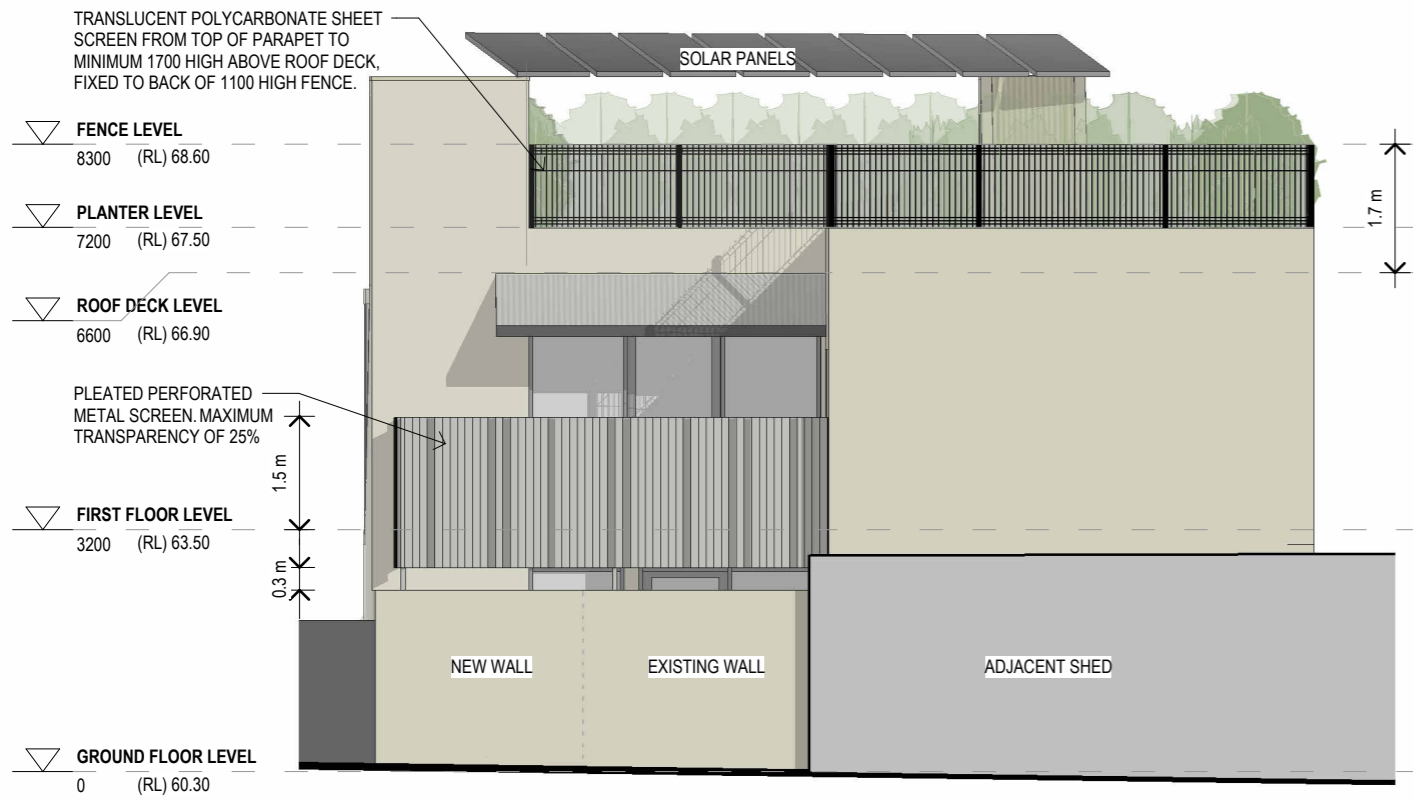
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1	31/07/2023	Colours amended
0	31/05/2023	Issued for Planning Consent

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Drawing Title:
ELEVATIONS

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Project Number: 2307	Issue Date: 31/07/2023	Sheet Number: A200
Revision: 1	Sheet Size: A3	



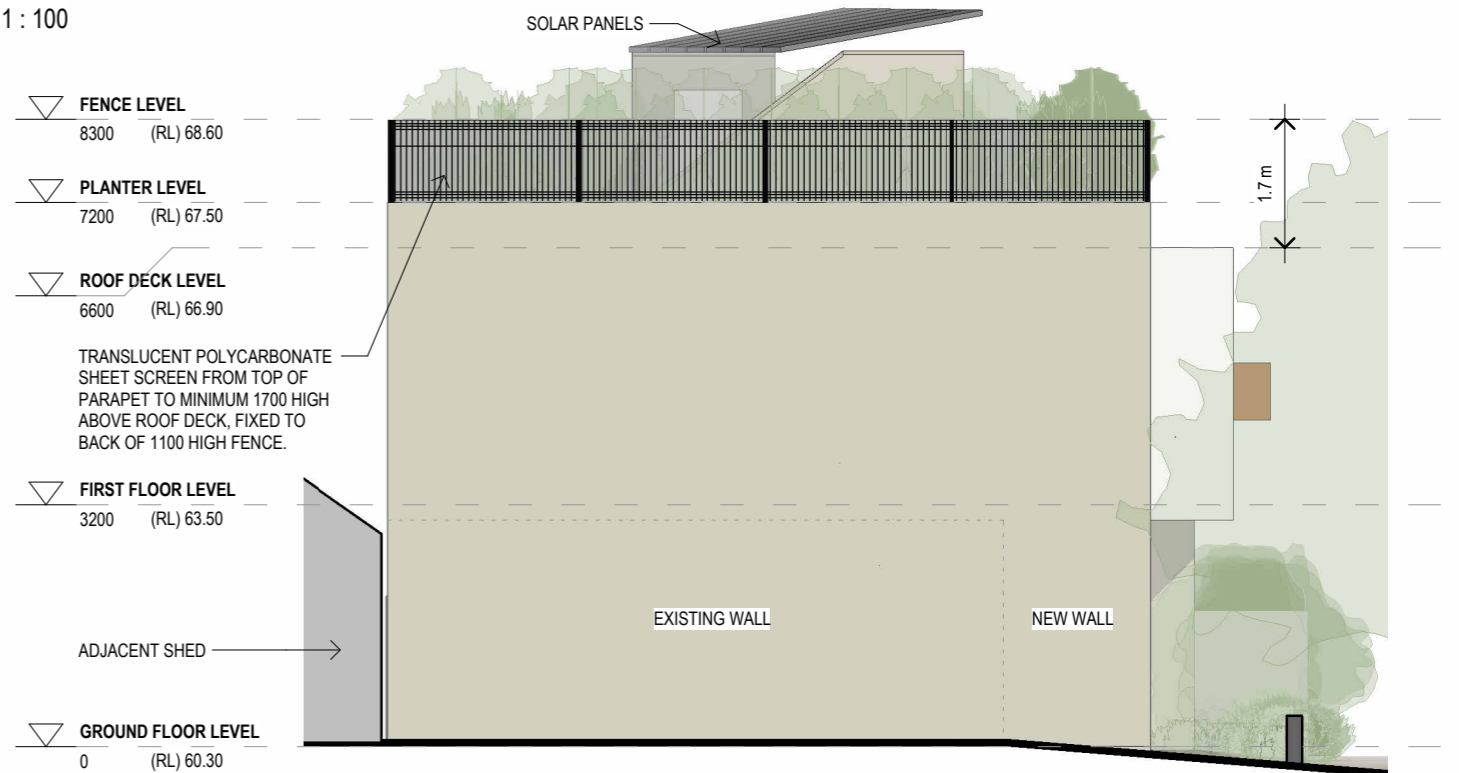
N
A100 **NORTH ELEVATION**
1 : 100

NC
A100 **NORTH ELEVATION COURTYARD**
1 : 100

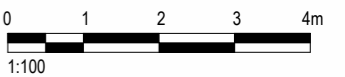
WALL AND ROOF AREAS

-  COLORBOND 'DEEP OCEAN' OR SIMILAR
-  COLORBOND 'SHALE GREY' OR SIMILAR
-  COLORBOND 'EVENING HAZE' OR SIMILAR
-  COLORBOND 'SURFMIST' OR SIMILAR
-  COR-TEN STEEL

COLOUR PALETTE



W
A100 **WEST ELEVATION**
1 : 100



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1	31/07/2023	Colours amended
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3D VIEW B



3D VIEW C



3D VIEW A



3D VIEW D

No.	Date	Amendment
1	31/07/2023	Colours amended
0	31/05/2023	Issued for Planning Consent



AERIAL VIEW 1



AERIAL VIEW 2



AERIAL VIEW 3

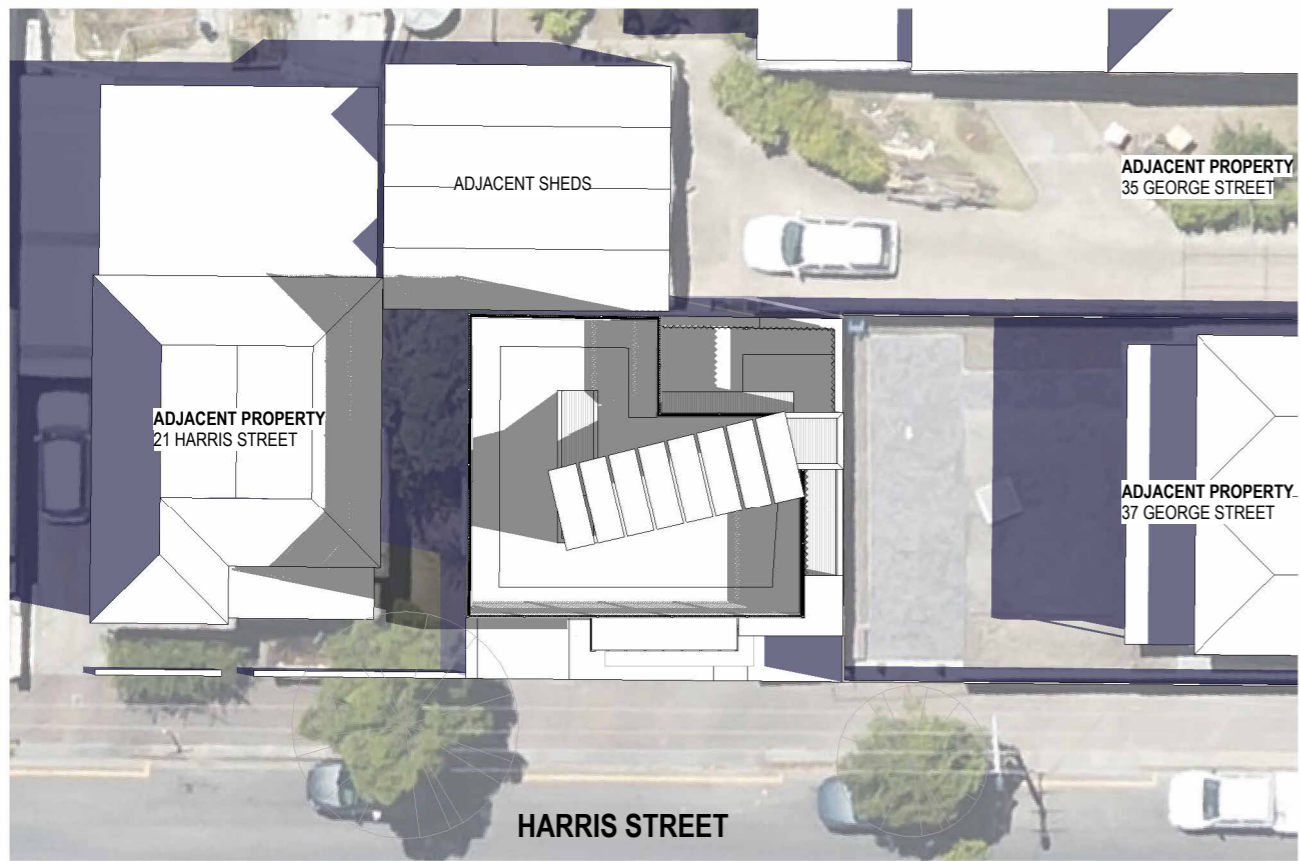


AERIAL VIEW 4



3D VIEW E

No.	Date	Amendment
1	31/07/2023	Colours amended
0	31/05/2023	Issued for Planning Consent



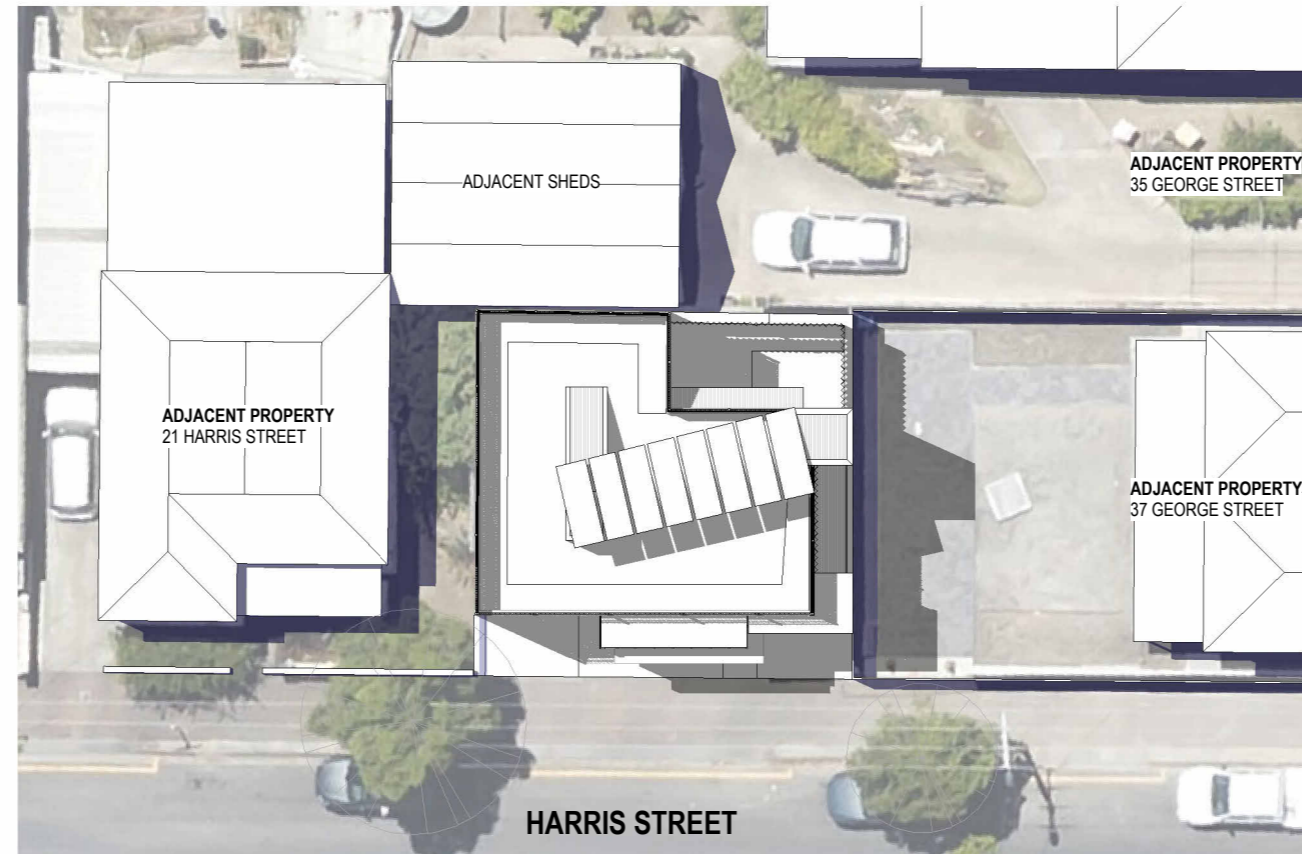
9.00am SUMMER SOLSTICE (22nd DEC)

1 : 250



12.00pm SUMMER SOLSTICE (22nd DEC)

1 : 250

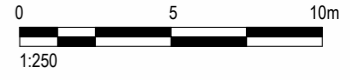
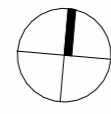


3.00pm SUMMER SOLSTICE (22nd DEC)

1 : 250

SHADOW LEGEND

- NEW BUILDING SHADOWS
- EXISTING SHADOWS



A.D. DESIGN
building design & drafting
E admin@adesign.com.au
T 0410 818 518
ABN 40 660 247 043

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No.	Date	Amendment
0	31/05/2023	Issued for Planning Consent

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At
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Drawing Title:
SHADOW DIAGRAMS - SUMMER SOLSTICE

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Project Number: 2307	Issue Date: 31/05/2023	Sheet Number: A702
	Revision: 0	Sheet Size: A3



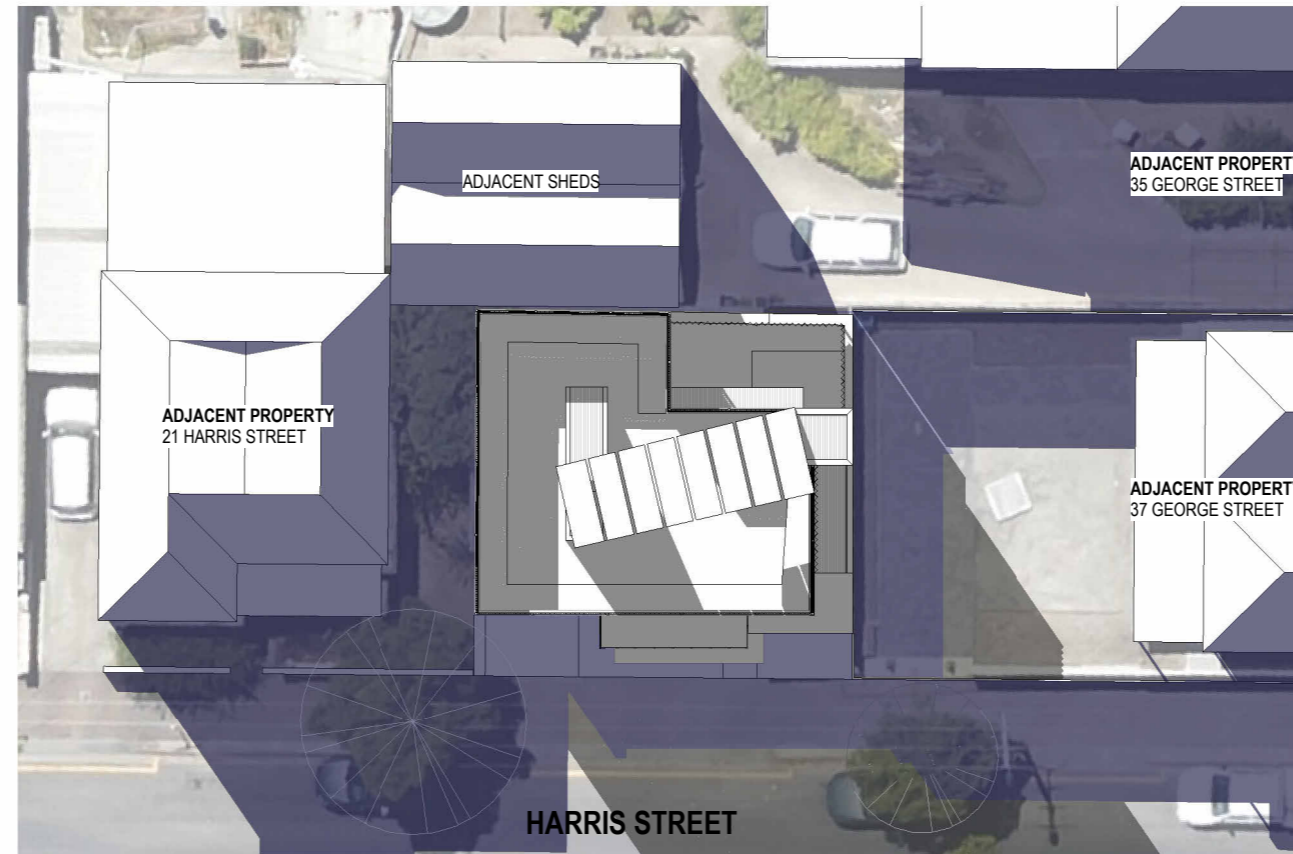
9.00am WINTER SOLSTICE (21st June)

1 : 250



12.00pm WINTER SOLSTICE (21st June)

1 : 250

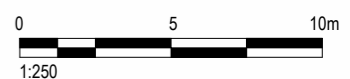
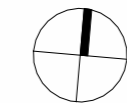


3.00pm WINTER SOLSTICE (21st June)

1 : 250

SHADOW LEGEND

- NEW BUILDING SHADOWS
- EXISTING SHADOWS



A.D. DESIGN
building design & drafting

E admin@adesign.com.au
T 0410 818 518
ABN 40 660 247 043

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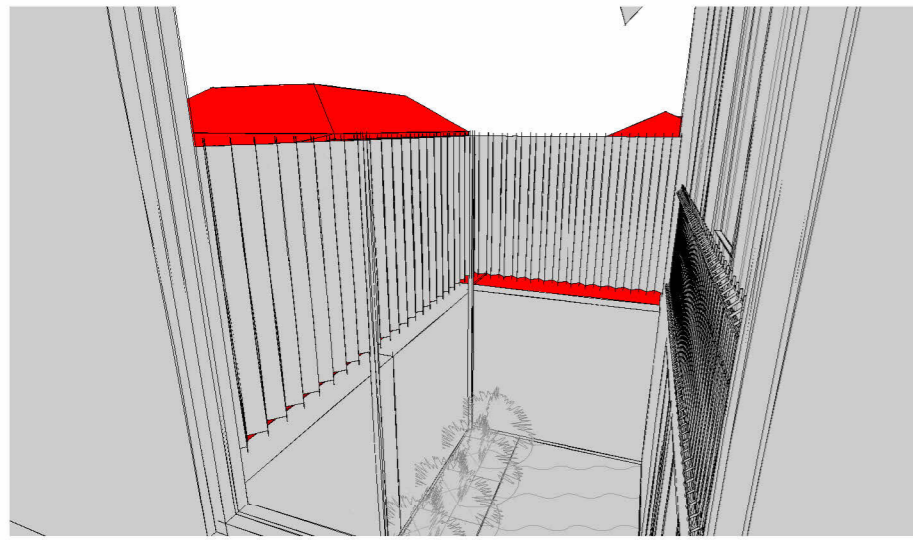
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No.	Date	Amendment
0	31/05/2023	Issued for Planning Consent

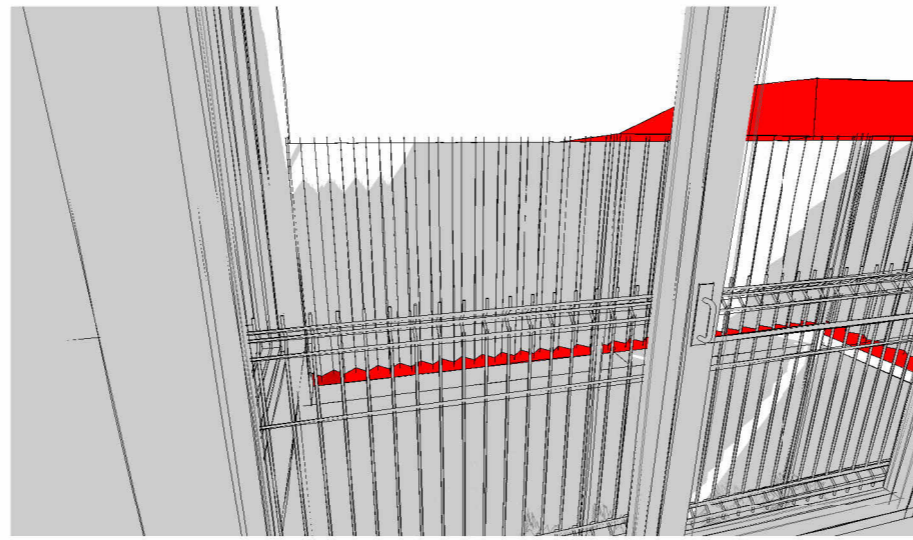
Project
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For
STEVE & ALISON BROOKMAN
At
23 HARRIS ST, NORWOOD SA

Drawing Title:
SHADOW DIAGRAMS - WINTER SOLSTICE

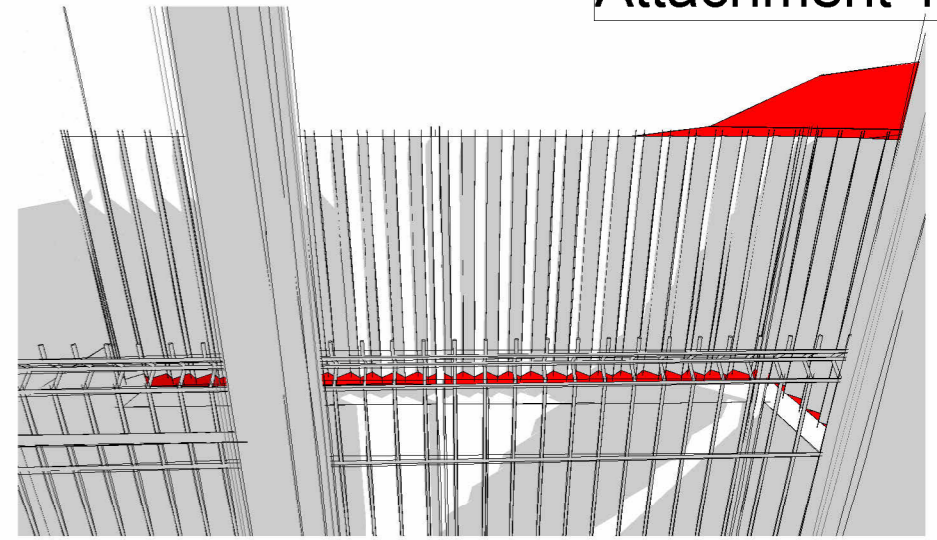
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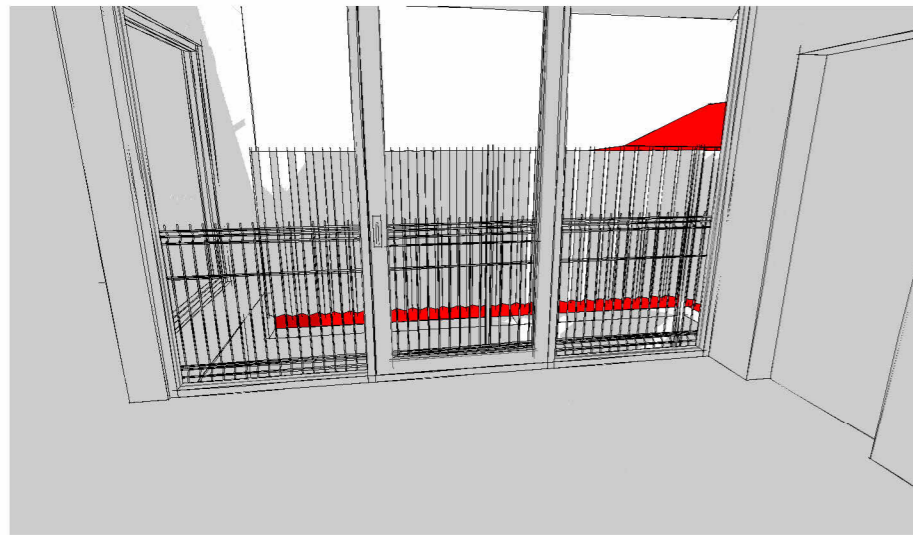
3 3D SIGHTLINE 1



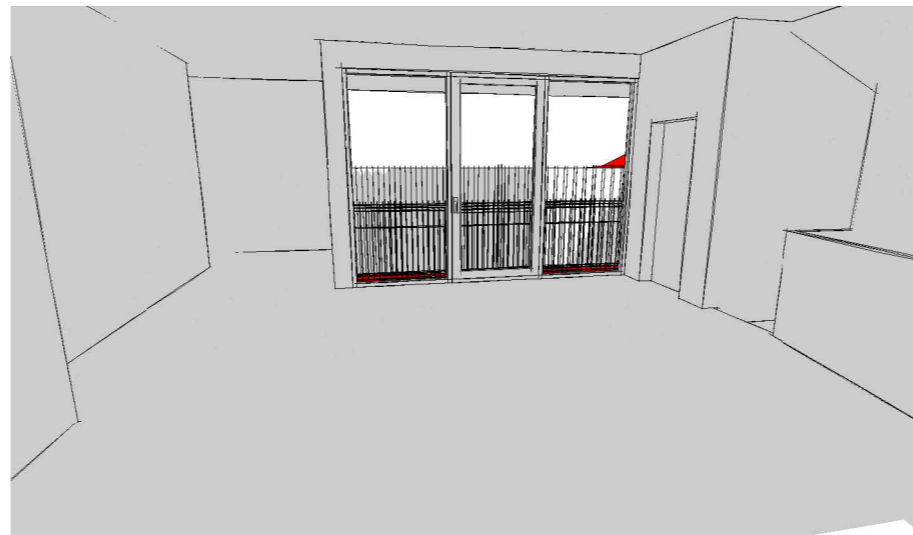
1 3D SIGHTLINE 2



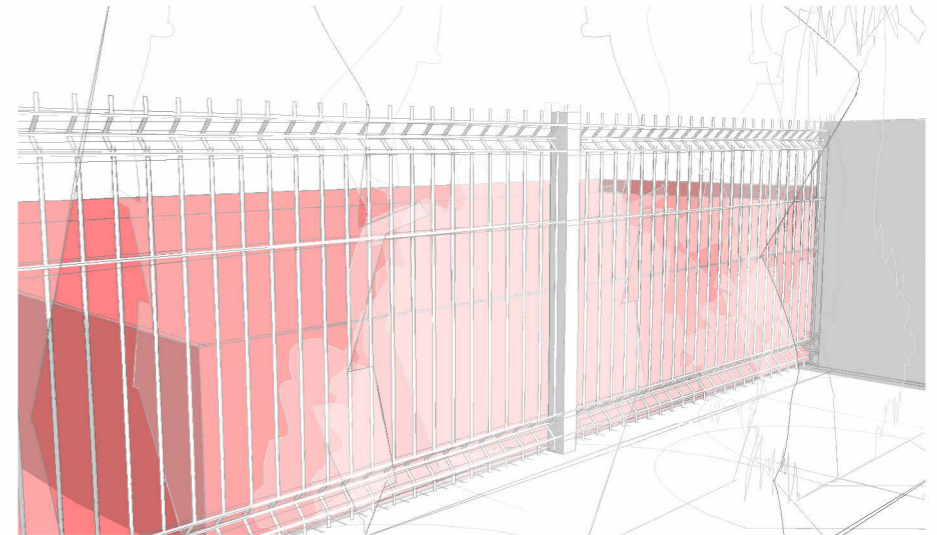
2 3D SIGHTLINE 3



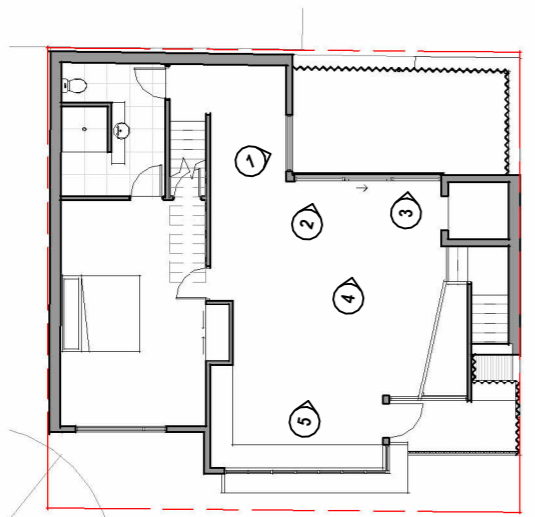
4 3D SIGHTLINE 4



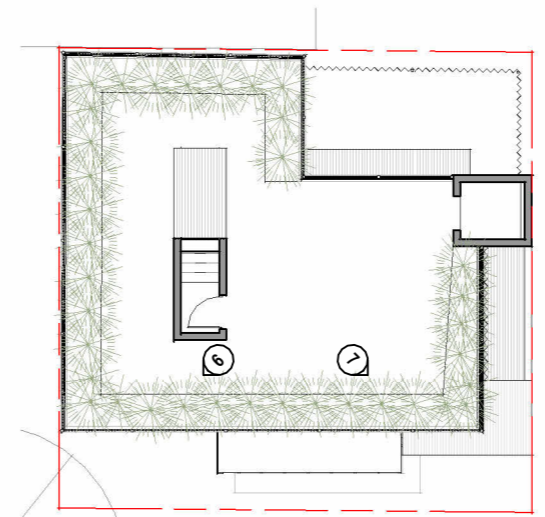
5 3D SIGHTLINE 5



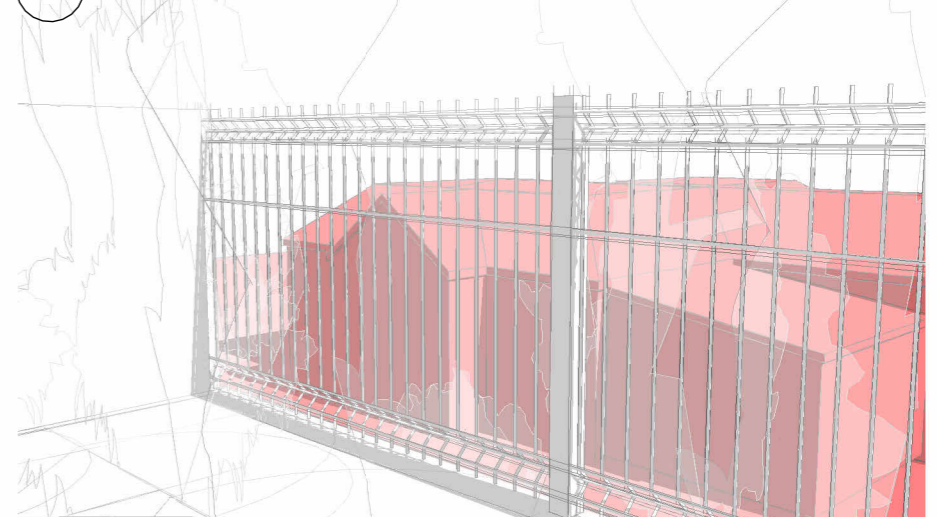
6 3D SIGHTLINE 6



FIRST FLOOR PLAN SIGHTLINES
1 : 200

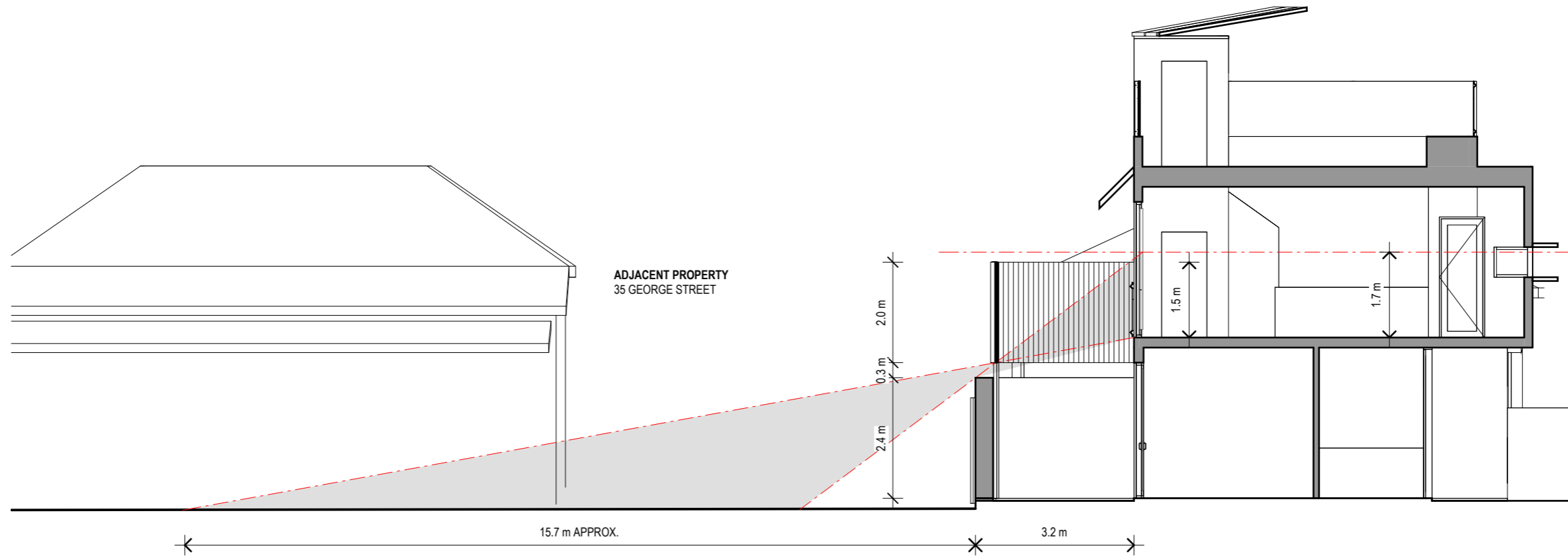


ROOF PLAN SIGHTLINES
1 : 200



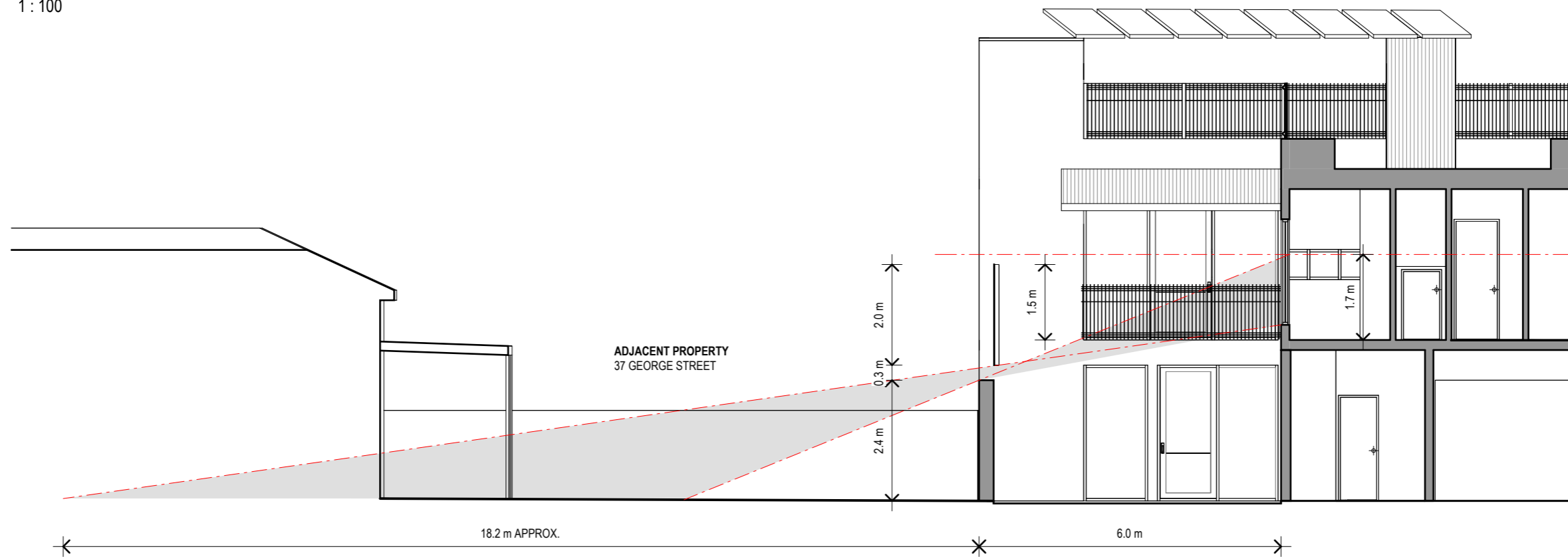
7 3D SIGHTLINE 7

NOTE: ALL VIEWS ARE TAKEN FROM AN EYE HEIGHT OF 1.7m ABOVE ASSOCIATED FLOOR LEVEL.



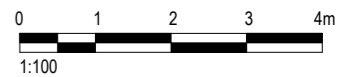
SIGHTLINES LOOKING NORTH FROM LIVING

1 : 100



SIGHTLINES LOOKING EAST FROM LIVING

1 : 100



A.D. DESIGN
building design & drafting
E admin@adesign.com.au
T 0410 818 518
ABN 40 660 247 043

ISSUED FOR PLANNING CONSENT

All dimensions and levels are approximate only & subject to site measure / final working drawings. These drawings are planning sketches only & are not to be used for construction purposes.

No.	Date	Amendment
0	31/05/2023	Issued for Planning Consent

Project
NEW RESIDENCE
For
STEVE & ALISON BROOKMAN
At
23 HARRIS ST, NORWOOD SA

Drawing Title:
SIGHTLINES SECTIONS

Drawn: AJD	© Copyright	Drawing Scale: 1 : 100
Project Number: 2307	Issue Date: 31/05/2023	Sheet Number: A705
	Revision: 0	Sheet Size: A3

SAPPA Report

The SA Property and Planning Atlas is available on the Plan SA website: <https://sappa.plan.sa.gov.au>

Subject Land Map

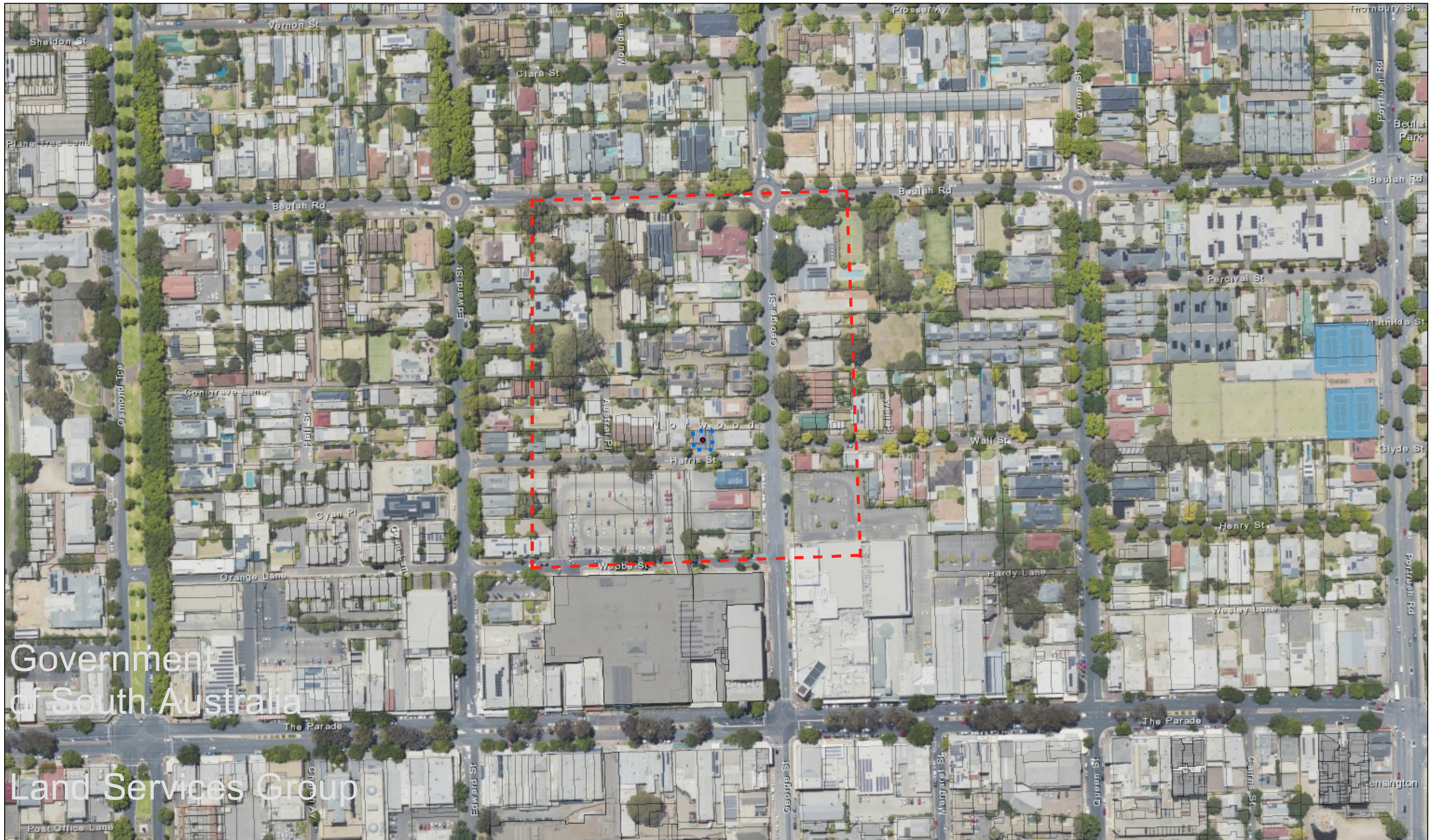


Government
of South Australia
Land Services Group

SAPPA Report

The SA Property and Planning Atlas is available on the Plan SA website: <https://sappa.plan.sa.gov.au>

Locality Map



Government
of South Australia

Land Services Group

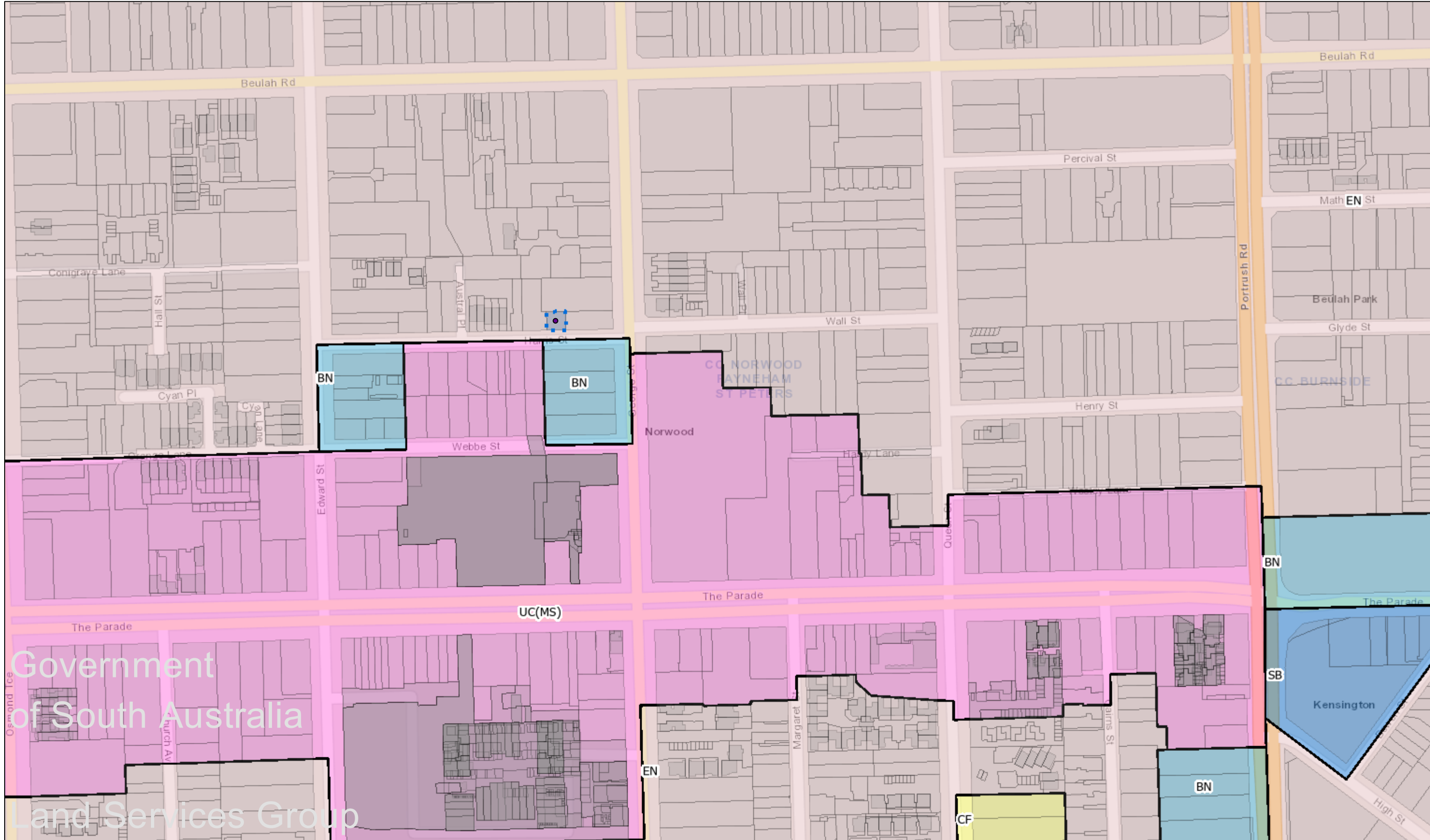
SAPPA Report

The SA Property and Planning Atlas is available on the Plan SA website: <https://sappa.plan.sa.gov.au>

Zoning Map

Zone Legend:

- EN Established Neighbourhood
- BN Business Neighbourhood
- UC(MS) Urban Corridor (Main Street)
- SB Suburban Business
- CF Community Facilities



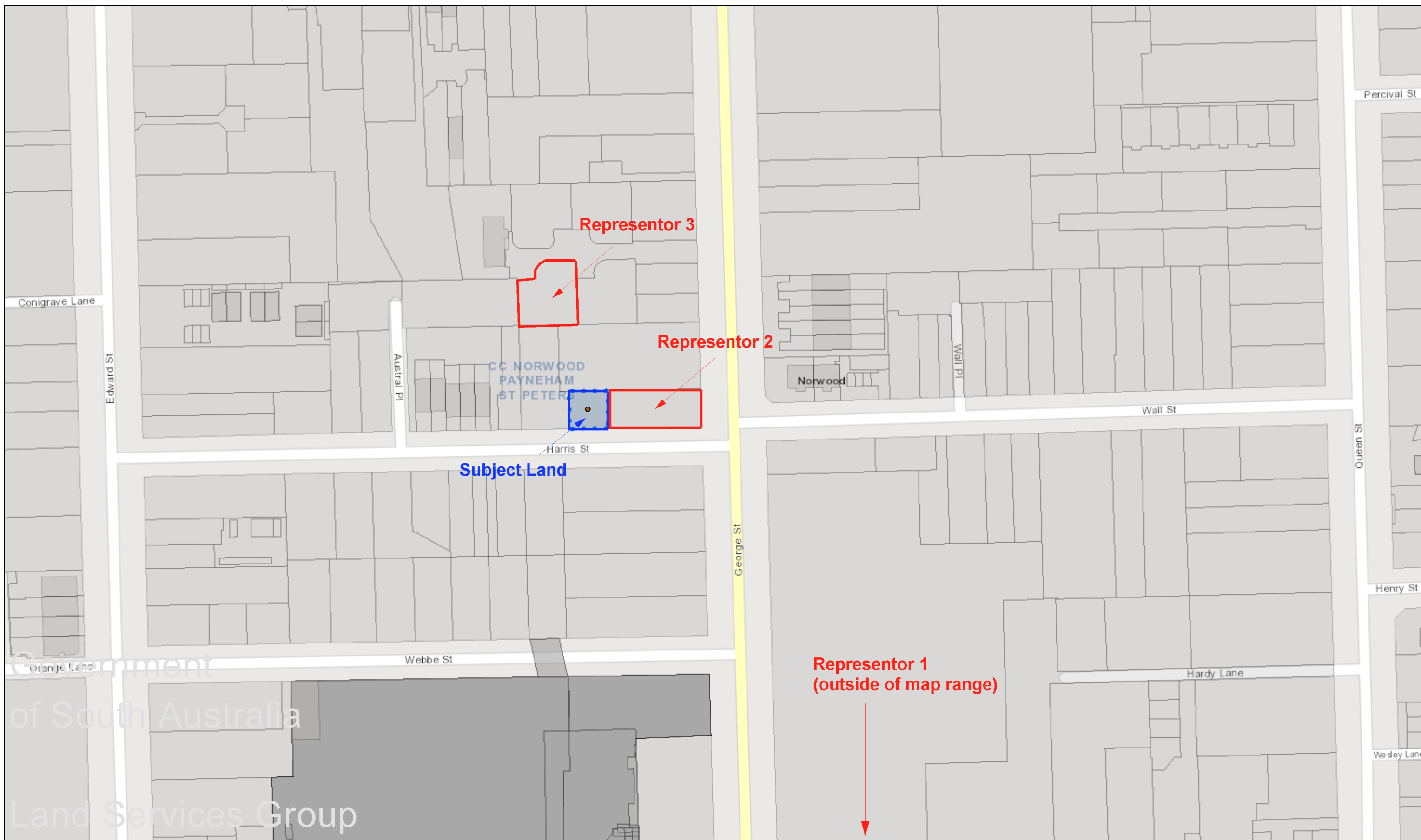
Government
of South Australia

Land Services Group

SAPPA Report

The SA Property and Planning Atlas is available on the Plan SA website: <https://sappa.plan.sa.gov.au>

Representation Map



Details of Representations

Application Summary

Application ID	22033399
Proposal	Construction of a two-storey detached dwelling with a roof-top garden, and construction of a swimming pool
Location	23 HARRIS ST NORWOOD SA 5067

Representations

Representor 1 - Oren Klemich

Name	Oren Klemich
Address	2C Macklin St. PARKSIDE SA, 5063 Australia
Submission Date	25/08/2023 04:15 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development
Reasons	
A good design which suits the allotment.	

Attached Documents

Representations

Representor 2 - Elizabeth Slatter

Name	Elizabeth Slatter
Address	C/- 33 Carrington Street ADELAIDE SA, 5000 Australia
Submission Date	29/08/2023 05:12 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
<p>Reasons Please refer to the attached Correspondence prepared by MasterPlan expressing my opposition to the proposed development.</p>	

Attached Documents

53589LET01-1269052.pdf

29 August 2023

Presiding Member
Council Assessment Panel
City of Norwood Payneham and St Peters

Attention: Kieran Fairbrother

Dear Kieran

**Re: Representation on Application ID: 22033399
Proposed Dwelling & Swimming pool, spa pool or associated safety features
At 23 Harris St, NORWOOD ,SA, 5067**

MasterPlan has been engaged by Elizabeth Slatter the owner and occupier of the property at 37 George Street, Norwood.

Our client's property abuts the eastern boundary of the subject land, and their private open space and visual outlook is directly affected by the nature of the proposed development.



Photo 1: View looking West at development site across Private Open Space from Rear Door of 37 George Street.





Photo 2: Google Street View of relationship between Subject Site and 37 George Street

The subject land represents a unique parcel of land measuring 12.50 metres by 12.19 metres with a site area of 152.36 square metres in a zone that has a minimum site area of 300 square metres for the construction of a dwelling. We note that the land parcel dates back to April 1911, but historically formed part of and provided the on-site car parking associated with the property at 37 George Street. It currently accommodates a single-storey four-car garage built abutting the western and northern boundaries of the site. The eastern elevation of the existing garage is currently setback approximately 2.6 metres from the common boundary between our client's property and the subject land.

Our client's property accommodates a single-storey detached dwelling with private open space orientated on the western portion of their land with an outlook towards the site of the proposed development.

The locality comprises a mix of land uses and built forms derived from a combination of zones. The northern side of Harris Street is located within the Established Neighbourhood Zone while the southern side of Harris Street has properties located within both the Business Neighbourhood Zone and the Urban Corridor (Main Street Zone).

The predominant built form comprises single-storey detached dwellings and detached dwelling built form occupied as offices albeit Harris Street includes some 1960 style two-storey row dwellings and the Council's two (2) level open deck car park which make a contribution to the Harris Street Streetscape.

Notwithstanding the existence of the Council's deck car park and the two-storey row dwellings, the proposed development is located within the Established Neighbourhood Zone which seeks development that *"includes a range of housing types, with **new buildings sympathetic to the predominant built form character and development patterns**"* (Zone DO 1).



While our client is not fundamentally opposed to the redevelopment of the land for the construction of a dwelling, they hold significant concerns about the design in its current form and the obtrusive visual intrusion over their private open space.

The design of the proposed dwelling introduces the construction of boundary wall abutting common boundary with our client's land measuring 10.69 metres in length representing 87.7 per cent of the length of the boundary. The boundary wall at ground level has a wall height extending to 3.2 metres high, and then extends above that height accommodating the stair well and lift shaft both abutting the common boundary to a height of 9.0 metres. This eastern elevation also incorporates pleated perforated metal screening with a 25 per cent transparency to act as privacy screens at Level 1, which add to visual dominance of boundary structures intruding upon the visual impact of our client's property.

The boundary screening the level 1 balcony extends to a height of 6.3 metres above ground level presenting a solid wall structure and the screening above the rear courtyard boundary wall extends to a height of 4.5 metres above ground level marginally set in 200 millimetres from the boundary but for all intents and purposes presenting as a solid boundary wall that results in an obtrusive visual form when viewed from our clients private open space and in the Harris Street streetscape.

Whilst there is a change in materials used in the eastern elevation, the resulting bulk and scale of the dwelling presents an imposing built form.

Zone Performance Outcome (PO) 7.1 expressly seeks that "**walls on boundaries are limited in height and length to manage visual and overshadowing impacts on adjoining properties**". The accompanying Desired Performance Feature (DPF) which I note is one way of achieving the corresponding PO provides for boundary walls that do not exceed 3.2 metres in height, 8.0 metres in length and 45 per cent of the length of the boundary. The proposed boundary wall abutting our client's property exceeds all of these quantitative measures and notwithstanding any other performance outcome, it cannot be concluded that the proposed wall on the boundary is "limited in height and length" so as to manage the visual impact on our client's adjoining property.

The extent of the boundary development is exacerbated by the fact that the proposed dwelling also exceeds the height and length of boundary walls on the northern and western boundaries of the subject development site.

Furthermore, it is considered that the proposed side boundary setbacks do not provide sufficient separation between buildings in a way that complements the established character of the locality and therefore does not satisfy PO 8.1, noting that:

- Level 1 incorporates a combination of Nil setback (to the lift shaft and stairwell) to a maximum 0.2 metres to the Living Dining Wall.
- Roof Deck Parapet and combined 1.7-metre-high translucent screen has a minimum 1.2 meters setback measured at 6.2 metres above ground level.



Having regard to the associated DPF, that portion of the wall above 3.0 metres in height should be setback a minimum of 2.7 metres (Being 900 millimetres + 1/3rd of the 8.2 metre wall height >3.0 metre wall height). For the purposes of calculating wall height, I have considered the wall height to be to the top of the translucent polycarbonate screening to the roof deck, albeit the lift shaft continues to a height of 9.0 meters and accordingly should potentially be setback further.

The failure of the dwelling design to incorporate appropriate side setbacks at the upper level of the eastern elevation further exacerbates the imposing visual intrusion of the built form over our client's private open space.

While the constraints of the site are acknowledged, the extent of boundary walls and the failure of the proposed dwelling to incorporate appropriate setbacks from the side boundaries is a direct result of trying to build too much on a small parcel of land, noting that the proposed development results in a site coverage of approximately **71 per cent** contrary to Zone PO 3.1 which seeks that "building footprints are consistent with the character and pattern of the neighbourhood and **provide sufficient space around buildings to limit visual impact**, provide an attractive outlook and access to light and ventilation." The associated DPF provides guidance as to one way of achieving sufficient space is to minimise site coverage to no more than 50 per cent of the site. For comparison purposes, the existing building form (Four-car garage) on the land has an approximate site coverage of 50 per cent.

While it is not our client's role to redesign the proposed development, there would appear opportunities to reconfigure the design of the proposed dwelling to significantly limit the extent and height of boundary wall on our client's abutting boundary so as to minimise the visual impact of the proposed dwelling over our client's property.

Accordingly, our client opposes the design of the proposed dwelling as presented on the plans made available for Public Notification.

We are of the opinion that the proposed development does not sufficiently meet the relevant Performance Outcomes and should be refused on the basis that:

- The proposed boundary walls are not limited in height and length and result in a significant visual impact over our client's property at 37 George Street contrary to achieving Zone PO 7.1.
- The overall height of the proposed building does not contribute to the "prevailing" single-storey character of the neighbourhood and does not complement the height of nearby buildings contrary to achieving PO 4.1.
- The proposed dwelling with a site coverage in the order of 71 per cent results in a building footprint that is not consistent with the character and pattern of the neighbourhood so as to provide sufficient space around the building and accordingly does not limit its visual impact contrary to Zone PO 3.1.



- The proposed dwelling does not incorporate sufficient side setbacks to provide separation between buildings in a way that complements the established character of the locality and accordingly does not achieve Zone PO 8.1.
- The design of the dwelling does not achieve Desired Outcome 1 of the zone in that the proposed building is not sympathetic to the predominate built form character and development patterns for the neighbourhood and will be visually dominant in the Harris Street streetscape and when viewed from the adjoining residential properties.

Our client would welcome their concerns being considered by the applicant with amendments to the plans being incorporated that limit the visual impact of the proposed dwelling through the reduction in the extent of boundary walls and incorporation of sufficient side setbacks.

Our client reserves the right to make verbal submissions before the Council's Assessment Panel in support of the representation.

Can you please advise of the date and time of the meeting of Council's Assessment Panel at which this matter will be reported for determination.

Yours sincerely

Greg Vincent
MasterPlan SA Pty Ltd

Representations

Representor 3 - Deborah Skelly

Name	Deborah Skelly
Address	2/31 George Street NORWOOD SA, 5967 Australia
Submission Date	29/08/2023 07:15 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I support the development with some concerns
<p>Reasons</p> <p>The roof top garden at rear of property will allow for direct viewing into our living space. Likewise we will be able to see all people on the roof space. We request that the privacy screen around this space is of sufficient height to block viewing from either aspect.</p>	

Attached Documents

Response to representations development application ID 22033399

23 Harris St Norwood

Background

We bought this land three years ago knowing that it was a challenging site. This land and that at 37 George St were sold separately at that time. We have been working on the design of a house for two of those years with architect John Maitland who specialises in energy efficient and sustainable houses.

We are committed to an energy efficient house, therefore orientation is important and hence the ability to include a northern aspect and the decision to have the living area upstairs. This also shows in the lack of windows and features on the western wall to avoid the western sun; there is no overlooking issue on that side. Naturally we want to maximise the northern aspect.

We are also intending that this be our final house. We are both retired and have designed for 'ageing in place' with the inclusion of a lift showing this.

We do not want to alienate our neighbours and have been very conscious of the impact the building will have upon them, in particular those at 37 George St. The land is unusual and so requires careful design and use of space. Any residential use will probably be more than one storey, and we think our design appears as a two storey house, albeit with the fence around the roof garden. The addition of a roof garden is because we enjoy growing our own fruit and vegetables, and it will also increase green space and help to counteract urban heat.

Over the design process we have tried many different options for locating the lift without limiting northern light; orienting internal spaces so living areas do not overlook neighbours; articulating the eastern and northern aspects for visual interest; and designing spaces for our lifestyle in retirement.

Representation – 2/31 George St

The concern of this neighbour is that there will be the ability for us and them to see each other from and to the roof garden. On a search of SAPPa we see that this neighbour is part of a group of units/townhouses at 31 George St, no. 2 appears to be at the rear of 21 Harris St. The SAPPa measuring tool shows the distance between our northern boundary and the southern boundary of 2/31 George as approximately 22.8m.

In addition to the distance, there will be no ability for us to see into any part of 31 George St due to the screening proposed at the edge of the roof garden to a height of 1.7m. This is a common planning requirement for preventing overlooking.

We hope the hedging to be planted around the perimeter will assist the screening and provide a green and soft, pleasant look. Other aspects of the roof garden will be close to the perimeter for structural load-bearing purposes and these may be visual and pleasant in time as the plants grow.

The roof garden is a garden only, there is no outdoor entertaining area, note the BBQ will not be on this level. There will be flooring or decking so we can walk on the roof to tend the

garden. Note also the structure for solar panels is a light structure to support the panels only, not to be weather-proof.

Representation – 37 George St

Understandably the main concern for this neighbour is the visual impact of our eastern wall. We are aware that this is somewhat imposing but have worked hard to make it of more interest.

We address the concerns mentioned as follows:

Predominantly single-storey.

- Desired Outcome 1 (DO1)
A neighbourhood that includes a range of housing types, with new buildings sympathetic to the predominant built form character and development patterns.
- We disagree that the predominant built form *of the neighbourhood* (our emphasis) is single-storey.
‘Neighbourhood’ is not defined and does not (yet) have an established planning meaning. A reading of the other zone provisions makes it clear that it is the neighbourhood which is to be considered.
This neighbourhood must surely include The Parade and at least the buildings on the northern side. We say in fact that a neighbourhood may not be capable of exact delineation and will be different in different cases.
- We think this neighbourhood is a true mix of built form, including dwellings and other buildings. This neighbourhood includes many single and double storey residential buildings in the Established Neighbourhood Zone, but also many shops and other commercial premises on The Parade, including the Town Hall, cinema complex, shopping centre and car park.

Quantitative measures

- We say that these are not to be rigidly applied in Performance Assessed considerations.
- The representation notes that Desired Performance Features (DPF) are just one way of achieving a desired result. The Code specifically states that these may assist an Authority to make its assessment, they are not specific requirements. We agree that they provide a guide and are to facilitate assessment but say it is inappropriate to get too caught up in strict application. The purpose of the Performance Assessed provisions is to assess a development application that does not fall within the numerical criteria. The code states that a Designated Performance Feature ‘... *does not derogate from the discretion to determine that the outcome is met in another way, or from the need to assess development on its merits against all relevant policies.*’ We think that this design does meet the performance outcomes sought in various ways.
- We also think any residential development on this site would not be capable of meeting the DPF criteria.

Bulk, height

- The building is tall in places and if walls are measured including the screens then a false impression can be given. We have been careful to make the screening less

intrusive by using translucent screens behind pool fencing on the roof so it will not look solid. In order to avoid overlooking it is necessary to use screens, the perforated and pleated metal screens are to give another texture and interest but are to be maximum 25% as required by council planning considerations. These will not appear bland and flat. See photos below of examples.

- The rear screens have been set in 200mm in order to avoid the appearance of a solid wall; this will produce different light and shade and different material is used. The pleating of the perforated metal will generate shadow within the screens.
- We have proposed different colours and materials generally to ‘... *manage visual and overshadowing impacts* ...’.
- We acknowledge there are no side setbacks but say that this building will not affect any light or ventilation for any other buildings.
- We disagree that this building will be an overdevelopment of this land. It is a small site but is ideal for a small residence. The house is a modest two-bedroom home with proportions that are not excessive. We see this as responsible infill development, particularly considering the street in which it is located, the carpark opposite, the mixed nature of the neighbourhood, the proximity of shopping and public transport. In addition we will be adding to the aesthetic of the street with garden at the front and on the roof. This will increase greenery and hopefully assist to attract more birds and bees.


Potential changes

- We have now noted that the application plans do not show the existing fence on our eastern boundary remaining in situ which can of course occur and will assist to limit the look of a blank wall.
- We are happy to discuss other paint colours for the eastern wall, though will not agree to a dark, heat attracting colour. We strongly disagree with the popular trend of black, Colorbond boxes as second storeys.
- We are happy to engage a mural artist. While this sounds facetious, it is not. At our previous house we greatly enjoyed a mural by the swimming pool, further we have the impression that street art is generally increasing in Adelaide and the suburbs.
- Non-operable, frosted windows or other inserts could be included in the eastern boundary wall above the stairs to add interest. At one time we were going to include a number of coloured glass bricks to break up the wall and are happy to do this.
- The metal perforated screens can be powder-coated any colour, again we would not advocate a colour too dark.
- The screen at the front balcony can be reduced in height to 1.7m but will have to be at least this to prevent overlooking.



DRAWING LIST				
Sheet #	Sheet Name	Current Revision	Current Revision Date	Current Revision Description
A011	PROPOSED SITE PLAN	0	07/09/2022	Preliminary
A100	FLOOR PLAN	0	07/09/2022	Preliminary
A120	ROOF PLAN	0	07/09/2022	Preliminary
A200	ELEVATIONS	0	07/09/2022	Preliminary
A201	ELEVATIONS	0	07/09/2022	Preliminary
A700	3D VIEWS 1	0	07/09/2022	Preliminary
A701	SHADOW DIAGRAMS - SUMMER SOLSTICE	0	07/09/2022	Preliminary
A702	SHADOW DIAGRAMS - WINTER SOLSTICE	0	07/09/2022	Preliminary


PRELIMINARY

CLIENT: STEVE & ALISON		REV	DESCRIPTION	DATE	BROOKMAN DWELLING		
 <p>LITTLE FOOT DESIGN</p> <p>SCALE BAR A3 SHEET</p> <p>ALL RIGHTS RESERVED TO THE COMPANY. NO REPRODUCTION UNLESS WRITTEN CONSENT GIVEN</p>		0	Preliminary	07/09/2022	23 HARRIS ST, NORWOOD SA		
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						REVISION	
					2206-A000		
					0		
					DRAWN: Author		

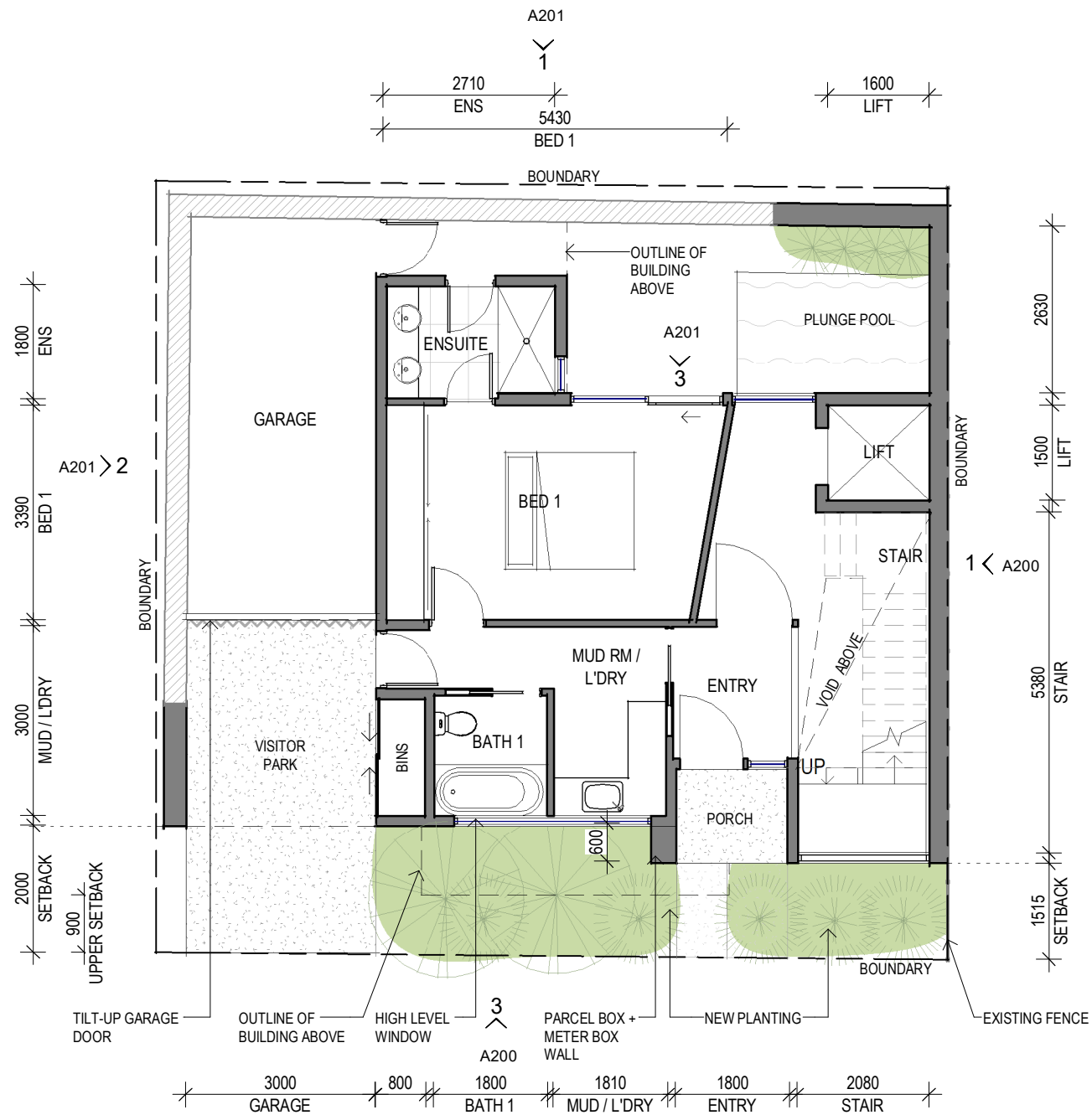
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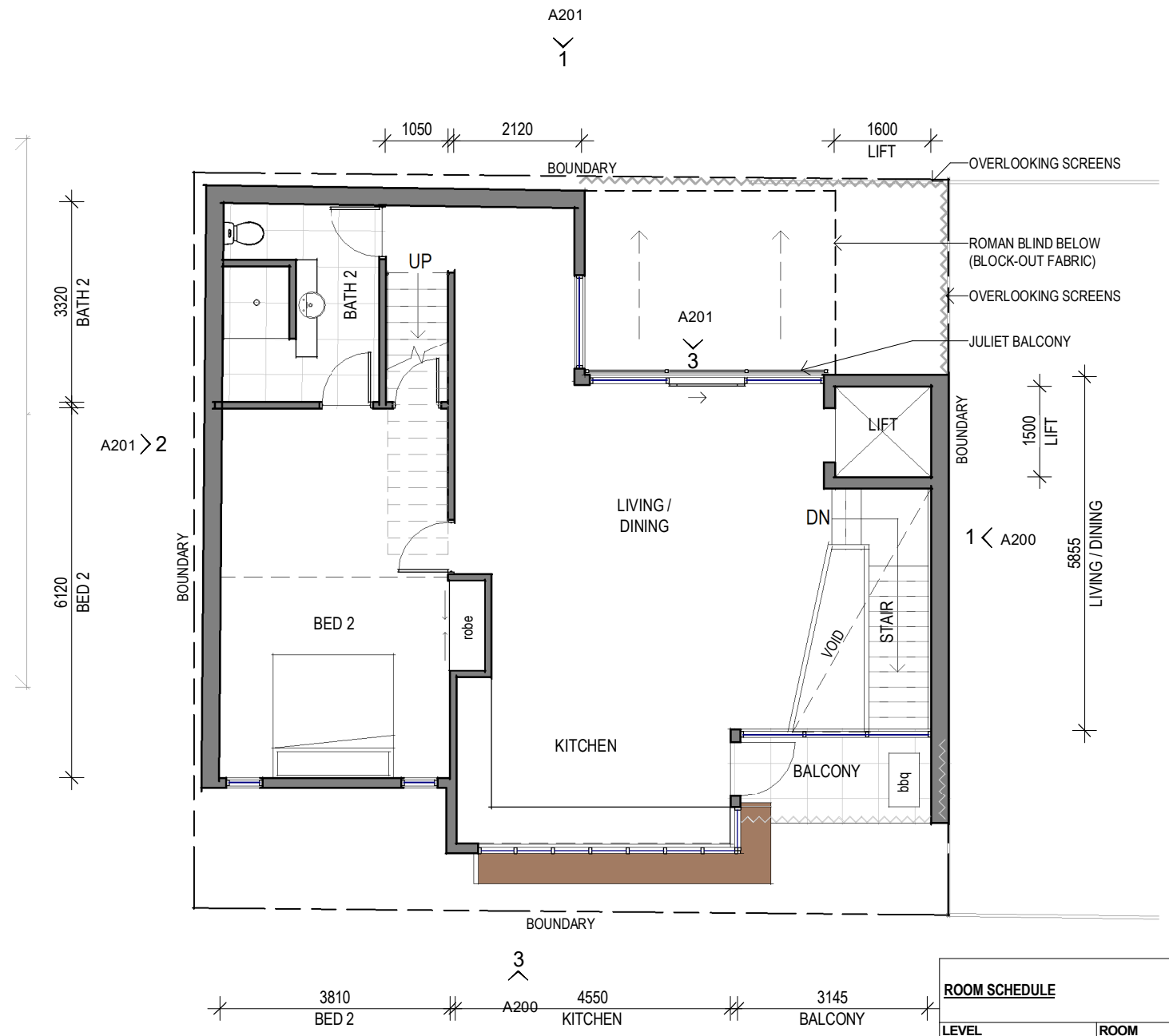
1 SITE PLAN
A200 1:100

CLIENT: STEVE & ALISON		REV	DESCRIPTION	DATE	BROOKMAN DWELLING	
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					SCALE: 1:100	JOB / DRAWING No.
			DRAWN: SM	2206-A011		0

Original Submission



1 GROUND FLOOR PLAN
A200 1 : 100

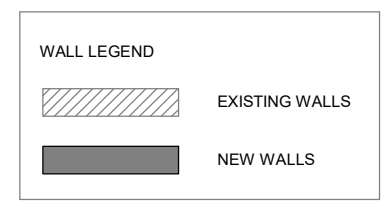


2 FIRST FLOOR PLAN
A200 1 : 100

LEVEL	ROOM	AREA
GROUND FLOOR LEVEL	BATH 1	3.4
GROUND FLOOR LEVEL	BED 1	17.2
GROUND FLOOR LEVEL	ENSUITE	4.7
GROUND FLOOR LEVEL	ENTRY	3.9
GROUND FLOOR LEVEL	LIFT	2.4
GROUND FLOOR LEVEL	MUD RM / L'DRY	7.8
GROUND FLOOR LEVEL	STAIR	16.0
		55.4

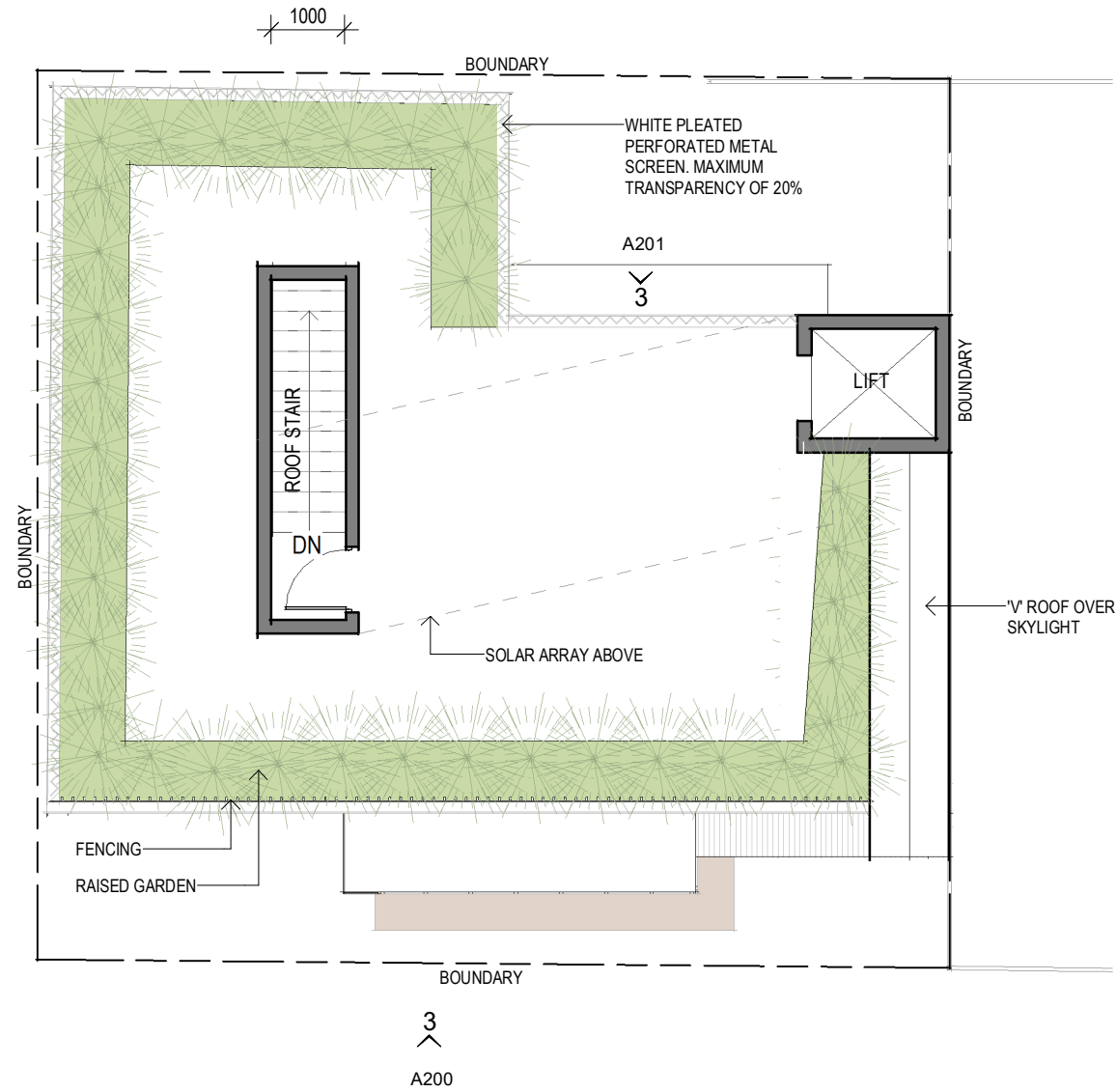
FIRST FLOOR LEVEL	BATH 2	8.3
FIRST FLOOR LEVEL	BED 2	13.5
FIRST FLOOR LEVEL	KITCHEN	12.5
FIRST FLOOR LEVEL	LIFT	2.4
FIRST FLOOR LEVEL	LIVING / DINING	38.4
FIRST FLOOR LEVEL	STAIR	7.5
		82.6

ROOF LEVEL	LIFT	2.6
ROOF LEVEL	ROOF STAIR	4.7
		7.3
Grand total		145.3

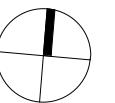


	CLIENT: STEVE & ALISON	REV 0 Preliminary	DESCRIPTION: BROOKMAN DWELLING 23 HARRIS ST, NORWOOD SA FLOOR PLAN	DATE: 07/09/2022
	SCALE BAR A3 SHEET ALL RIGHTS RESERVED TO THE COMPANY. NO REPRODUCTION UNLESS WRITTEN CONSENT GIVEN	SCALE: As indicated DRAWN: SM	JOB / DRAWING No. 2206-A100 REVISION 0	

Original Submission



1 ROOF PLAN
A200 1 : 100



CLIENT: STEVE & ALISON

REV	DESCRIPTION	DATE
0	Preliminary	07/09/2022

BROOKMAN DWELLING
23 HARRIS ST, NORWOOD SA
ROOF PLAN



SCALE BAR | A3 SHEET

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SCALE: 1 : 100

DRAWN: SM

JOB / DRAWING No.

2206-A120

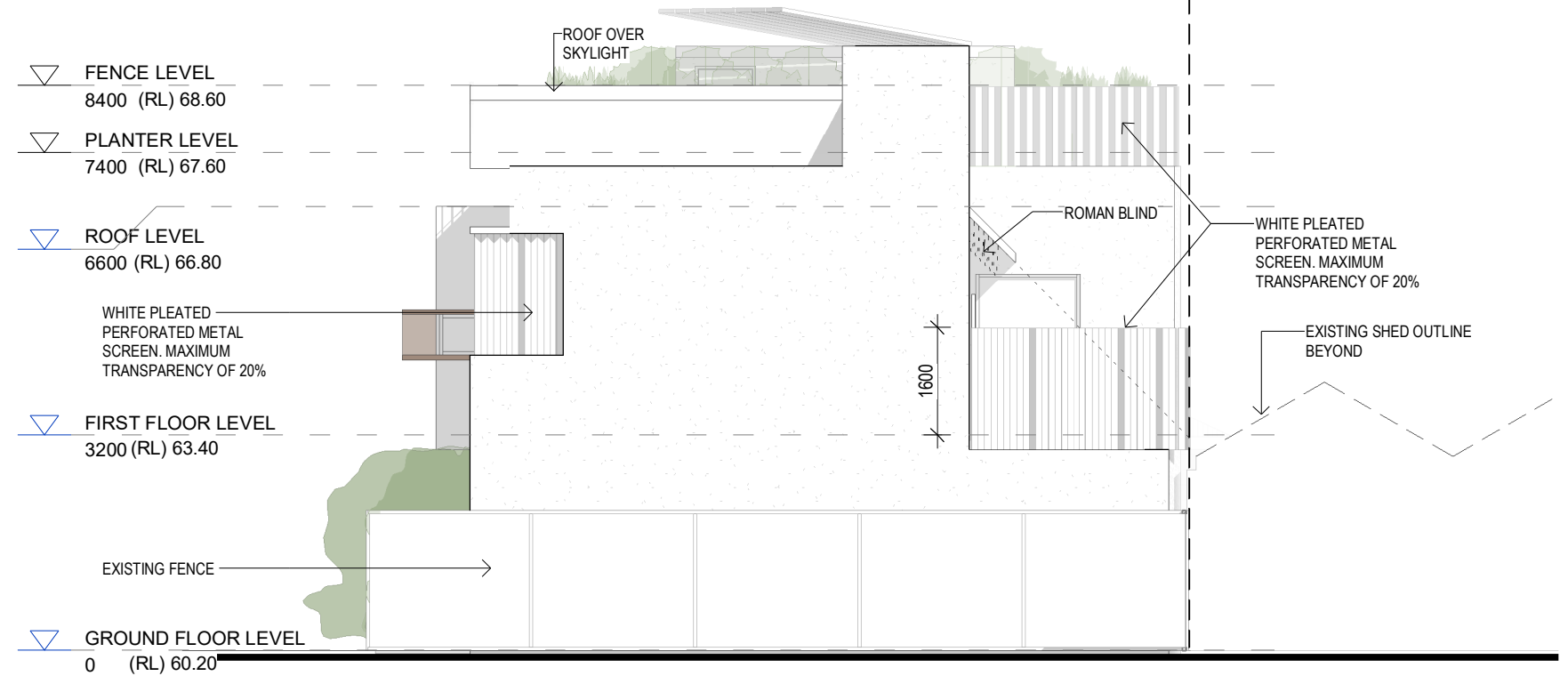
REVISION

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Original Submission



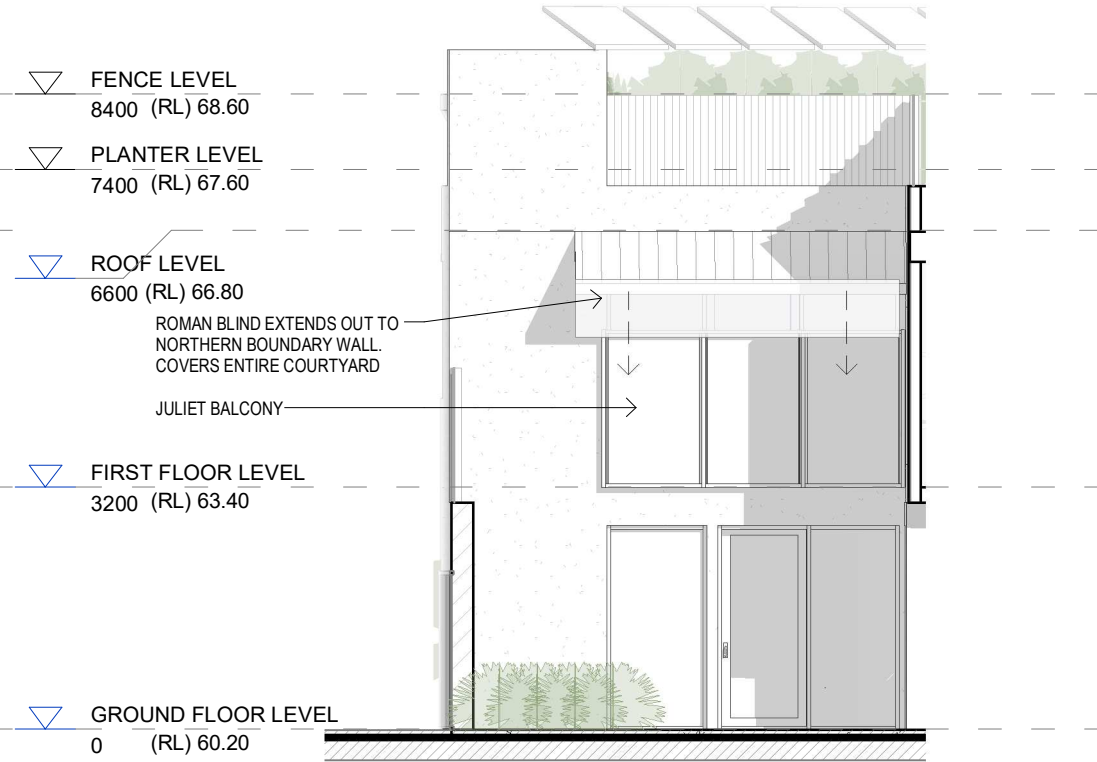
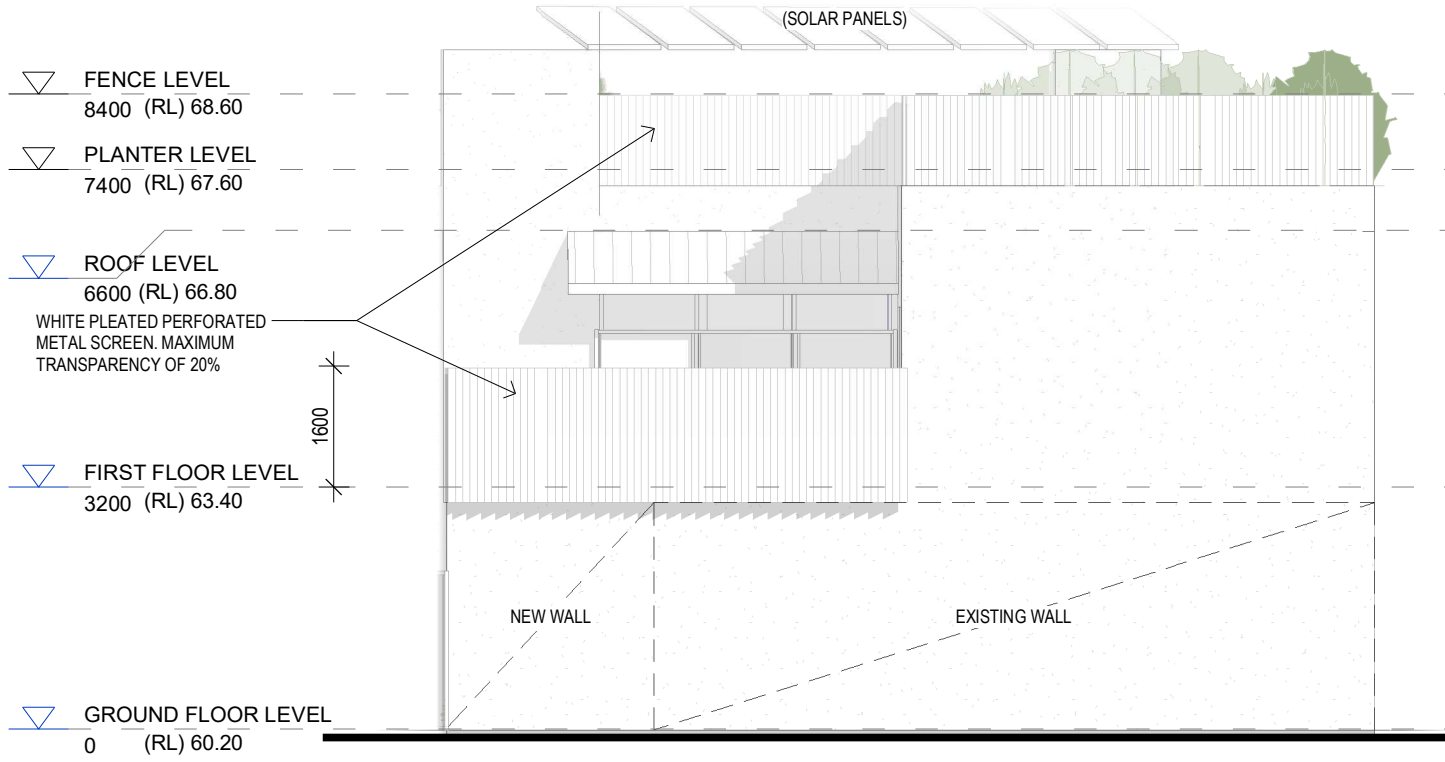
3 SOUTH ELEVATION
A100 1 : 100



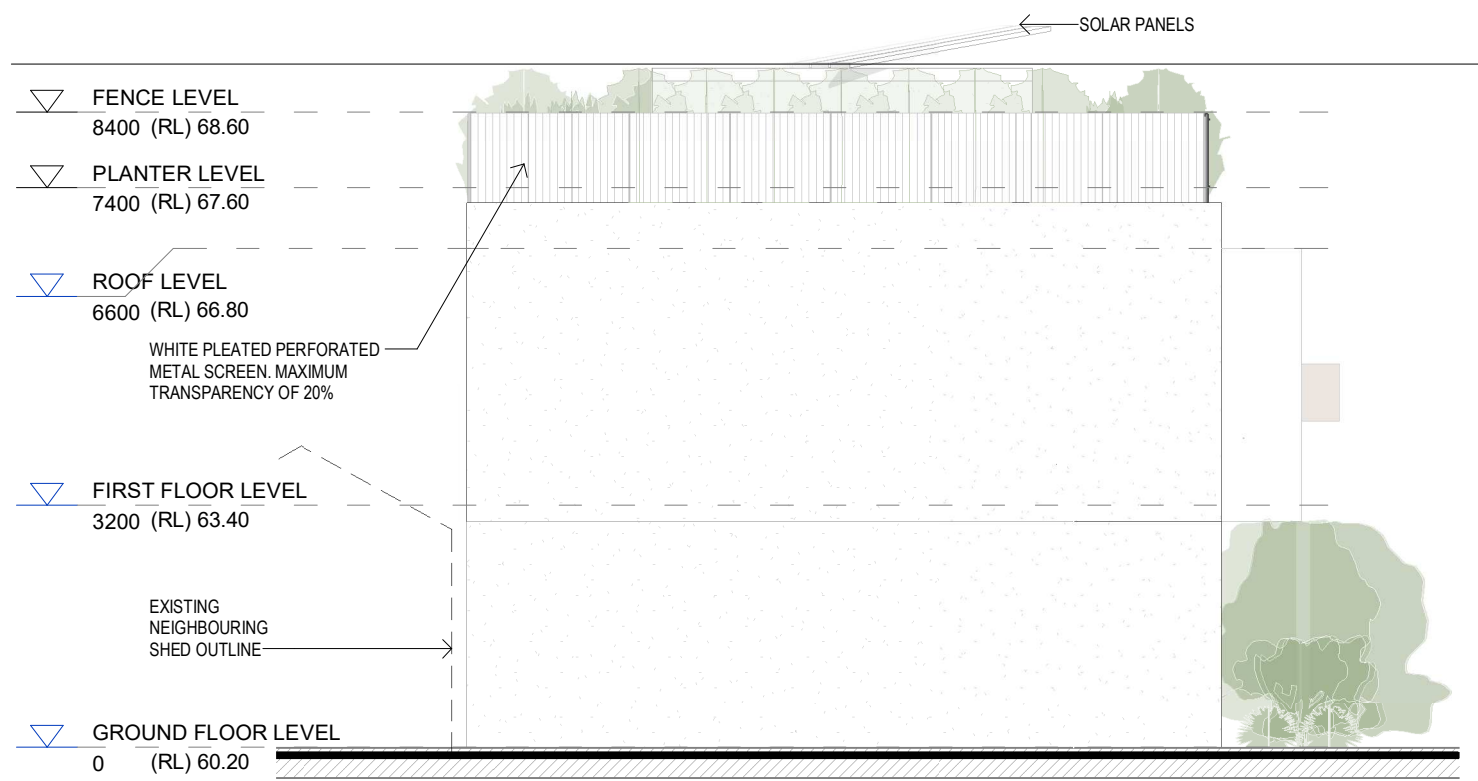
1 EAST ELEVATION
A100 1 : 100

CLIENT: STEVE & ALISON		REV	DESCRIPTION	DATE	BROOKMAN DWELLING 23 HARRIS ST, NORWOOD SA		
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					SCALE: 1 : 100	JOB / DRAWING No.	REVISION
					DRAWN: SM	2206-A200	0


Original Submission



1 NORTH ELEVATION
A100 1 : 100



2 WEST ELEVATION
A100 1 : 100

CLIENT: STEVE & ALISON		REV	DESCRIPTION	DATE	BROOKMAN DWELLING 23 HARRIS ST, NORWOOD SA ELEVATIONS	
 <p>SCALE BAR A3 SHEET</p> <p>ALL RIGHTS RESERVED TO THE COMPANY. NO REPRODUCTION UNLESS WRITTEN CONSENT GIVEN</p>		0	Preliminary	07/09/2022	SCALE: 1 : 100	JOB / DRAWING No. 2206-A201
					DRAWN: Author	REVISION 0

Original Submission



3D VIEW 1



3D VIEW 2



3D VIEW 3



AERIAL VIEW



STREETSCAPE PERSPECTIVE

CLIENT:

STEVE & ALISON

REV

DESCRIPTION

DATE

0

Preliminary

07/09/2022

BROOKMAN DWELLING

23 HARRIS ST, NORWOOD SA

3D VIEWS 1

SCALE: NTS

DRAWN: SM

JOB / DRAWING No.

2206-A700

REVISION

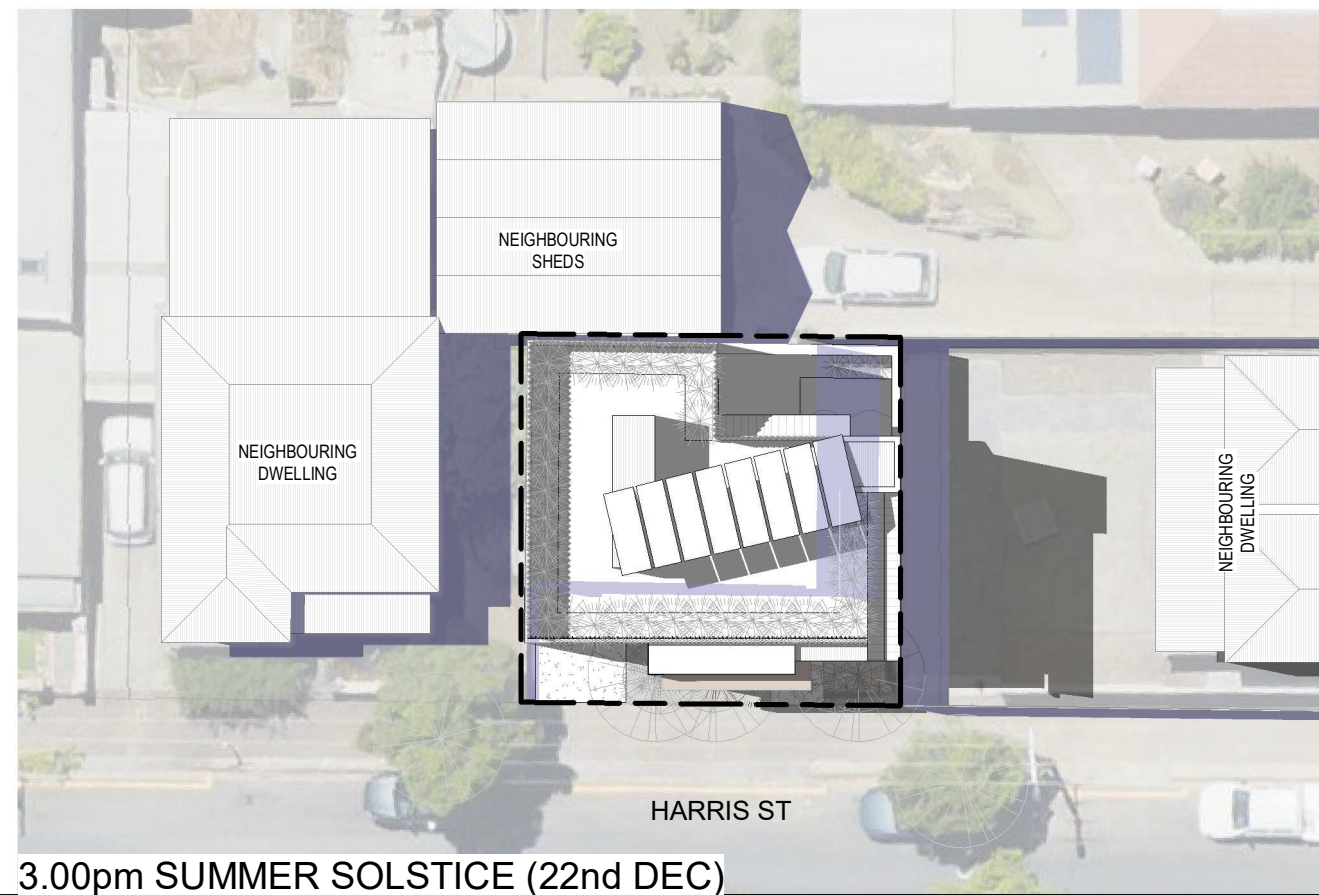
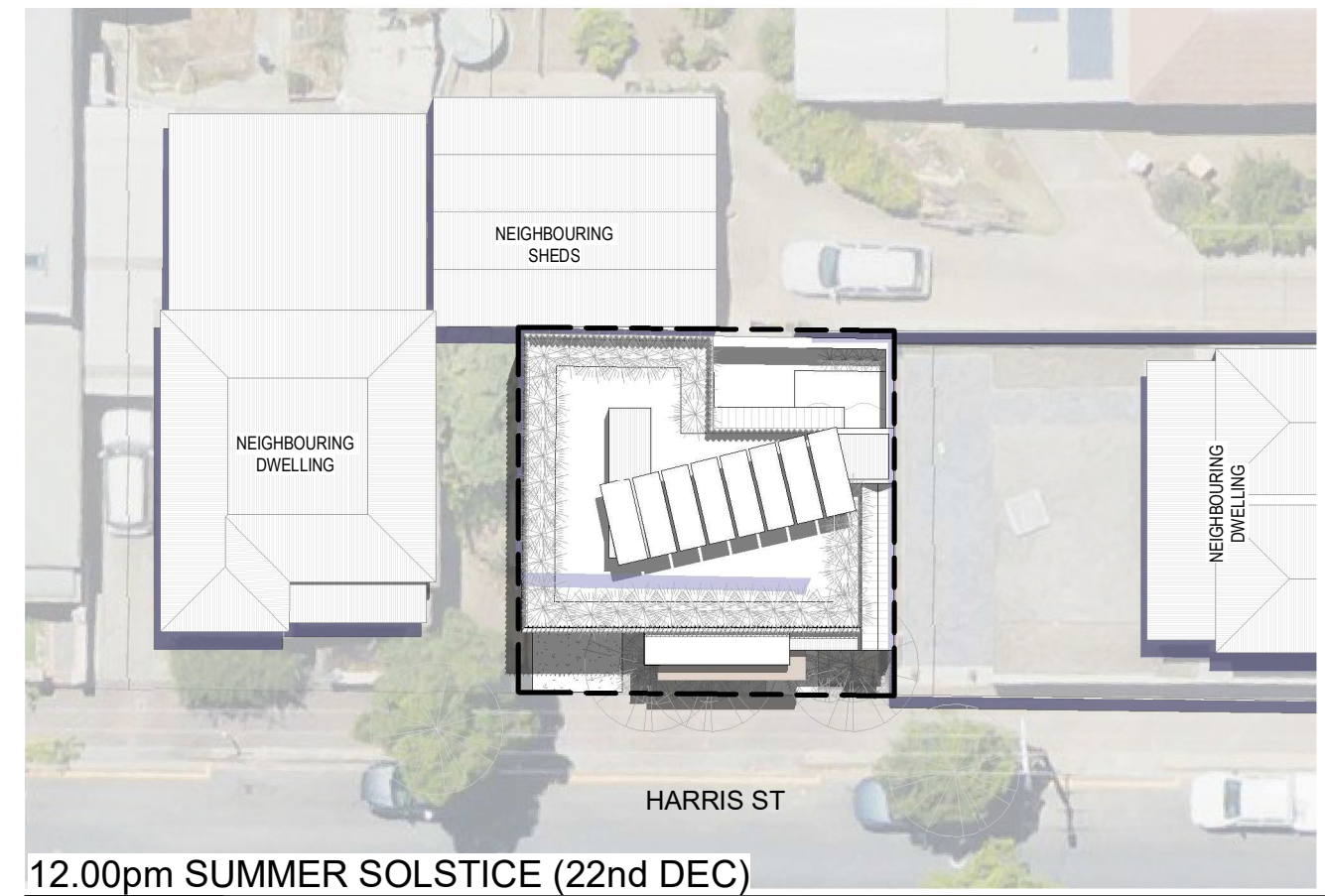
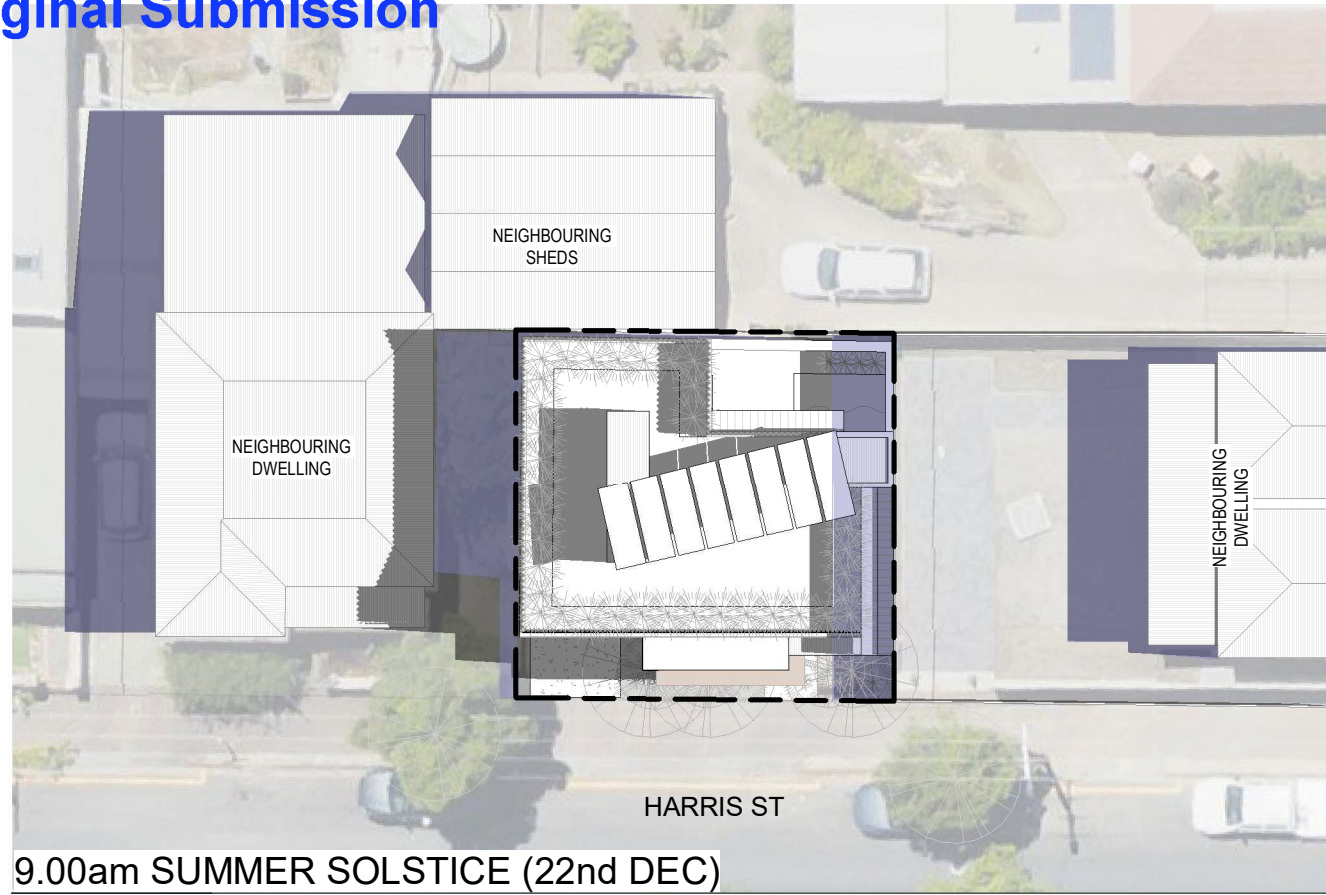
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SCALE BAR | A3 SHEET

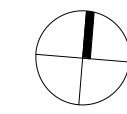
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Original Submission



SHADOW LEGEND

- NEW BUILDING SHADOWS
- EXISTING SHADOWS



CLIENT: **STEVE & ALISON**

REV	DESCRIPTION	DATE
0	Preliminary	07/09/2022

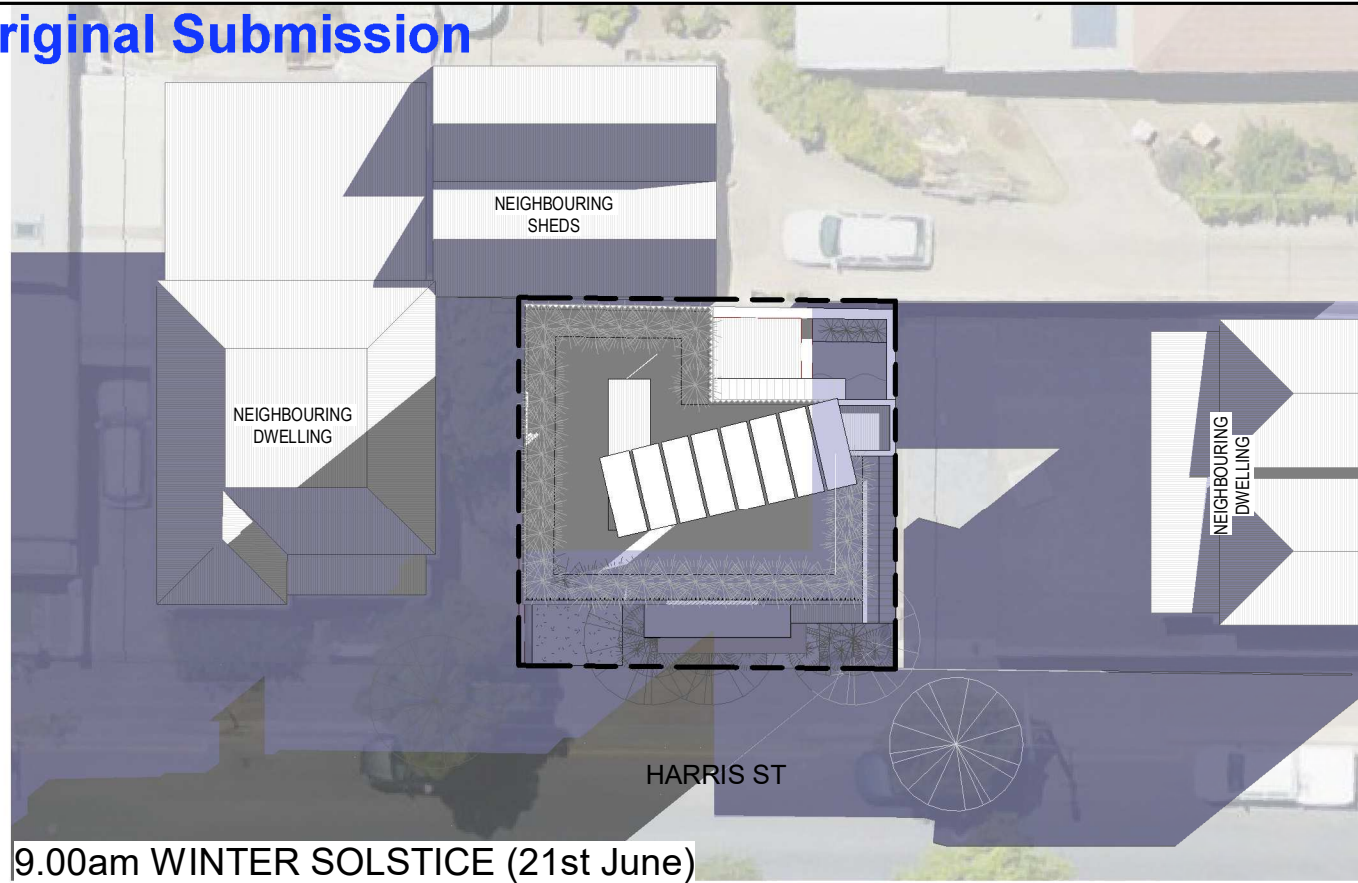
BROOKMAN DWELLING
 23 HARRIS ST, NORWOOD SA
SHADOW DIAGRAMS - SUMMER SOLSTICE



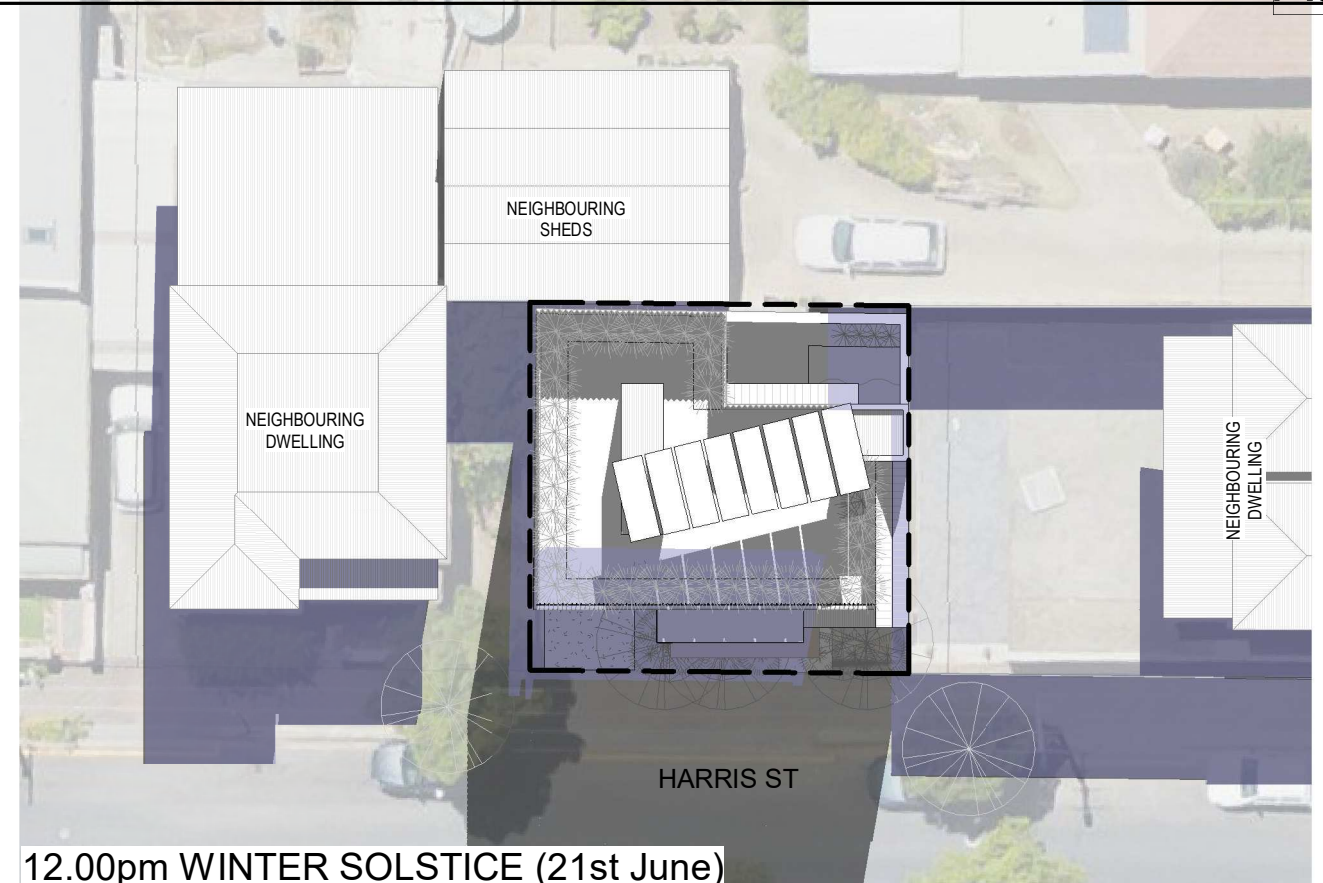
SCALE BAR | A3 SHEET
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SCALE: As indicated	JOB / DRAWING No.	REVISION
DRAWN: Author	2206-A701	0

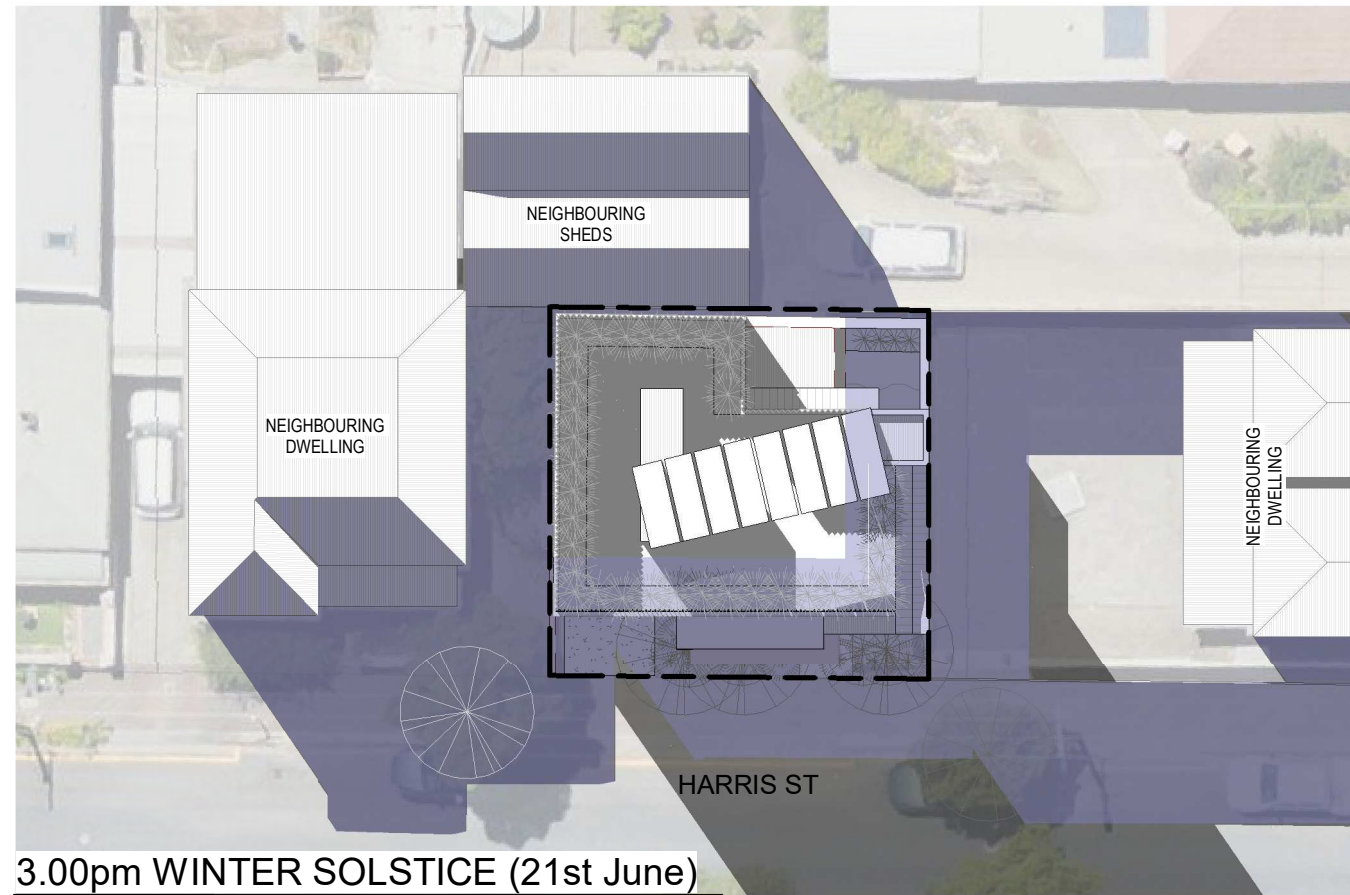
Original Submission



9.00am WINTER SOLSTICE (21st June)



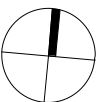
12.00pm WINTER SOLSTICE (21st June)



3.00pm WINTER SOLSTICE (21st June)

SHADOW LEGEND

- NEW BUILDING SHADOWS
- EXISTING SHADOWS



CLIENT: STEVE & ALISON	REV DESCRIPTION DATE	BROOKMAN DWELLING 23 HARRIS ST, NORWOOD SA SHADOW DIAGRAMS - WINTER SOLSTICE
	0 Preliminary 07/09/2022	SCALE: As indicated DRAWN: Author
SCALE BAR A3 SHEET <small>ALL RIGHTS RESERVED TO THE COMPANY. NO REPRODUCTION UNLESS WRITTEN CONSENT GIVEN</small>		JOB / DRAWING No. 2206-A702 REVISION 0

Kieran Fairbrother

From: Kieran Fairbrother
Sent: Friday, 28 October 2022 3:52 PM
To: 'alibrookman7@outlook.com'; 'brookie1950@hotmail.com'
Subject: Development Application 22033399 - 23 Harris St, Norwood - Concerns with Application

Hi Alison and Stephen,

I am the planner with carriage of the abovementioned development application, for the construction of a new dwelling at 23 Harris St, Norwood. I have undertaken a brief assessment of this proposal which has raised some concerns, that I'd like to bring to your attention with the intention of potentially resolving these concerns before the application goes out on public notification.

As you might be aware, this site is located in the *Established Neighbourhood Zone* which has specific Performance Outcomes (PO's) – guided by Designated Performance Features (DPF's) – that provide guidance on what development is anticipated and reasonable within the Zone. I have detailed my concerns below, with reference to the applicable PO's and/or DPF's.

Once you have read and understood my concerns outlined below, I think it would be worthwhile organising a time to meet next week to discuss these, and how I suggest you would be able to overcome them.

Eastern Boundary Wall

The following policies are most pertinent to this element of the proposal:

Established Neighbourhood Zone:

Boundary Walls	
<p>PO 7.1</p> <p>Dwelling boundary walls are limited in height and length to manage visual and overshadowing impacts on adjoining properties.</p>	<p>DTS/DPF 7.1</p> <p>Dwellings do not incorporate side boundary walls where a side boundary setback value is returned in (a) below:</p> <p>(a)</p> <p>or</p> <p>(b) where no side boundary setback value is returned in (a) above, and except where the dwelling is located on a central site within a row dwelling or terrace arrangement, side boundary walls occur only on one side boundary and satisfy (i) or (ii) below:</p> <p>(i) side boundary walls adjoin or abut a boundary wall of a building on adjoining land for the same or lesser length and height</p> <p>(ii) side boundary walls do not:</p> <p>A. exceed 3.2m in height from the lower of the natural or finished ground level</p> <p>B. exceed 8m in length</p> <p>C. when combined with other walls on the boundary of the subject development site, exceed a maximum 45% of the length of the boundary</p> <p>D. encroach within 3m of any other existing or proposed boundary walls on the subject land.</p>

Side Boundary Setback	
<p>PO 8.1</p> <p>Buildings are set back from side boundaries to provide:</p> <p>(a) separation between buildings in a way that complements the established character of the locality</p> <p>(b) access to natural light and ventilation for neighbours.</p>	<p>DTS/DPF 8.1</p> <p>Other than walls located on a side boundary in accordance with Established Neighbourhood Zone DTS/DPF 7.1, building walls are set back from the side boundary:</p> <p>(a) no less than:</p> <p>(b) in all other cases (i.e. there is a blank field), then:</p> <p>(i) at least 900mm where the wall is up to 3m</p> <p>(ii) other than for a south facing wall, at least 900mm plus 1/3 of the wall height above 3m</p> <p>(iii) at least 1.9m plus 1/3 of the wall height above 3m for south facing walls.</p>

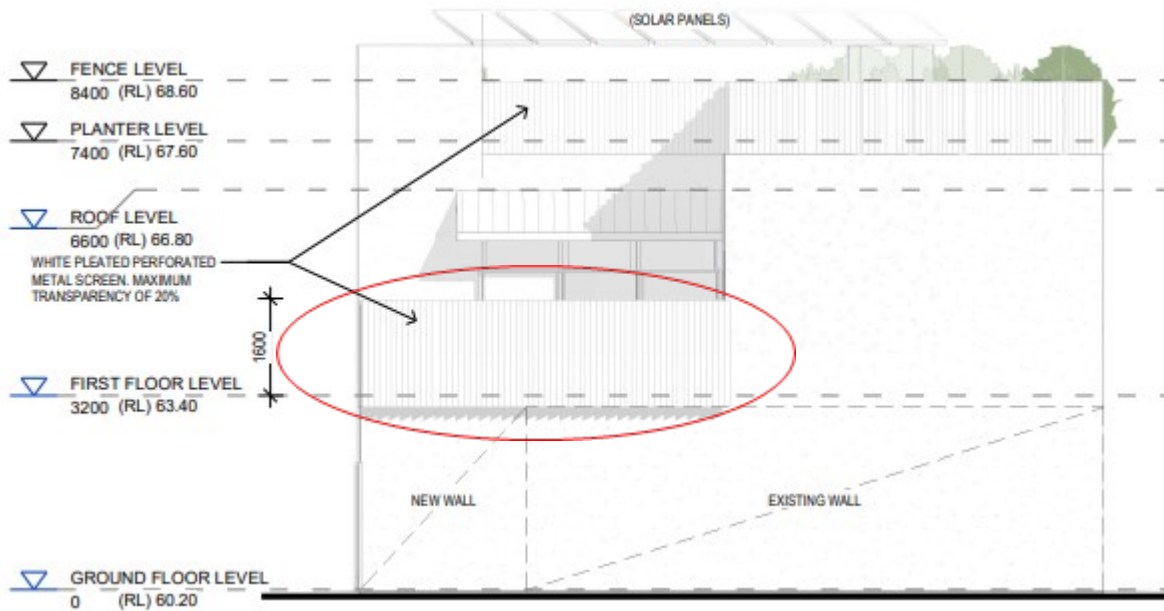
General Development Policies – Design in Urban Areas

<p>PO 20.3</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 20.3</p> <p>None are applicable</p>
---	--

The proposed eastern boundary wall is severely at odds with the abovementioned policies. This wall presents a visually-imposing and dominant element to the eastern neighbour, which will severely impact the amenity of their private open space; and this is an unreasonable outcome in my opinion. Additionally, the wall will be visible from Harris Street and so it will also have a negative effect on the streetscape. I appreciate the restrictions that come with trying to build a reasonable dwelling on a site of this size, but there has to be a balance between what dispensations we're willing to accept and what a neighbour should be expected to accept.

White-pleated perforated metal screen

I share similar concerns about the eastern boundary wall as I do about the proposed screening along the eastern and northern boundaries of the site (below). I understand the intent of the screening is to prevent overlooking, but in my opinion it is unnecessary to place this screening on the boundary, atop 3.2m boundary walls. This only exacerbates the departures from the above Performance Outcomes, without any real justification. The screens ideally should be fixed to the exterior walls surrounding the windows they intend to screen; or alternatively the windows are treated with obscure glazing/privacy film.



Colour Scheme

The plans seem to suggest that all exterior walls of the dwelling are going to be white in colour (or similar), which isn't a great outcome from a streetscape perspective or for neighbouring properties that have outlooks onto the building (despite being great environmentally). Dwellings should create visual interest and be articulated in colours and materials in such a manner that they:

- make a positive contribution to the streetscape;

External ap
<p>PO 20.2</p> <p>Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and the appearance of common driveway areas.</p>

- reduce the visual mass of the building;

<p>PO 20.3</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>

- are sympathetic to the existing housing stock in the locality

PO 10.2

The appearance of development as viewed from public roads is sympathetic to the wall height, roof forms and roof pitches of the predominant housing stock in the locality.

Other considerations

1. Your garage is currently only set back 5.2m from the street, whereas it should be moved back an extra 300mm to properly accommodate another vehicle in accordance with the relevant policy, and thus count as a second car parking space

DTS/DPF 10.1

Garages and carports facing a street (other than an access lane way):

- (a) are set back at least 0.5m behind the building line of the associated dwelling
- (b)** are set back at least 5.5m from the boundary of the primary street
- (c) have a total garage door / opening width not exceeding 30% of the allotment or site frontage, to a maximum width of 7m.

Detached Dwelling	Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.
	Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.

2. Your plans currently do not indicate a rainwater tank being installed on site, but you will have to make provision for one in accordance with the Stormwater Management Overlay, which requires a 2000L combination tank (1000L retention and 1000L detention);

DTS/DPF 1.1

Residential development comprising detached, semi-detached or row dwellings, or less than 5 group dwellings or dwellings within a residential flat building:

- (a) includes rainwater tank storage:
 - (i) connected to at least:
 - A. in relation to a detached dwelling (not in a battle-axe arrangement), semi-detached dwelling or row dwelling, 60% of the roof area
 - B. in all other cases, 80% of the roof area
 - (ii) connected to either a toilet, laundry cold water outlets or hot water service for sites less than 200m²
 - (iii) connected to one toilet and either the laundry cold water outlets or hot water service for sites of 200m² or greater
 - (iv) with a minimum total capacity in accordance with Table 1
 - (v) where detention is required, includes a 20-25 mm diameter slow release orifice at the bottom

of the detention component of the tank

- (b) incorporates dwelling roof area comprising at least 80% of the site's impervious area

Table 1: Rainwater Tank

Site size (m ²)	Minimum retention volume (Litres)	Minimum detention volume (Litres)
<200	1000	1000
200-400	2000	Site perviousness <30%: 1000 Site perviousness ≥30%: N/A
>401	4000	Site perviousness <35%: 1000 Site perviousness ≥35%: N/A

3. You will also be required to plant a small tree in accordance with the Urban Tree Canopy Overlay:

DTS/DPF 1.1

Tree planting is provided in accordance with the following:

Site size per dwelling (m ²)	Tree size* and number required per dwelling
<450	1 small tree
450-800	1 medium tree or 2 small trees
>800	1 large tree or 2 medium trees or 4 small trees

*refer Table 1 Tree Size

Table 1 Tree Size			
Tree size	Mature height (minimum)	Mature spread (minimum)	Soil area around tree within development site (minimum)
Small	4 m	2m	10m ² and min. dimension of 1.5m
Medium	6 m	4 m	30m ² and min. dimension of 2m
Large	12 m	8m	60m ² and min. dimension of 4m

For what it's worth, overall I think you have made a very good attempt at designing a reasonable dwelling on this site! Given it's constraints I have allowed dispensations in respect of site coverage, boundary development, heights of some walls, and setbacks, but the abovementioned concerns render the application unable to be supported in its current form.

As mentioned earlier in this email, I think it is worth meeting face-to-face to discuss the above concerns once you've had a chance to consider them. I will be leaving the office now for the rest of the afternoon, but I will be back on Monday if you wish to contact me then to arrange a time to meet, or otherwise email me over the weekend when best suits you. At this stage I am free any time on Monday, Tuesday morning, Wednesday or Thursday.

To avoid the assessment clock running down while we go through this process, I am able to place the application on hold to facilitate these amendments if you would like to take up that opportunity? Alternatively, I will have to put the application out on public notification mid-next week in its current form, which obviously runs the risk of receiving representations that may not necessarily be received if amended plans are notified instead. Please let me know ASAP if you would like the application placed on hold.

I look forward to hearing from you, and meeting with you in due course.

Thanks and regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters

175 The Parade, Norwood SA 5067

Telephone 8366 4560

Email kfairbrother@npsp.sa.gov.au

Website www.npsp.sa.gov.au

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Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Sunday, 30 October 2022 5:53 PM
To: Kieran Fairbrother
Subject: RE: Development Application 22033399 - 23 Harris St, Norwood - Concerns with Application

Hi Kieran

Thank you very much for your email.

We would like you to put the application on hold for a time while we discuss it with you and consider other options, thanks.

We have given your comments some consideration and are happy to make some changes and see if we can reach a position that you can support.

We are also free all Tuesday morning and can come to the council any time you suggest.

Many thanks.
Alison and Steve

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Sent: Friday, 28 October 2022 3:52 PM
To: 'alibrookman7@outlook.com' <alibrookman7@outlook.com>; 'brookie1950@hotmail.com' <brookie1950@hotmail.com>
Subject: Development Application 22033399 - 23 Harris St, Norwood - Concerns with Application

Hi Alison and Stephen,

I am the planner with carriage of the abovementioned development application, for the construction of a new dwelling at 23 Harris St, Norwood. I have undertaken a brief assessment of this proposal which has raised some concerns, that I'd like to bring to your attention with the intention of potentially resolving these concerns before the application goes out on public notification.

As you might be aware, this site is located in the *Established Neighbourhood Zone* which has specific Performance Outcomes (PO's) – guided by Designated Performance Features (DPF's) – that provide guidance on what development is anticipated and reasonable within the Zone. I have detailed my concerns below, with reference to the applicable PO's and/or DPF's.

Once you have read and understood my concerns outlined below, I think it would be worthwhile organising a time to meet next week to discuss these, and how I suggest you would be able to overcome them.

Eastern Boundary Wall

The following policies are most pertinent to this element of the proposal:

Established Neighbourhood Zone:

Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 8 November 2022 12:53 PM
To: Kieran Fairbrother
Subject: 23 Harris St DA 22033399
Attachments: John quick idea Nov 22.pdf

Hi Kieran

Thanks for your feedback at our meeting last week, we have been thinking and sketching some more.

Looking mostly at the eastern wall of our building we think we can achieve pretty much what you are after. It is difficult to move everything off the boundary on the first floor even just where the stairs are. In part this is because there is not much room and also as the stairs need to come up somewhere. We cannot fit things well by taking the whole stair off the boundary.

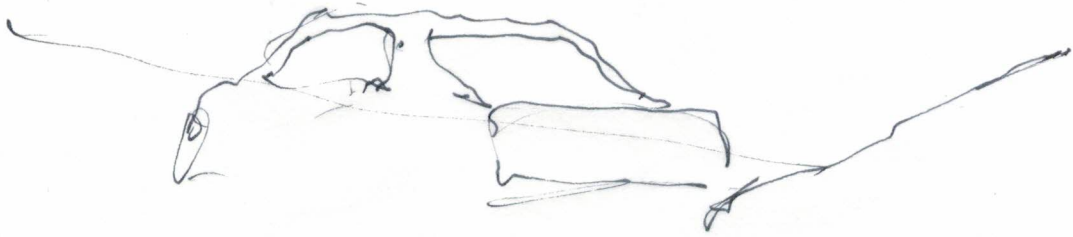
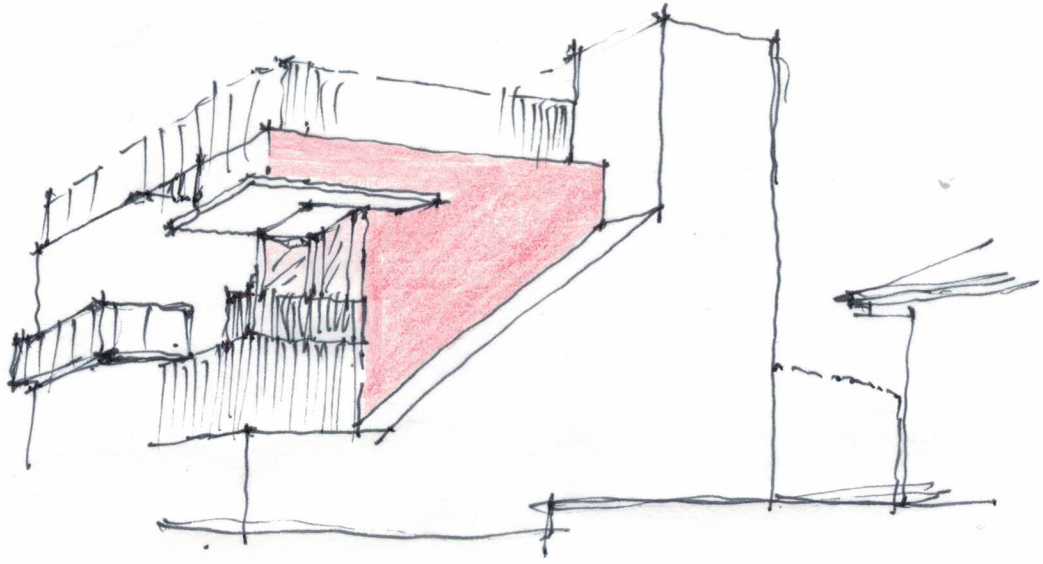
We are thinking of making the roof line of the boundary wall follow the angle of the stairs, and then the main wall of that section can be set back from the boundary. While it may not be as much as 1.5m (not sure yet) it should be at least about 1m.

I attach a rough sketch to give an idea.

We are still looking at the issue of the screens, in particular in the north east area and thinking of constructing them inside the boundary and of another material such as timber slats or similar. We will come back to you on this.

We look forward to hearing any further feedback you may have.

Regards
Alison



Kieran Fairbrother

From: Kieran Fairbrother
Sent: Thursday, 17 November 2022 10:51 AM
To: 'Alison Brookman'
Subject: RE: 23 Harris St DA 22033399

Hi Alison,

Thanks for your patience on this one.

I am comfortable enough with the concept you've provided insofar as the eastern wall is concerned to progress that side of the application. Of course the application will be subject to notification and we are yet to see if the neighbour will have objections with the wall, but I am comfortable enough at this stage to progress it to that stage.

I'll wait to hear more from you regarding the screening, and then we can move from there!

Thanks and regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters

175 The Parade, Norwood SA 5067

Telephone 8366 4560

Email kfairbrother@npsp.sa.gov.au

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From: Kieran Fairbrother
Sent: Monday, 14 November 2022 9:49 AM
To: 'Alison Brookman' <alibrookman7@outlook.com>
Subject: RE: 23 Harris St DA 22033399

Good morning Alison,

Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 6 December 2022 5:44 PM
To: Kieran Fairbrother
Subject: RE: 23 Harris St DA 22033399

Hi Kieran

We have not given up! Thanks for being patient.

We have lately been looking at colour and materials which are of course inter-related. We are conscious that bright white will be glarey and so have been looking at options. In this quest we have looked at a number of display homes and new builds in our area, there is a lot of white! Having said this we are thinking of using a material or paint that is not actually white, matte, but will look off white. We have not decided on anything just yet.

We are also thinking that the eastern wall which is set back from the boundary may be another colour, probably darker or deeper but that is undecided as yet. May give more interest and better aesthetic for that neighbour.

We are also still looking at the screens. We now think we might use Corten, perforated and maybe pleated, but ensuring it has max visibility of 25% (I think that was what we discussed). Also still looking at positioning of these. We feel it will break up the bulk also.

We will get back to you, I just thought I would keep communication open and let you know how we are thinking.

Regards, and hope you have a lovely Christmas.

Alison

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Sent: Thursday, 17 November 2022 10:51 AM
To: 'Alison Brookman' <alibrookman7@outlook.com>
Subject: RE: 23 Harris St DA 22033399

Hi Alison,

Thanks for your patience on this one.

I am comfortable enough with the concept you've provided insofar as the eastern wall is concerned to progress that side of the application. Of course the application will be subject to notification and we are yet to see if the neighbour will have objections with the wall, but I am comfortable enough at this stage to progress it to that stage.

I'll wait to hear more from you regarding the screening, and then we can move from there!

Thanks and regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters

Kieran Fairbrother

From: Kieran Fairbrother
Sent: Wednesday, 7 December 2022 9:58 AM
To: 'Alison Brookman'
Subject: RE: 23 Harris St DA 22033399

Good morning Alison,

Thanks for your email.

This application can sit on hold for another 6 months if that's what you need, so don't stress! It's good to hear you're working towards a solution still, and the Corten screens sound ok providing they are set off the boundary as discussed and transparency is limited to 25% (you are right).

I hope you have a great Christmas too, thank you, and I look forward to hearing more about this project in the New Year 😊

Thanks and regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
175 The Parade, Norwood SA 5067
Telephone 8366 4560
Email kfairbrother@npsp.sa.gov.au
Website www.npsp.sa.gov.au

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From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 6 December 2022 5:44 PM
To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Subject: RE: 23 Harris St DA 22033399

Hi Kieran

Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 14 March 2023 11:38 AM
To: Kieran Fairbrother
Subject: RE: 23 Harris St ID22033399
Attachments: sketches for Kieran.pdf

Hi Kieran

Attached are some sketches showing some of our latest ideas for our meeting on Thursday. Sorry for not sending sooner.

John Maitland has suggested -

- An idea is to clad the 'feature' coloured wall with a profiled colorbond metal sheet – [as a ventilated wall – reducing heat on that face which would be insulated and have a thermal mass internal skin]. The roof over the stair then in matching coloured metal skin – so the overall 'indented' section be coloured the same. Not including the elevator wall ... I believe this system would create a long term 'maintenance free' energy efficient skin with a strong visual impression. The colour ought to be strong.

We are thinking Colorbond 'Deep Ocean', I have colour samples.

I also attach a photo I saw in a magazine which is a solar panel carport but gives the idea of a light structure just for the panels.

See you on Thursday.

Alison

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Sent: Tuesday, March 7, 2023 8:22 AM
To: 'Alison Brookman' <alibrookman7@outlook.com>
Subject: RE: 23 Harris St ID22033399

Hi Alison,

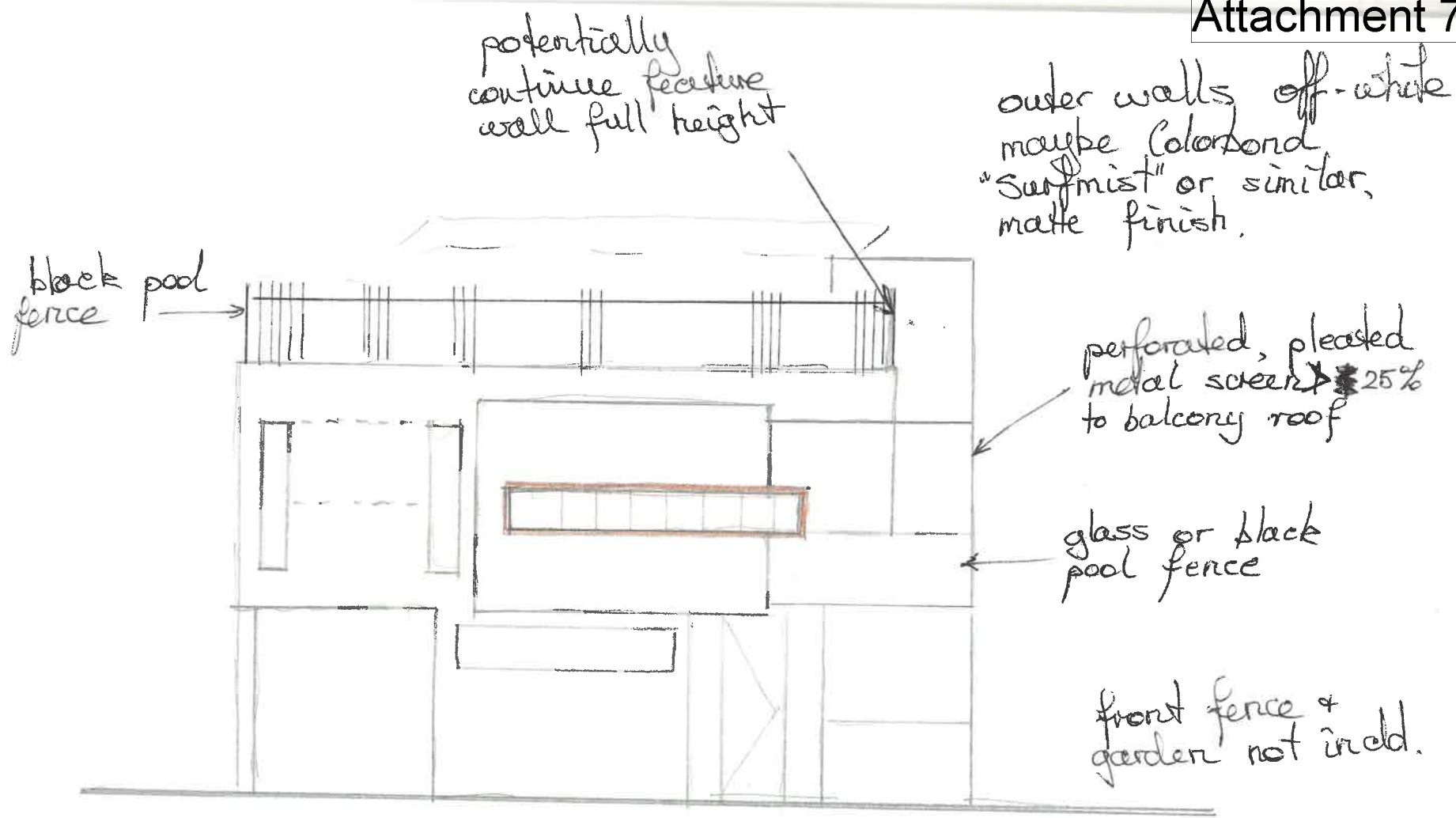
You should've just received an invite from me for 10am on Thursday.

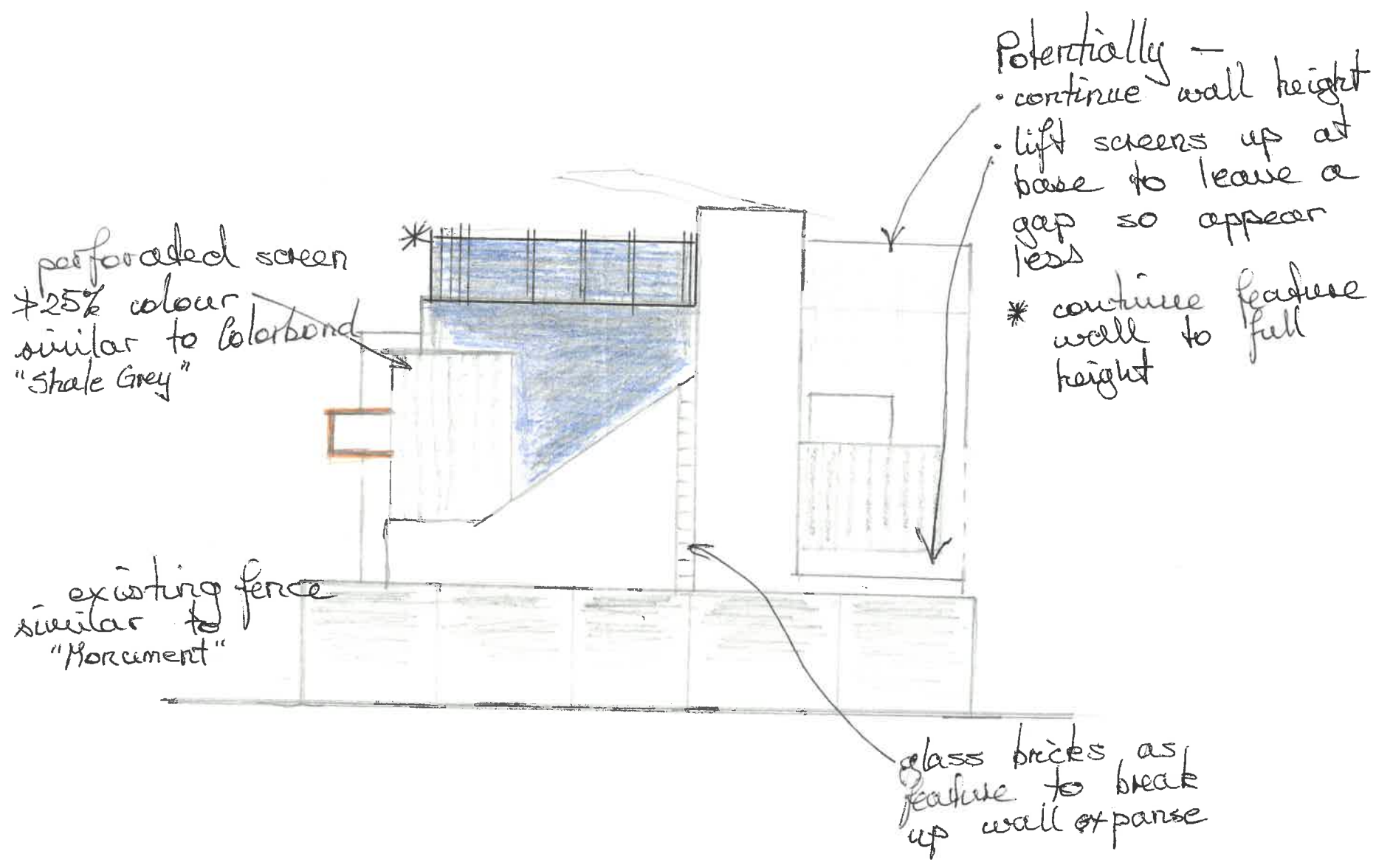
Prior to the meeting, would you mind emailing me some brief detail on the changes etc you'd like to discuss, so I can be better prepared for our meeting?

Thanks and regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
175 The Parade, Norwood SA 5067
Telephone 8366 4560
Email kfairbrother@npsp.sa.gov.au
Website www.npsp.sa.gov.au





Kieran Fairbrother

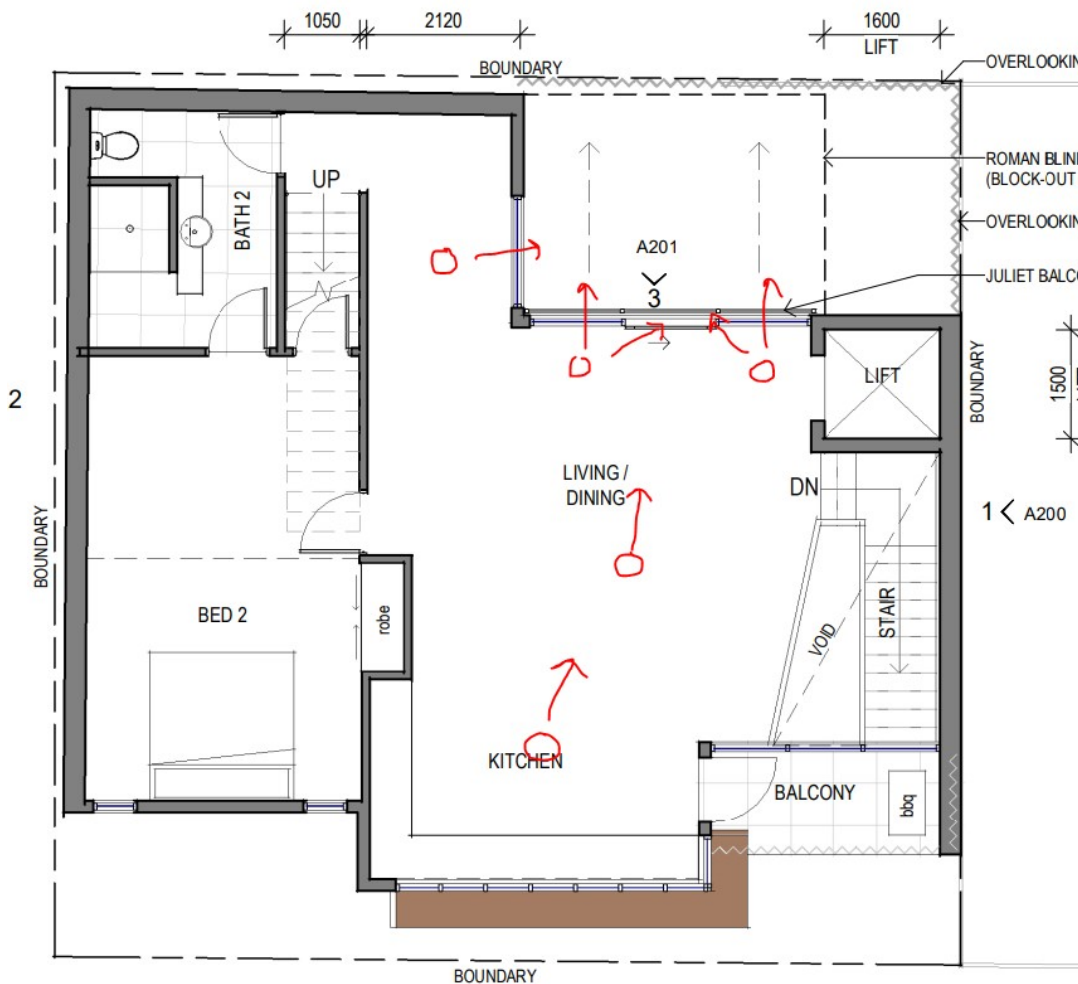
From: Kieran Fairbrother
Sent: Friday, 17 March 2023 8:20 AM
To: 'Alison Brookman'
Subject: RE: 23 Harris St ID22033399

Hi Alison,

I had a bit more of a think about our meeting yesterday and your proposal, and wanted to provide you with some additional thoughts.

With respect to the east-facing portions of the rooftop garden, you raised the possibility of extending the dwelling wall higher in lieu of 1.7m screening. After further consideration, I think the screening would be a better option and would present better to the street and the neighbour through both further articulation of materials and colours but also by allowing more of the rooftop landscaping to be visible; both to the street and the neighbours.

Also, with the sightlines I requested for the proposed rear screening, can I request that you provide sightlines at different positions within the upstairs living areas, as per below for example:



Let me know if you have any questions or queries.

Thanks and regards,

Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 28 March 2023 3:45 PM
To: Kieran Fairbrother
Subject: 23 Harris

Hi Kieran

Quick question. What do you think about the west elevation, can we continue the wall up or have the black pool fence instead of the screen on that boundary? we are happy with whatever you think.

Arranging updated plans and sightlines now.

Cheers
Alison

Kieran Fairbrother

From: Kieran Fairbrother
Sent: Wednesday, 29 March 2023 8:55 AM
To: 'Alison Brookman'
Subject: RE: 23 Harris

Hi Alison,

Thanks for your email.

I am of the view that the western wall (and for clarity the whole rooftop level) should be distinguished from the balance of the building through the use of a perforated/pool-type screening, rather than continuing the wall further up.

I hope that helps.

Thanks and regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

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From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 28 March 2023 3:45 PM
To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Subject: 23 Harris

Hi Kieran

Kieran Fairbrother

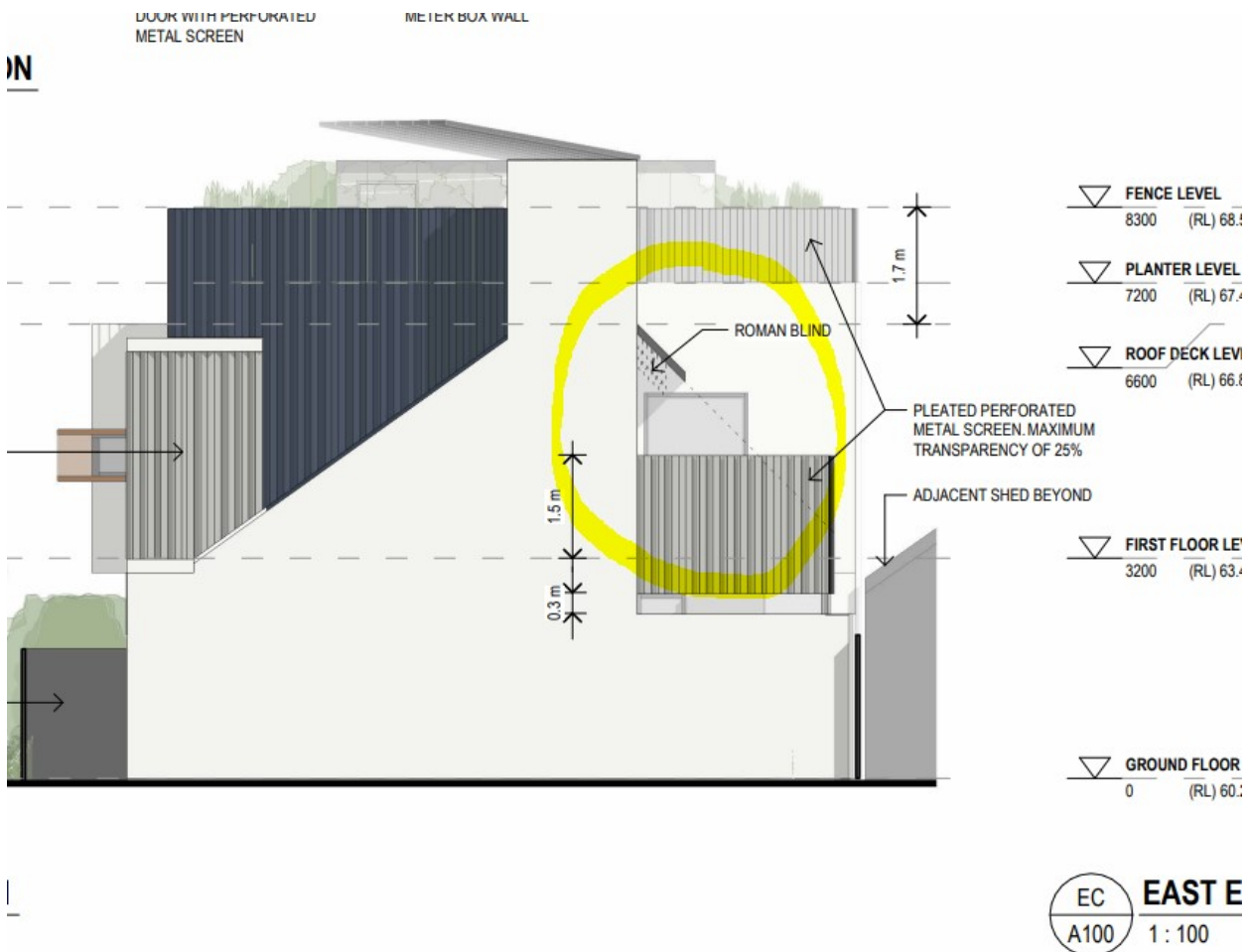
From: Kieran Fairbrother
Sent: Friday, 28 April 2023 3:51 PM
To: 'Alison Brookman'
Subject: RE: next set of plans

Hi Alison,

Thanks for getting in touch – I hope you've been well!

I have no issue with the FFL being close to the footpath level.

With respect to the screening, I think the extent of views possible with the configuration provided in the drawings is acceptable, any more transparency could be problematic though. I've also just thought too that it might be worth considering the amount of sunlight you're likely to receive in the upper-floor living area – and probably something worth discussing with the designer who would know best. I'm just wondering if the screening and the roman blind might impact the amount of sunlight received into these rooms during summer (It might be a non-issue but something I thought I'd raise).



Otherwise, I am happy with how things are progressing. In terms of time frames, an application can remain on hold for up to 12 months, which gives you until October this year – so you've got plenty of time!

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Monday, 5 June 2023 3:13 PM
To: Kieran Fairbrother
Subject: Fwd: 23 Harris Street, Norwood: Planning Drawings

Hi Kieran from the Scottish highlands!

Attached is the latest set of plans we hope are final. One change to note is -

- at the roof level we have gone for pool-type fencing all the way around. Behind this on east, north and west sides is polycarbonate sheets to block overlooking.

Depending on any other comments you may have, we are ready to proceed with the application. Will you please let me know if we need to do anything more. We return at the end of June but can still access emails.

Many thanks
Alison

Get [Outlook for](#)

Hi Alison and Steve

Please find attached the latest set of drawings for you to look over and hopefully lodge for planning approval.

Get back to me if there is anything I have missed or you would like changed before lodgement, otherwise let me know of any requests for further information from the planner.

Regards

Andrew Davidson
Building Designer

A.D.Design
Building Design and Drafting

Ph 0410 818 518
Email admin@addesign.com.au

Kieran Fairbrother

From: Kieran Fairbrother
Sent: Monday, 19 June 2023 1:44 PM
To: 'Alison Brookman'
Subject: RE: 23 Harris Street, Norwood: Planning Drawings

Hi Alison,

I trust you're enjoying the remainder of your holiday!

I have just had a quick review of the amended set of plans and can advise that I am supportive of the proposal with the exception of one thing: the monochromatic colour/material palette for the front façade.

I have raised this previously, but there should be a mixture of materials within the front façade and some mix of colours too. Would you like to address this before I take the application off hold and notify the application?

No rush for you to respond either; enjoy your holiday while you can!

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
 175 The Parade, Norwood SA 5067
Telephone 08 8366 4560
Email kfairbrother@npsp.sa.gov.au
Website www.npsp.sa.gov.au

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From: Kieran Fairbrother
Sent: Friday, 16 June 2023 3:41 PM
To: 'Alison Brookman' <alibrookman7@outlook.com>
Subject: RE: 23 Harris Street, Norwood: Planning Drawings

Hi Alison,

I am sorry but this week has gotten away from me before I have had time to review your plans. I will do these on Monday for you and provide you with my comments before COB that day – I trust that's ok with you!

Apologies again – hope you're enjoying your trip!

Regards,

Kieran Fairbrother

From: Alison Brookman <alibrookman7@outlook.com>
Sent: Monday, 10 July 2023 4:15 PM
To: Kieran Fairbrother
Cc: 'brookie1950@hotmail.com'
Subject: 23 Harris - front facade
Attachments: 20230710_160040.jpg; 20230710_160051.jpg

Hi Kieran

Attached are a couple of photos of my colouring-in. I was trying to think of colours to complement the design but also give some contrast and interest.

In the end we prefer the flip around of colouring the rest of the house and having the front protruding part off-white (probably to look white but not blinding).

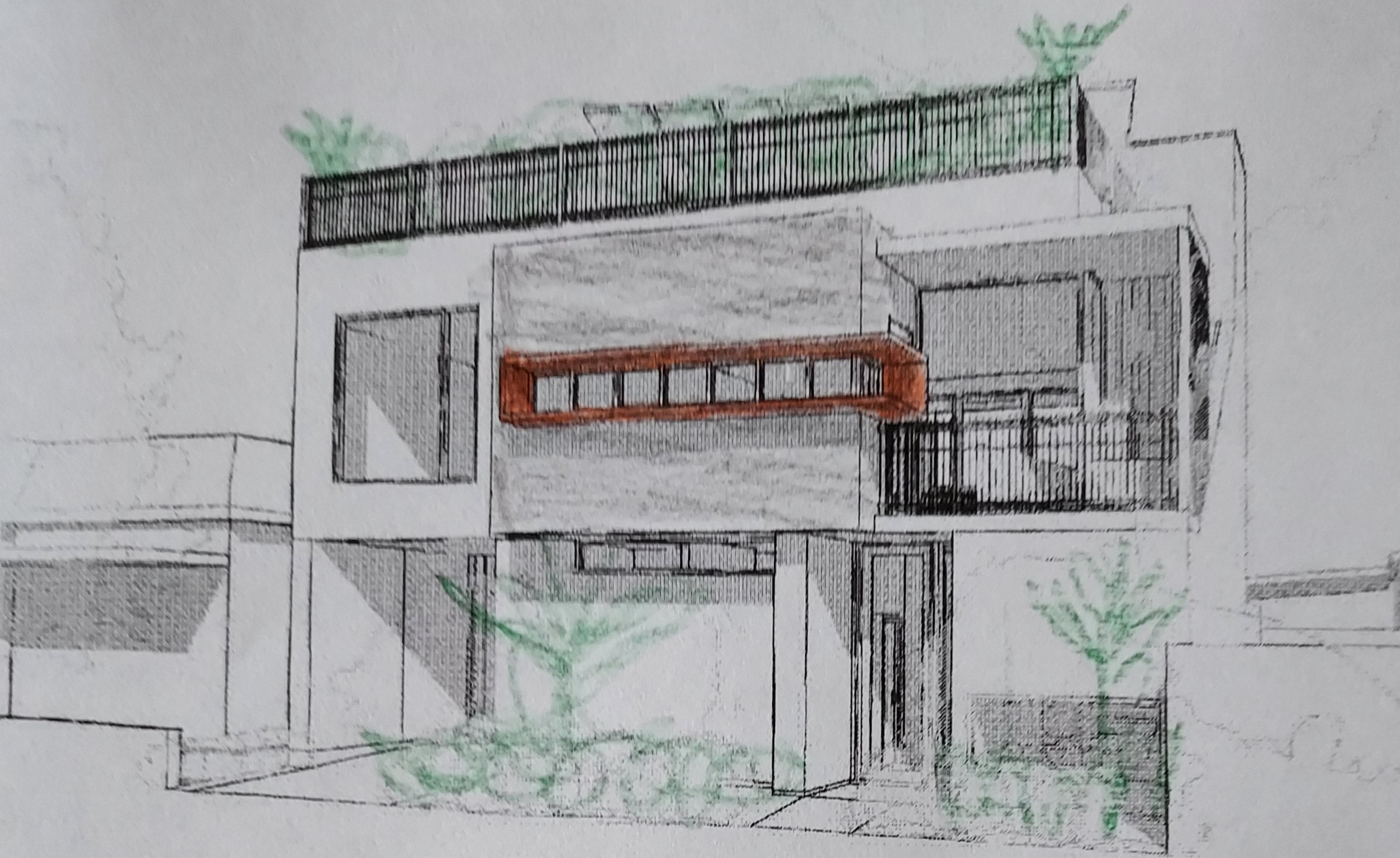
Here is my reasoning:

- I don't really like the colour grey, but tried to show it.
- Thought about the 'Deep Ocean' that we have on the eastern side, but don't like it
- When looking at the 3D images I noticed the western wall and thought it could be rather reflective/glarey. Maybe that should be painted also?
- Then I thought the rest of the house could be painted a colour. We still want it light so it is heat reflective not absorbing, but maybe something like Colorbond 'Evening Haze' or 'Dune'. Using Colorbond colours for ease of reference more than anything but can commit to those colours or similar (might depend on branding of paint).

We like the idea of the front protruding window being a highlight along with its Corten perimeter and if this was white but the rest coloured think it would look smart.

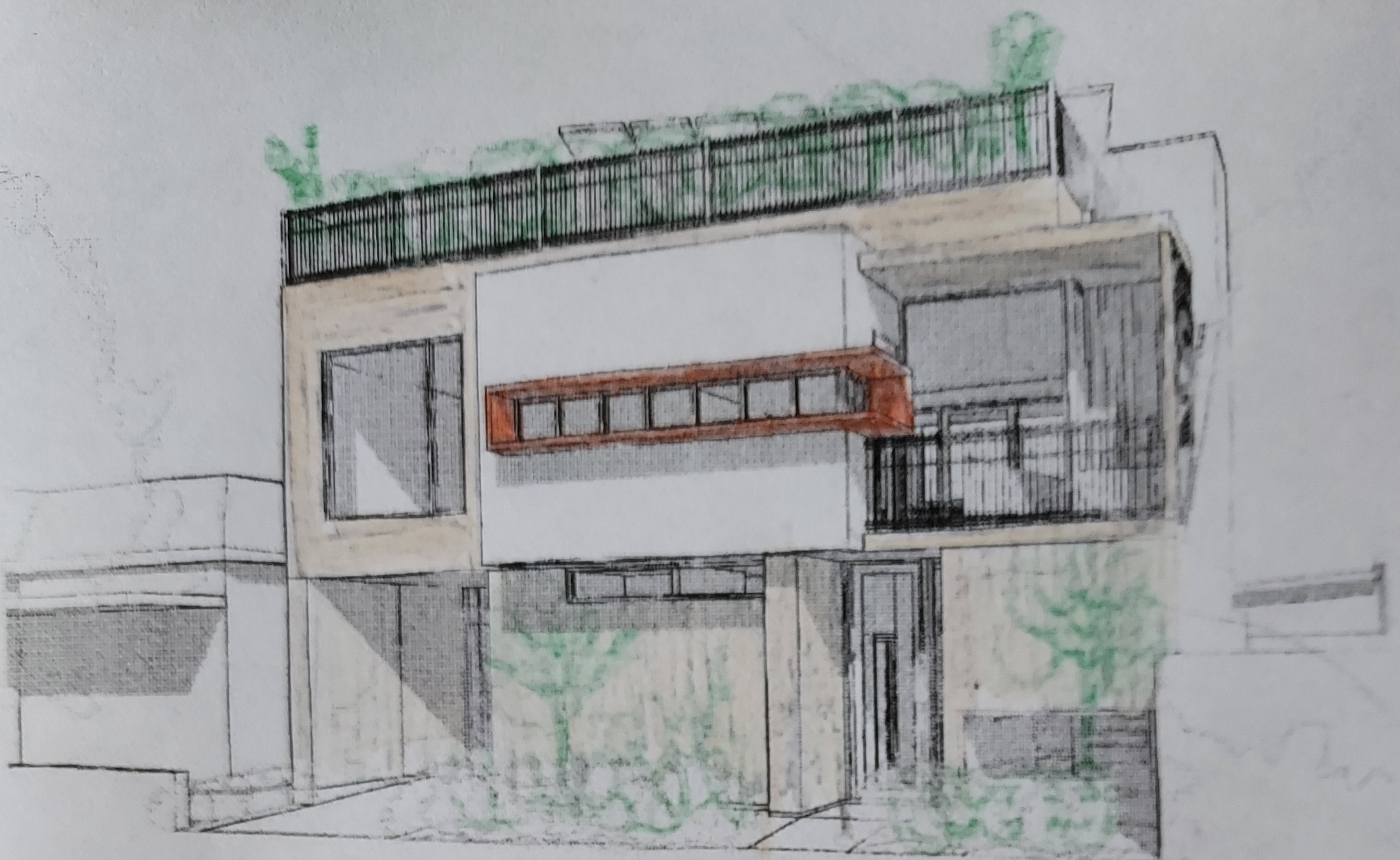
Look forward to hearing what you think before we get the plans to show it.

Regards
Alison



Sheet #
A000
A011
A100
A120
A200
A201
A700
A701
A702
A703
A704
A705

ISSUED FOR PLANNING CONSENT



Sheet
A00
A01
A10
A12
A20
A25
A70
A75
A76
A77
A78

ISSUED FOR PLANNING CONSENT

Project
NEW
For
STEVE



Kieran Fairbrother

From: Kieran Fairbrother
Sent: Thursday, 27 July 2023 8:12 AM
To: 'Alison Brookman'
Subject: RE: 23 Harris - front facade

Hi Alison,

I am happy with a note that says 'evening haze or similar'. Just make sure this note is also on the 2d elevations as well as the 3d perspectives please (you don't have to colour the elevations if you don't want, although it might help when the application goes out on public notification for the public to see).

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
 175 The Parade, Norwood SA 5067
Telephone 08 8366 4560
Email kfairbrother@npsp.sa.gov.au
Website www.npsp.sa.gov.au

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From: Alison Brookman <alibrookman7@outlook.com>
Sent: Wednesday, 26 July 2023 9:15 PM
To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Subject: RE: 23 Harris - front facade

Thanks Kieran that is great.

Yes we have that same preference, with the protrusion white. I will get Andrew to just change the 3D images to show the colour as 'evening haze' or similar. Do you want us to commit to a colour? I will see if I can find an actual house with that colour so we can, I know it will look lighter once done but we don't want it to be any darker than evening haze if possible.

Cheers
 Alison

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Sent: Wednesday, July 26, 2023 9:56 AM

To: 'Alison Brookman' <alibrookman7@outlook.com>

Subject: RE: 23 Harris - front facade

Hi Alison,

I just discussed with my team and we all agree that the deep ocean and grey are not good. We'd be happy with either of the other two options – using 'dune' or 'evening haze' – but if I was to choose one it is to leave the protrusion in white and change the balance of the building (which I think was your preference also).

I hope that helps 😊

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
175 The Parade, Norwood SA 5067
Telephone 08 8366 4560
Email kfairbrother@npsp.sa.gov.au
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From: Alison Brookman <alibrookman7@outlook.com>

Sent: Monday, 24 July 2023 9:35 AM

To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>

Subject: Re: 23 Harris - front facade

Thanks Kieran

No problems at all and we can of course wait a few more days, interested to hear if anyone has any comments.

Cheers

Alison

Get [Outlook for Android](#)

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>

Sent: Monday, July 24, 2023 9:19:28 AM

To: 'Alison Brookman' <alibrookman7@outlook.com>

Subject: RE: 23 Harris - front facade

Hi Alison,

Sorry I didn't get back to you – our meeting that week got cancelled and I forgot to discuss it last week with my team. I can do it this Wednesday if you're able to hold out a couple more days?

And to be honest I wouldn't worry about updating your written submission; I am fairly supportive of the proposal anyway and there really isn't anything worthwhile you might add, respectfully.

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
 175 The Parade, Norwood SA 5067
Telephone 08 8366 4560
Email kfairbrother@npsp.sa.gov.au
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From: Alison Brookman <alibrookman7@outlook.com>
Sent: Saturday, 22 July 2023 12:49 PM
To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Subject: RE: 23 Harris - front facade

Hi Kieran

I wonder if you have any more comments, or if we should 'nail our colours to the mast' and then you will have something definite to assess?

I have not reviewed my written submission but will do so in case I need to supplement it, or address the changes we made. I think I should do that.

Look forward to hearing from you.

Cheers, Alison

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Sent: Tuesday, July 11, 2023 12:15 PM
To: 'Alison Brookman' <alibrookman7@outlook.com>
Cc: 'brookie1950@hotmail.com' <brookie1950@hotmail.com>
Subject: RE: 23 Harris - front facade

Hi Alison,

No worries – if we get the colour scheme right I think we'll be fine. I'll discuss this with my colleagues tomorrow when we have our weekly meeting and I'll let you know our collective thoughts.

And I don't think it's worth changing anything with the garage – garages should be discrete structures anyway.

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
 175 The Parade, Norwood SA 5067
Telephone 08 8366 4560
Email kfairbrother@npsp.sa.gov.au
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From: Alison Brookman <alibrookman7@outlook.com>
Sent: Tuesday, 11 July 2023 11:44 AM
To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Cc: 'brookie1950@hotmail.com' <brookie1950@hotmail.com>
Subject: RE: 23 Harris - front facade

Hi again Kieran

Thanks for that. We had thought briefly about stone or another cladding but don't really want to. We would rather achieve an outcome with colour if possible.

Having said this, we thought of the garage door, at the moment shown as the grey-ish perforated metal. It is not a big part but we could do this in another material, perhaps something that ties in with the Corten. Realising that it is set back under the upper floor anyway.

I also want to assure you that we are committed to planting 2 small trees in the front yard, say crepe myrtle or crab apple or similar which should help also.

Cheers
 Alison

From: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Sent: Monday, July 10, 2023 4:51 PM
To: 'Alison Brookman' <alibrookman7@outlook.com>
Cc: 'brookie1950@hotmail.com' <brookie1950@hotmail.com>
Subject: RE: 23 Harris - front facade

Hi Alison,

Thanks for sending those through. I agree re: the 'deep ocean' and the grey colour – they don't look great.

I'll have a think on the other idea re: painting the balance of the building and leaving the protrusion in white.

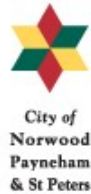
Out of interest, have you considered any other materials for the protrusion/facade? Or would changing an element to brick/stone affect the environmental performance of the building that you're seeking to achieve (notwithstanding cost differences etc)?

Regards,

Kieran Fairbrother
SENIOR URBAN PLANNER

City of Norwood Payneham & St Peters
 175 The Parade, Norwood SA 5067
Telephone 08 8366 4560
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From: Alison Brookman <alibrookman7@outlook.com>
Sent: Monday, 10 July 2023 4:15 PM
To: Kieran Fairbrother <KFairbrother@npsp.sa.gov.au>
Cc: 'brookie1950@hotmail.com' <brookie1950@hotmail.com>
Subject: 23 Harris - front facade

Hi Kieran

Attached are a couple of photos of my colouring-in. I was trying to think of colours to complement the design but also give some contrast and interest.

In the end we prefer the flip around of colouring the rest of the house and having the front protruding part off-white (probably to look white but not blinding).

Here is my reasoning:

- I don't really like the colour grey, but tried to show it.
- Thought about the 'Deep Ocean' that we have on the eastern side, but don't like it
- When looking at the 3D images I noticed the western wall and thought it could be rather reflective/glarey. Maybe that should be painted also?
- Then I thought the rest of the house could be painted a colour. We still want it light so it is heat reflective not absorbing, but maybe something like Colorbond 'Evening Haze' or 'Dune'. Using Colorbond colours for ease of reference more than anything but can commit to those colours or similar (might depend on branding of paint).

We like the idea of the front protruding window being a highlight along with its Corten perimeter and if this was white but the rest coloured think it would look smart.

Look forward to hearing what you think before we get the plans to show it.

5.2 DEVELOPMENT NUMBER 23014875 – ANDREW CAMPBELL LLOYD & ALANA CAMPBELL LLOYD – 120 RUNDLE STREET, KENT TOWN

DEVELOPMENT NO.:	23014875
APPLICANT:	Andrew Campbell Lloyd Alana Campbell Lloyd
ADDRESS:	120 RUNDLE ST KENT TOWN SA 5067
NATURE OF DEVELOPMENT:	Two-level alterations and additions to an existing Local Heritage Place (comprising the expansion of existing dwelling and new consulting rooms)
ZONING INFORMATION:	<p>Zones:</p> <ul style="list-style-type: none"> • Business Neighbourhood <p>Overlays:</p> <ul style="list-style-type: none"> • Airport Building Heights (Regulated) • Future Road Widening • Historic Area • Heritage Adjacency • Hazards (Flooding - General) • Local Heritage Place • Prescribed Wells Area • Regulated and Significant Tree • Traffic Generating Development • Urban Transport Routes <p>Technical Numeric Variations (TNVs):</p> <ul style="list-style-type: none"> • Maximum Building Height (Levels) (Maximum building height is 2 levels)
LODGEMENT DATE:	8 June 2023
RELEVANT AUTHORITY:	Assessment panel
PLANNING & DESIGN CODE VERSION:	8 June 2023
CATEGORY OF DEVELOPMENT:	Code Assessed - Performance Assessed
NOTIFICATION:	Yes
RECOMMENDING OFFICER:	Mark Thomson Consultant Planner
REFERRALS STATUTORY:	Nil
REFERRALS NON-STATUTORY:	Heritage Advisor Traffic Engineer

CONTENTS:

APPENDIX 1:	Relevant P&D Code Policies	ATTACHMENT 5:	Representations
ATTACHMENT 1:	Application Documents	ATTACHMENT 6:	Response to Representations
ATTACHMENT 2:	Subject Land Map	ATTACHMENT 7:	Internal Referral Advice
ATTACHMENT 3:	Zoning Map		
ATTACHMENT 4:	Representation Map		

DETAILED DESCRIPTION OF PROPOSAL:

The existing building is currently used as consulting rooms at ground level and a dwelling at upper level, pursuant to the Development Approval granted in 2022.

The proposal comprises the construction of a two level addition to the rear of the existing building, replacing an existing single level lean-to.

At ground level, only 8m² of the proposed addition is associated with the existing consulting rooms. After taking into account the demolition of the lean-to and increased usable space resulting from relocating the stairs, the proposal results in a small reduction in consulting room floor area of 4m². No change to the number of consulting rooms is proposed; this is to remain at 1 consulting room.

Also proposed at ground level is a garage, mud room, entry and stairs; all of which is associated with the dwelling.

At the upper level, the proposed addition comprises a stair landing, and master bedroom suite. In total (ground and upper level combined), the addition increases the dwelling floor area by approximately 120m².

Changes to the front elevation of the building include the demolition of the western upper level windows, and replacing them with a pair of French doors, and the replacement of the cast iron balcony balustrade with a new balustrade reusing the cast iron elements.

Aside from two car parking spaces proposed within the garage to be associated with the dwelling, two patient car parking spaces are proposed.

SUBJECT LAND & LOCALITY:

Site Description:

Location reference: 120 RUNDLE ST KENT TOWN SA 5067

Title ref.: CT 5427/463 **Plan Parcel:** D46592 AL51 **Council:** THE CITY OF NORWOOD PAYNEHAM AND ST PETERS

The subject land is mostly rectangular with a width of 16.42m and depth of 24.06m, however a dog-leg part-way along the north-eastern side boundary increases the width of the front 13.2m of the allotment by 600mm, giving this section a width of 17.02m. The land is essentially flat in topography.

The existing building on the land is a Local Heritage Place. It appears to have had a range of uses. Council records include an approval to use the building as a coffee lounge and residence in 1984 and later the same year another approval to “use existing coffee lounge / residence as licensed restaurant / residence”. No further development authorisations exist on Council records until 2022 when approval was given to change the use of the land from consulting rooms at both ground and first floor levels (5 practitioners total) to consulting rooms (1 practitioner) at ground level and a dwelling at first floor level.

The Development Approval which was granted in 2022 has been implemented, with the single consulting room being used by Dr Andrew Campbell-Lloyd, a plastic surgeon specialising in reconstruction, supported by one staff member and conducted within the hours of 9 AM to 4 PM Monday to Friday.

Four on-site car parking spaces exist at the rear of the subject land, shared amongst the consulting room and dwelling.

Locality

The locality of the subject land contains a mix of commercial and residential land uses, generally utilising historic buildings.

Adjoining the subject land to the south-west is a two level Local Heritage Place building containing office uses at ground level and dwellings at upper level.

Adjoining the subject land to the east and south is the site of a two level office building occupied by Kojo marketing agency at 31 Fullarton Road.

Adjacent the subject land on the opposite side of Rundle Street is a large two level office building occupied by Fairmont Homes on the corner of Fullarton Road and various other smaller office uses.

Many of the buildings in the locality are from the pre 1890s (Victorian) era, resulting in a strong historic built form theme.

CONSENT TYPE REQUIRED:

Planning Consent

CATEGORY OF DEVELOPMENT:

- **PER ELEMENT:**
Dwelling addition: Code Assessed - Performance Assessed
Consulting room: Code Assessed - Performance Assessed
- **OVERALL APPLICATION CATEGORY:**
Code Assessed - Performance Assessed
- **REASON**
P&D Code

PUBLIC NOTIFICATION

- **REASON**
P&D Code
The proposal involves the construction of boundary walls that exceed 3.2m in height.

- **LIST OF REPRESENTATIONS**

Given Name	Family Name	Address	Position on Application	Wishes to be Heard
George	Samaras	PO Box 7434, Halifax Street, Adelaide	Opposed	Yes

-

• SUMMARY

Mr Samaras' representation was prepared by a Planning Consultant, Mr Andrew Humby of Humby Consulting. Mr Humby raised concerns on behalf of Mr Samaras, which are summarised as follows:

- reduction in access to natural light and ventilation for the occupants of 31 Fullarton Road
- overshadowing of outdoor courtyard at 31 Fullarton Road
- overshadowing of west-facing windows on the lower level of the building
- detrimental visual impact on the occupants of 31 Fullarton Road
- impact of proposed building work on a Regulated Tree at 31 Fullarton Road
- adequacy of car parking (if additional consulting rooms are proposed)
- on-site manoeuvring (if additional consulting rooms are proposed)

The applicant has responded to the representation by Mr Samaras, including a written response prepared by Mr Phillip Brunning of Phillip Brunning & Associates, an Arborist report by Adelaide Tree Surgery and an amended set of plans which include an overshadowing analysis. Of particular note, Mr Brunning has advised:

- no additional consulting rooms are proposed. There will continue to be one practitioner
- shadowing will fall predominantly over the car parking area to the rear of the adjoining property
- the adjoining property to the south is not residential in nature
- Adelaide Tree Surgery have advised the proposal will not impact upon the Regulated Tree
- The on-site parking and manoeuvring arrangements have been amended and vehicles can leave in a forward direction
- Some flexibility in on-site parking provision is afforded in the code in situations such as an adaptive reuse of a heritage place, mixed-use developments and where on-street parking is available.

AGENCY REFERRALS

Nil

INTERNAL REFERRALS

- Heritage Advisor
- Traffic Engineer

PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Planning & Design Code, which are contained in Appendix One.

Land Use

The proposal does not involve any change to the use of the land, nor to the intensity of use of the land.

With respect to the consulting room, the proposal results in a small decrease in floor area and the same number of consulting rooms (one).

With respect to the dwelling, whilst the proposal increases the accommodation by adding a bedroom, it remains a single dwelling.

Both dwellings and consulting rooms are listed as desired land uses in Designated Performance Feature 1.1 of the Business Neighbourhood Zone.

Performance Outcomes 1.2 and 1.3 of the Business Neighbourhood Zone and their associated Designated Performance Features, seek to generally limit the floor area of commercial uses, including consulting rooms, to 250m². With the proposed slight reduction in floor area, the consulting room premises would have a floor area of 126m².

There is no density or size criteria for dwellings within the Business Neighbourhood Zone. Performance Outcome 1.1 seeks "*Housing and accommodation types appropriate to the locality*". The proposal does not alter the type of housing or accommodation on the land.

Building Height

Performance Outcome 3.1 of the Business Neighbourhood Zone states:

"Buildings are generally of low-rise construction, with taller buildings positioned towards the centre of the zone and away from any adjoining neighbourhood-type zone to positively contribute to the built form character of the locality."

The Code defines low-rise as up to two levels and consistent with this definition, Designated Performance Feature 3.1 contains a maximum height criteria of 2 levels. The proposal is consistent with this, with the addition containing two levels.

Performance Outcome 2.2 of the Historic Area Overlay states:

"Development is consistent with the prevailing building and wall heights in the historic area."

In relation to height, the Historic Area Statement identifies the attributes of the historic area as being "*generally up to two storeys*". The proposed two level addition is therefore consistent with the height policy of the Historic Area Overlay.

Boundary Walls and Overshadowing

The proposal includes an 8.8m long boundary wall on the north-eastern side boundary and a 7.1m long boundary wall on the south-eastern (rear) boundary. Both walls are 7.3m high.

Performance Outcome 3.4 of the Business Neighbourhood Zone states (my emphasis):

"Walls on boundaries are limited in height and length to manage visual and overshadowing impacts on adjoining residential properties."

Accordingly, the zone does not seek to manage visual or overshadowing impacts to neighbouring commercial properties resulting from boundary walls.

Similarly, Performance Outcomes 3.1 and 3.2 of the Interface Between Land Uses section of the General Development Policies only seek to manage overshadowing impacts to residential properties. They respectively state (my emphasis):

“Overshadowing of habitable room windows of adjacent residential land uses in:

- a) a neighbourhood-type zone is minimised to maintain access to direct winter sunlight
- b) other zones is managed to enable access to direct winter sunlight.”

and

“Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in:

- a) a neighbourhood type zone is minimised to maintain access to direct winter sunlight
- b) other zones is managed to enable access to direct winter sunlight.”

Therefore, the concern raised by the owner of the adjoining property at 31 Fullarton Road, that the proposal will reduce sunlight access to a courtyard and office windows, is not supported by policy contained in the Code.

The representor’s Planning Consultant has opined that the boundary walls deviate from the “*intent*” of Performance Outcomes 3.6 and 3.7. These two policies relate to the setback of buildings from side and rear boundaries. These policies do not apply to boundary walls as there is a separate policy (PO 3.4) which expressly deals with boundary walls, as previously discussed.

The suggestion from the representor’s Planning Consultant seems to be that PO’s 3.6 and 3.7 ‘intend’ for all walls to be set back from side and rear boundaries, despite there being a separate policy which expressly allows for boundary walls (and in the case of boundary walls adjacent to commercial properties, does not seek to manage visual or overshadowing impacts of those walls). Respectfully, a more logical interpretation of the Code is that it contains policies to deal with the setback of buildings from side and rear boundaries but allows for some walls to be located on boundaries and the assessment of the impacts of such boundary walls should occur against that directly relevant policy.

Appearance and Heritage

Performance Outcome 2.1 of the Local Heritage Place Overlay states:

“Alterations and additions complement the subject building and are sited to be unobtrusive, not conceal or obstruct heritage elements and detailing, or dominate the Local Heritage Place or its setting.”

Also relevant is Performance Outcome 2.2 which states:

“Adaptive reuse and revitalisation of Local Heritage Places to support their retention in a manner that respects and references the original use of the Local Heritage Place.”

The application has been reviewed by the Council’s Heritage Advisor, David Brown, who in summary has advised:

- The proposed addition is an elegant simple design with good visual and physical separation from the original building.
- The addition is set at the rear, borrows from the form, and heights of the existing building, and does not have a major impact on the historic fabric of the Local Heritage Place.
- The change from slim sash windows on the front to slim French doors built in a style to match the old windows is understandable to allow access to the balcony. The changeover of this element will not have a detrimental impact on the heritage value of the building. The new balcony reuses the important historic cast iron, so is a good outcome.

- It is likely that the loss of the original stair, if it is in good condition, is not an ideal outcome, but it is debatable as to whether it would impact the heritage value of the place.
- Overall the proposed addition is an acceptable outcome in terms of the impact on the Local Heritage Place and the streetscape.

Performance Outcome 3.1 of the Historic Area Overlay has similar wording to PO 2.1 of the Local Heritage Place Overlay, stating:

“Alterations and additions complement the subject building, employ a contextual design approach and are sited to ensure they do not dominate the primary façade.”

Having regard to the comments of the Heritage Advisor, this policy is considered to be satisfied.

At the zone level, Performance Outcomes 2.1, 2.2 and 2.3 of the Business Neighbourhood Zone relate to built form and character and respectively state:

“Buildings are of a scale and design that complements surrounding built form, streetscapes and local character.”

“Development provides attractive landscaping to the primary street frontage.”

and

“Site coverage is limited to provide space for landscaping, open space and pervious areas.”

Surrounding built form is all two storey and is typically larger in floor area than the subject building. The scale resulting from the proposed addition is therefore complementary to surrounding built form, consistent with PO 2.1. Having regard to the comments of the Heritage Advisor, the style is also complementary.

With respect to PO 2.2, the proposal includes landscaping to the primary street façade, forward of the entry gate. This area is currently landscaped, however the plans show an increase in the amount of landscaping in this area. An existing landscape bed alongside the north-western side boundary is proposed to be removed to make way for two parallel parking spaces. As this is behind an existing gate, it has no significant impact on streetscape character and is not inconsistent with PO 2.2 which seeks landscaping of the primary street façade.

With respect to PO 2.3, the associated Designated Performance Feature (DPF 2.3) seeks a maximum site coverage of 60%. The proposal is consistent with this, with 52% site coverage.

Traffic Impact, Access and Parking

According to the rates in *Table 2 Off-Street Car Parking Requirements in Designated Areas*, the minimum number of spaces for all non-residential land uses is 3 spaces per 100m² of gross leasable floor area. This equates to 3.75 spaces for the consulting rooms component of the proposal.

Neither *Table 2 Off-Street Car Parking Requirements in Designated Areas*, nor *Table 1 - General Off-Street Car Parking Requirements*, contain a car parking rate applicable to the subject dwelling. The closest ‘fit’ is considered to be the rate provided for the residential component of a multi-storey building, despite the fact that the Business Neighbourhood Zone is not listed as an applicable zone for that rate. In particular, the rate specified is 1 space for a 2 bedroom dwelling and 1.25 spaces for a 3 or more bedroom dwelling and in both instances an additional 0.25 spaces per dwelling is specified for visitor parking.

The existing dwelling has 2 rooms capable of use as bedrooms, resulting in a parking demand of 1.25 spaces. With the proposal adding a third bedroom, the resulting parking demand would be 1.5 spaces.

Therefore, the proposal increases the total parking demand from 5 spaces to 5.25 spaces.

Although there are formally only 4 on-site car parking spaces currently available, there is capacity for more parking (likely 2 more spaces) on site subject to some modifications to landscaping etc. The proposed building addition results in the maximum on-site parking potential of the site being 4 spaces.

Performance Outcome 5.1 of the Transport, Access and Parking section of the General Development Policies states:

“Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:

- a) availability of on-street car parking*
- b) shared use of other parking areas*
- c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared*
- d) the adaptive reuse of a State or Local Heritage Place.”*

Of the factors listed in PO 5.1, factors a), c) and d) are considered relevant, as explained below:

- on-street parking is available on Rundle Street, other than between 7am and 9am when it is used as a bike lane;
- the fact that the practitioner of the consulting rooms is also the resident of the dwelling reduces parking demand by 1 space; and
- although the current application does not propose an adaptive reuse of the building (such re-use is already occurring) it does seek to revitalise and ensure the long-term viability of the Local Heritage Place. A slight dispensation to car parking demand in this context is considered consistent with the intent of PO 5.1(d).

Accordingly, despite the proposal resulting in a theoretical shortfall of 1.25 on-site car parking spaces when applying the most relevant parking rates in the Code, the shortfall is considered to be justified on multiple grounds.

Some concern was initially raised by the Council’s Traffic Engineer, that the constrained on-site manoeuvring arrangements may cause vehicles to reverse out of the site. This was particularly concerning due to the close proximity of the access driveway to the intersection of Rundle Street and Fullarton Road.

In response to this concern, the application was amended by reducing the depth of the garage, thereby increasing the area at the rear of the site for patient vehicles to perform a 3-point-turn to leave the site in a forward direction. With this amendment, it has been demonstrated that a B85 vehicle can perform the manoeuvre, however a B99 vehicle cannot.

According to the relevant Australian Standard:

“Design dimensions based on the B99 vehicle are required at all locations where failure of a vehicle to be able to physically fit into the facility would occasion intolerable congestion and possible hazard. Such locations shall include all access driveways, ramps and circulation roadways, unless there are special circumstances of severe space limitation coupled with relatively low traffic volumes in which case the B85 vehicle dimensions may be used.”

Given the space limitations of the compact allotment and the low traffic volumes associated with the single consulting room, it is considered reasonable to use the B85 dimensions in this instance. Initially the Council’s Traffic Engineer advised that a comment from the Department for Infrastructure and Transport would be required, however subsequently advised that it would not be required.

In order to minimise the prospect of patients attempting to reverse out of the site, the gate between the ‘public driveway’ and the ‘private driveway’ will need to be open during business hours. The Council’s Traffic Engineer has queried whether a formal arrangement will be in place for this to occur. A condition of consent is recommended to reinforce this requirement.

The Council’s Traffic Engineer has advised that vegetation within the sight line triangles as per AS2890.1 Figure 3.3, should be below eye height (1.15m), to maximise sight lines to pedestrians crossing the driveway. It is recommended that this be a condition of consent.

Additionally, the Traffic Engineer advised that the garden bed shown on the western side of the two car parking spaces will interfere with access to the two cars. As such, a condition is recommended requiring this landscaping to be replaced with hard paving. Although unfortunate, the loss of this narrow landscaping strip is not considered detrimental to the application as a whole.

Regulated Tree

As noted by the representor, a regulated *Cupressus sp* (Conifer) tree is located on the neighbouring property at 31 Fullarton Road, adjacent to the rear boundary of the subject land.

The report by Adelaide Tree Surgery advises that the proposal does not result in any major excavation works within the calculated 7.2m Tree Protection Zone (TPZ). The building addition is located outside of the TPZ, so the only excavation works are those associated with the paved driveway and car parking area.

Adelaide Tree Surgery have recommended the use of permeable pavers within the TPZ of the tree, to allow air and water to the tree root system.

CONCLUSION

The proposed development comprises an addition to the rear of a Local Heritage Place, increasing the size of the existing dwelling accommodation and consolidating/rationalising the layout of the existing consulting rooms.

With the addition located at the rear of the building, it has minimal streetscape impact and according to advice from the Council’s Heritage Advisor, has been designed so as to complement the existing building.

Visual and overshadowing impacts on the adjoining commercial property resulting from proposed boundary walls are a negative outcome of the proposal, however the Planning and Design Code only seeks to manage such impacts on residential properties.

On site car parking is strictly limited to 4 spaces as a result of the proposed addition. Having regard to the efficiency gained by the consulting room practitioner also being the resident of the site, the availability (albeit limited) of on-street parking and the positive heritage outcomes of the proposal, the amount of car parking is considered adequate.

On-site vehicle manoeuvring is constrained, however is acceptable in the context of the constraints of the small site and retaining the heritage listed building. The proposal is unlikely to impact on the adjacent Regulated Tree, subject to the use of permeable paving and careful construction of the car parking and driveway areas.

RECOMMENDATION

It is recommended that the Council Assessment Panel/SCAP resolve that:

1. Pursuant to Section 107(2)(c) of the Planning, Development and Infrastructure Act 2016, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code; and
2. Development Application Number 23014875, by Andrew Campbell Lloyd and Alana Campbell Lloyd is granted Planning Consent subject to the following conditions and notes:

CONDITIONS

Planning Consent

Condition 1

The development granted Planning Consent shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below (if any).

Condition 2

The gate at the entrance to the driveway from Rundle Street and the gate between the section of driveway shown on the plans as 'public driveway' and the section shown as 'private driveway' shall be open at all times that the consulting rooms are open.

Condition 3

The landscaping strip shown on the plans on the western side of the two parallel patient car parking spaces shall be replaced with hard paving to allow for access to cars.

Condition 4

All vegetation within the sight line triangles at the driveway entrance, as per AS2890.1 Figure 3.3, should be below eye height (1.15m), to maximise sight lines to pedestrians crossing the driveway.

Condition 5

The following tree protection measures shall be implemented within the Tree Protection Zone of the Cupressus sp (Conifer) tree located on the adjacent property at 31 Fullarton Road, ie. within 7.2m of the base of the trunk of the tree:

There is to be no soil level changes below topsoil within the Tree Protection Zone;

- a. There is to be no mechanical excavation works undertaken within the Tree Protection Zone (TPZ) of the subject tree. All works are to be undertaken using non-destructive methods;
- b. Any excavation works that are required for the preparation and the construction phase within the Tree Protection Zone (TPZ) of the subject tree is to be undertaken the supervision of the Project Arborist or by a suitably qualified arborist (minimum level 4 in Arboriculture).
- c. If any larger tree roots with a diameter greater than 40 mm are discovered during the construction phase of the proposal, the Project Arborist needs to be contacted to inspect and assess and obtain council approval.
- d. If any tree roots are exposed, temporary protection measure may be required such as hessian sheeting as multiple layers and this should be secured and also maintained moist until tree roots are covered/remediated.
- e. The area of the Tree Protection Zone (TPZ) on the development site requires to be fenced during the construction phase. This fencing can be removed when paving of this area needs to be undertaken.
- f. Following are a list of activities that are not permitted within the Tree Protection Zone (TPZ) of the subject tree:
 - Storage of materials
 - Refuelling
 - Parking of Vehicles/plant
 - Dumping of waste
 - Placement/storage of fill
 - Soil level changes
 - Preparation of concrete products/chemicals
 - Mechanical excavation
 - Washing down of tools/equipment
 - Temporary or permanent installation of utilities and signs
- g. The profile for paving within the Tree Protection Zone (TPZ) is to be constructed without being detrimental to the trees health and condition
- h. When paving with a Tree Protection Zone (TPZ) of a tree, a permeable paver needs to be used along with a suitable base profile.
- i. Do not allow for changes of the soil to below the top soil when undertaking paving within the Tree Protection Zone (TPZ). Carefully remove/scraped away to the original soil surface (top soil).
- j. Paving Option Number 1
 - i. Paver 50 – 80mm (80mm paver is required to support vehicle loads) Joints filled with 2 - 5mm clean aggregate. Regular paving sand is not suitable.
 - ii. Bedding layer of 30 – 50mm deep bedding layer of washed uniformly graded aggregate between 5 – 7 mm.
 - iii. A base course layer 100 – 300mm deep and comprising 20 mm washed uniformly graded aggregate. A geotextile fabric may be required over the subbase, preventing different layers from mixing, blocking pore spaces.
- k. Paving Option Number 2
 - i. Paver 50 – 80mm (80mm paver is required to support vehicle loads) Joints filled with 2 - 5mm clean aggregate. Regular paving sand is not suitable.
 - ii. No bedding layers.
 - iii. A base course layer 100 – 300mm deep of SPACE structural soil (structurally permeable aerated compactable earth). These materials can be compacted and will still allow air and water exchange.
- l. Any services that maybe required to enter and exit the development area should avoid the TPZ and SRZ wherever possible however. If they must pass within the TPZ, non-

destructive methods such as Hydro vac® systems must be used and tree roots to remain intact.

ADVISORY NOTES

Planning Consent

Advisory Note 1

The Applicant is reminded of its responsibilities under the *Environment Protection Act 1993*, to not harm the environment. Specifically, paint, plaster, concrete, brick wastes and wash waters should not be discharged into the stormwater system, litter should be appropriately stored on site pending removal, excavation and site disturbance should be limited, entry/exit points to the site should be managed to prevent soil being carried off site by vehicles, sediment barriers should be used (particularly on sloping sites), and material stockpiles should all be placed on site and not on the footpath or public roads or reserves. Further information is available by contacting the EPA.

Advisory Note 2

The granting of this consent does not remove the need for the beneficiary to obtain all other consents which may be required by any other legislation.

The Applicant's attention is particularly drawn to the requirements of the *Fences Act 1975* regarding notification of any neighbours affected by new boundary development or boundary fencing. Further information is available in the 'Fences and the Law' booklet available through the Legal Services Commission.

Advisory Note 3

The Applicant is advised that construction noise is not allowed:

1. on any Sunday or public holiday; or
2. after 7pm or before 7am on any other day

Advisory Note 4

The Applicant is advised that any works undertaken on Council owned land (including but not limited to works relating to crossovers, driveways, footpaths, street trees and stormwater connections) will require the approval of the Council pursuant to the *Local Government Act 1999* prior to any works being undertaken. Further information may be obtained by contacting Council's Public Realm Compliance Officer on 8366 4513.

Advisory Note 5

The Applicant is advised that the condition of the footpath, kerbing, vehicular crossing point, street tree(s) and any other Council infrastructure located adjacent to the subject land will be inspected by the Council prior to the commencement of building work and at the completion of building work. Any damage to Council infrastructure that occurs during construction must be rectified as soon as practicable and in any event, no later than four (4) weeks after substantial completion of the building work. The Council reserves its right to recover all costs associated with remedying any damage that has not been repaired in a timely manner from the appropriate person.

Advisory Note 6

The Council has not surveyed the subject land and has, for the purpose of its assessment, assumed that all dimensions and other details provided by the Applicant are correct and accurate.

Advisory Note 7

Appeal Rights - General rights of review and appeal exist in relation to any assessment, request, direction or act of a relevant authority in relation to the determination of this application, including conditions.

Advisory Note 8

Consents issued for this Development Application will remain valid for the following periods of time:

1. Planning Consent is valid for 24 months following the date of issue, within which time Development Approval must be obtained;
2. Development Approval is valid for 24 months following the date of issue, within which time works must have substantially commenced on site;
3. Works must be substantially completed within 3 years of the date on which Development Approval is issued.

If an extension is required to any of the above-mentioned timeframes a request can be made for an extension of time by emailing the Planning Department at townhall@npsp.sa.gov.au. Whether or not an extension of time will be granted will be at the discretion of the relevant authority.

Advisory Note 9

No work can commence on this development unless a Development Approval has been obtained. If one or more Consents have been granted on this Decision Notification Form, you must not start any site works or building work or change of use of the land until you have received notification that Development Approval has been granted.

120 RUNDLE ST KENT TOWN SA 5067

Address:

Click to view a detailed interactive [SAILS](#) in [SAILIS](#)

To view a detailed interactive property map in SAPPa click on the map below



Property Zoning Details

Zone

Business Neighbourhood

Overlay

Airport Building Heights (Regulated) (*All structures over 110 metres*)
 Future Road Widening
 Historic Area (*NPSP25*)
 Heritage Adjacency
 Hazards (Flooding - General)
 Local Heritage Place (*5907*)
 Prescribed Wells Area
 Regulated and Significant Tree
 Traffic Generating Development
 Urban Transport Routes

Local Variation (TNV)

Maximum Building Height (Levels) (*Maximum building height is 2 levels*)

Selected Development(s)

Dwelling addition

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards.

If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

Dwelling addition - Code Assessed - Performance Assessed

Part 2 - Zones and Sub Zones

Business Neighbourhood Zone

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	A variety of housing and accommodation types and compatible employment-generating land uses in an environment characterised by primarily low-rise buildings
DO 2	Buildings of a scale and design that complements surrounding built form, streetscapes and local character and provide for landscaping and open space.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature		
Built Form and Character			
PO 2.1 Buildings are of a scale and design that complements surrounding built form, streetscapes and local character.	DTS/DPF 2.1 None are applicable.		
PO 2.2 Development provides attractive landscaping to the primary street frontage.	DTS/DPF 2.2 None are applicable.		
PO 2.3 Site coverage is limited to provide space for landscaping, open space and pervious areas.	DTS/DPF 2.3 Development does not result in site coverage exceeding 60%.		
Building height and setbacks			
PO 3.1 Buildings are generally of low-rise construction, with taller buildings positioned towards the centre of the zone and away from any adjoining neighbourhood-type zone to positively contribute to the built form character of the locality.	DTS/DPF 3.1 Except on a Catalyst site in the Melbourne Street West Subzone, Building height (excluding garages, carports and outbuildings) is no greater than: (a) the following: <table border="1" style="margin-left: 20px;"><tr><th style="text-align: center;">Maximum Building Height (Levels)</th></tr><tr><td style="text-align: center;">Maximum building height is 2 levels</td></tr></table> (b) in all other cases (ie there is a blank field for both values): (i) 2 building levels or 9m where the development is located adjoining a different zone that primarily envisages residential development (ii) 3 building levels or 12m in all other cases.	Maximum Building Height (Levels)	Maximum building height is 2 levels
Maximum Building Height (Levels)			
Maximum building height is 2 levels			

	<p>In relation to DTS/DPF 3.1, in instances where:</p> <ul style="list-style-type: none"> (c) more than one value is returned in the same field: <ul style="list-style-type: none"> (i) for the purpose of DTS/DPF 3.1(a), refer to the Maximum Building Height (Metres) Technical and Numeric Variation layer or Maximum Building Height (Levels) Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development (ii) only one value is returned for DTS/DPF 3.1(a), (i.e. there is one blank field), then the relevant height in metres or building levels applies with no criteria for the other. 										
<p>PO 3.2</p> <p>Buildings are set back from primary street boundaries consistent with the existing streetscape.</p>	<p>DTS/DPF 3.2</p> <p>Buildings setback from the primary street boundary in accordance with the following table:</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: center;">Development Context</th> <th style="text-align: center;">Minimum setback</th> </tr> </thead> <tbody> <tr> <td>There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.</td> <td>The average setback of the existing buildings.</td> </tr> <tr> <td>There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is not on a corner site.</td> <td>The setback of the existing building.</td> </tr> <tr> <td>There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is on a corner site.</td> <td> <ul style="list-style-type: none"> (a) Where the existing building shares the same primary street frontage - the setback of the existing building (b) Where the existing building has a different primary street frontage - 5m </td> </tr> <tr> <td>There is no existing building on either of the abutting sites sharing the same street frontage as the site of the proposed building.</td> <td>5m</td> </tr> </tbody> </table> <p>For the purposes of DTS/DPF 3.2:</p>	Development Context	Minimum setback	There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.	The average setback of the existing buildings.	There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is not on a corner site.	The setback of the existing building.	There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is on a corner site.	<ul style="list-style-type: none"> (a) Where the existing building shares the same primary street frontage - the setback of the existing building (b) Where the existing building has a different primary street frontage - 5m 	There is no existing building on either of the abutting sites sharing the same street frontage as the site of the proposed building.	5m
Development Context	Minimum setback										
There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.	The average setback of the existing buildings.										
There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is not on a corner site.	The setback of the existing building.										
There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is on a corner site.	<ul style="list-style-type: none"> (a) Where the existing building shares the same primary street frontage - the setback of the existing building (b) Where the existing building has a different primary street frontage - 5m 										
There is no existing building on either of the abutting sites sharing the same street frontage as the site of the proposed building.	5m										

	<ul style="list-style-type: none"> (a) the setback of an existing building on an abutting site to the street boundary that it shares with the site of the proposed building is to be measured from the closest building wall to that street boundary at its closest point to the building wall and any existing projection from the building such as a verandah, porch, balcony, awning or bay window is not taken to form part of the building for the purposes of determining its setback (b) any proposed projections such as a verandah, porch, balcony, awning or bay window may encroach not more than 1.5 metres into the minimum setback prescribed in the table
<p>PO 3.3</p> <p>Buildings set back from secondary street boundaries (other than rear laneways) contribute to a consistent streetscape.</p>	<p>DTS/DPF 3.3</p> <p>Building walls are set back from the secondary street frontage:</p> <ul style="list-style-type: none"> (a) the average of any existing buildings on adjoining sites having frontage to the same street or (b) not less than 900mm where no building exists on an adjoining site.
<p>PO 3.4</p> <p>Walls on boundaries are limited in height and length to manage visual and overshadowing impacts on adjoining residential properties.</p>	<p>DTS/DPF 3.4</p> <p>Except where the building is a dwelling and is located on a central site within a row dwelling or terrace arrangement, side boundary walls occur only on one side boundary and satisfy (a) or (b) below:</p> <ul style="list-style-type: none"> (a) side boundary walls adjoin or abut a boundary wall of a building on adjoining land for the same or lesser length and height (b) side boundary walls do not: <ul style="list-style-type: none"> (i) exceed 3.2m in height from the lower of the natural or finished ground level (ii) exceed 11.5m in length (iii) when combined with other walls on the boundary of the subject development site, exceed a maximum 45% of the length of the boundary (iv) encroach within 3m of any other existing or proposed boundary walls on the subject land.
<p>PO 3.5</p> <p>Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a suburban streetscape character.</p>	<p>DTS/DPF 3.5</p> <p>Dwelling walls of dwellings in a semi-detached or row arrangement are set back at least 900mm from side boundaries shared with allotments outside the development site.</p>
<p>PO 3.6</p> <p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between buildings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours. 	<p>DTS/DPF 3.6</p> <p>Other than walls located on a side boundary, building walls are set back at least 900mm from side boundaries.</p>
<p>PO 3.7</p>	<p>DTS/DPF 3.7</p>

Buildings are set back from rear boundaries to provide: <ul style="list-style-type: none"> (a) separation between buildings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours (c) open space recreational opportunities (d) space for landscaping and vegetation. 	Buildings walls are set back from the rear boundary at least: <ul style="list-style-type: none"> (a) 3m for the first building level (b) 5m for any second building level.
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Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

Notification tables exclude the classes of development listed in Column A from notification provided that they do not fall within a corresponding exclusion prescribed in Column B.

Where a development or an element of a development falls within more than one class of development listed in Column A, it will be excluded from notification if it is excluded (in its entirety) under any of those classes of development. It need not be excluded under all applicable classes of development.

Where a development involves multiple performance assessed elements, all performance assessed elements will require notification (regardless of whether one or more elements are excluded in the applicable notification table) unless every performance assessed element of the application is excluded in the applicable notification table, in which case the application will not require notification.

A relevant authority may determine that a variation to 1 or more corresponding exclusions prescribed in Column B is minor in nature and does not require notification.

Class of Development (Column A)	Exceptions (Column B)
1. Development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. All development undertaken by: <ul style="list-style-type: none"> (a) the South Australian Housing Trust either individually or jointly with other persons or bodies or (b) a provider registered under the Community Housing National Law participating in a program relating to the renewal of housing endorsed by the South Australian Housing Trust. 	Except development involving any of the following: <ul style="list-style-type: none"> 1. residential flat building(s) of 3 or more building levels 2. the demolition (or partial demolition) of a State or Local Heritage Place (other than an excluded building) 3. the demolition (or partial demolition) of a building in a Historic Area Overlay (other than an excluded building).
3. Any development involving any of the following (or of any combination of any of the following): <ul style="list-style-type: none"> (a) advertisement (b) ancillary accommodation (c) community facility 	Except development that: <ul style="list-style-type: none"> 1. exceeds the maximum building height specified in Business Neighbourhood Zone DTS/DPF 3.1 or

<ul style="list-style-type: none"> (d) dwelling (e) dwelling addition (f) residential flat building (g) student accommodation. 	<ul style="list-style-type: none"> 2. is on a Catalyst Site that exceeds the maximum building height in Business Neighbourhood Zone DTS/DPF 3.1 that applies to development not on a Catalyst Site or 3. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ul style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 11.5m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
<p>4. Any development involving any of the following (or of any combination of any of the following):</p> <ul style="list-style-type: none"> (a) consulting room (b) office (c) shop. 	<p>Except development that:</p> <ul style="list-style-type: none"> 1. does not satisfy Business Neighbourhood Zone DTS/DPF 1.2 or 2. exceeds the maximum building height specified in Business Neighbourhood Zone DTS/DPF 3.1 or 3. is on a Catalyst Site that exceeds the maximum building height in Business Neighbourhood Zone DTS/DPF 3.1 that applies to development not on a Catalyst Site or 4. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ul style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 11.5m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
<p>5. Any development involving any of the following (or of any combination of any of the following):</p> <ul style="list-style-type: none"> (a) air handling unit, air conditioning system or exhaust fan (b) carport (c) deck (d) fence (e) internal building work (f) land division 	<p>None specified.</p>

<ul style="list-style-type: none"> (g) outbuilding (h) pergola (i) private bushfire shelter (j) replacement building (k) retaining wall (l) shade sail (m) solar photovoltaic panels (roof mounted) (n) swimming pool or spa pool and associated swimming pool safety features (o) temporary accommodation in an area affected by bushfire (p) tree damaging activity (q) verandah (r) water tank. 	
<p>6. Demolition.</p>	<p>Except any of the following:</p> <ol style="list-style-type: none"> 1. the demolition (or partial demolition) of a State or Local Heritage Place (other than an excluded building) 2. the demolition (or partial demolition) of a building in a Historic Area Overlay (other than an excluded building).
<p>7. Railway line.</p>	<p>Except where located outside of a rail corridor or rail reserve.</p>

Placement of Notices - Exemptions for Performance Assessed Development

None specified.

Placement of Notices - Exemptions for Restricted Development

None specified.

Part 3 - Overlays

Airport Building Heights (Regulated) Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

<h2 style="text-align: center; margin: 0;">Desired Outcome</h2>	
<p>DO 1</p>	<p>Management of potential impacts of buildings and generated emissions to maintain operational and safety requirements of registered and certified commercial and military airfields, airports, airstrips and helicopter landing sites.</p>

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Building height does not pose a hazard to the operation of a certified or registered aerodrome.	DTS/DPF 1.1 Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas. In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Any of the following classes of development: (a) building located in an area identified as 'All structures' (no height limit is prescribed) or will exceed the height specified in the <i>Airport Building Heights (Regulated) Overlay</i> (b) building comprising exhaust stacks that generates plumes, or may cause plumes to be generated, above a height specified in the <i>Airport Building Heights (Regulated) Overlay</i> .	The airport-operator company for the relevant airport within the meaning of the <i>Airports Act 1996</i> of the Commonwealth or, if there is no airport-operator company, the Secretary of the Minister responsible for the administration of the <i>Airports Act 1996</i> of the Commonwealth.	To provide expert assessment and direction to the relevant authority on potential impacts on the safety and operation of aviation activities.	Development of a class to which Schedule 9 clause 3 item 1 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Future Road Widening Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development which is consistent with and will not compromise efficient delivery of future road widening requirements.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Future Road Widening	
PO 1.1 Development does not compromise or is located and designed to minimise its impact on future road widening requirements.	DTS/DPF 1.1 Development does not involve building work, or building work is located wholly outside the land subject to the 6m Consent Area, the C Type Requirement or the Strip Requirement of the Metropolitan Adelaide Road Widening Plan.

Procedural Matters (PM)

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Other than where all deemed-to-satisfy criteria for all policies relevant to this referral are met, development (including the division of land) that is within or may encroach within a Future Road Widening Area.	Commissioner of Highways.	To provide expert technical assessment and direction to the relevant authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 4 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Hazards (Flooding – General) Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Impacts on people, property, infrastructure and the environment from general flood risk are minimised through the appropriate siting and design of development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Flood Resilience	
PO 2.1 Development is sited, designed and constructed to prevent the entry of floodwaters where the entry of flood waters is likely to result in undue damage to or compromise ongoing activities within buildings.	DTS/DPF 2.1 Habitable buildings, commercial and industrial buildings, and buildings used for animal keeping incorporate a finished ground and floor level not less than: In instances where no finished floor level value is specified, a building incorporates a finished floor level at least 300mm above the height of a 1% AEP flood event.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Heritage Adjacency Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development adjacent to State and Local Heritage Places maintains the heritage and cultural values of those Places.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1	DTS/DPF 1.1

Development adjacent to a State or Local Heritage Place does not dominate, encroach on or unduly impact on the setting of the Place.	None are applicable.
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Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development which in the opinion of the relevant authority materially affects the context within which the State Heritage Place is situated.	Minister responsible for the administration of the <i>Heritage Places Act 1993</i> .	To provide expert assessment and direction to the relevant authority on the potential impacts of development adjacent State Heritage Places.	Development of a class to which Schedule 9 clause 3 item 17 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Historic Area Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Historic themes and characteristics are reinforced through conservation and contextually responsive development, design and adaptive reuse that responds to existing coherent patterns of land division, site configuration, streetscapes, building siting and built scale, form and features as exhibited in the Historic Area and expressed in the Historic Area Statement.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
PO 1.1 All development is undertaken having consideration to the	DTS/DPF 1.1 None are applicable.

historic streetscapes and built form as expressed in the Historic Area Statement.	
Built Form	
PO 2.1 The form and scale of new buildings and structures that are visible from the public realm are consistent with the prevailing historic characteristics of the historic area.	DTS/DPF 2.1 None are applicable.
PO 2.2 Development is consistent with the prevailing building and wall heights in the historic area.	DTS/DPF 2.2 None are applicable.
PO 2.3 Design and architectural detailing of street-facing buildings (including but not limited to roof pitch and form, openings, chimneys and verandahs) complement the prevailing characteristics in the historic area.	DTS/DPF 2.3 None are applicable.
PO 2.4 Development is consistent with the prevailing front and side boundary setback pattern in the historic area.	DTS/DPF 2.4 None are applicable.
PO 2.5 Materials are either consistent with or complement those within the historic area.	DTS/DPF 2.5 None are applicable.
Alterations and additions	
PO 3.1 Alterations and additions complement the subject building, employ a contextual design approach and are sited to ensure they do not dominate the primary façade.	DTS/DPF 3.1 Alterations and additions are fully contained within the roof space of an existing building with no external alterations made to the building elevation facing the primary street.
PO 3.2 Adaptive reuse and revitalisation of buildings to support retention consistent with the Historic Area Statement.	DTS/DPF 3.2 None are applicable.
Context and Streetscape Amenity	
PO 6.1 The width of driveways and other vehicle access ways are consistent with the prevailing width of existing driveways of the historic area.	DTS/DPF 6.1 None are applicable.
PO 6.2 Development maintains the valued landscape patterns and characteristics that contribute to the historic area, except where they compromise safety, create nuisance, or impact adversely on buildings or infrastructure.	DTS/DPF 6.2 None are applicable.
Ruins	
PO 8.1	DTS/DPF 8.1

Development conserves and complements features and ruins associated with former activities of significance.	None are applicable.
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Historic Area Statements

Statement#	Statement																
Historic Areas affecting City of Norwood, Payneham and St Peters																	
NPSP25	<p>The Parade / Fullarton Road Historic Area Statement (NPSP25)</p> <p>The Historic Area Overlay identifies localities that comprise characteristics of an identifiable historic, economic and / or social theme of recognised importance. They can comprise land divisions, development patterns, built form characteristics and natural features that provide a legible connection to the historic development of a locality.</p> <p>These attributes have been identified in the below table. In some cases State and / or Local Heritage Places within the locality contribute to the attributes of an Historic Area.</p> <p>The preparation of an Historic Impact Statement can assist in determining potential additional attributes of an Historic Area where these are not stated in the below table.</p>																
	<table border="1"> <tr> <td>Eras, themes and context</td> <td>Pre 1890s (Victorian). Offices, consulting rooms and residential dwellings, generally utilising historic buildings, with new residential apartments and townhouses.</td> </tr> <tr> <td>Allotments, subdivision and built form patterns</td> <td><i>[Not stated]</i></td> </tr> <tr> <td>Architectural styles, detailing and built form features</td> <td>Victorian villas, large two storey mansion buildings, double-fronted houses and groups of two storey row houses. Consistent set-backs. Buildings which address Fullarton Road and The Parade have deep set-backs.</td> </tr> <tr> <td>Building height</td> <td>Generally up to two storeys.</td> </tr> <tr> <td>Materials</td> <td>Sandstone or bluestone.</td> </tr> <tr> <td>Fencing</td> <td><i>[Not stated]</i></td> </tr> <tr> <td>Setting, landscaping, streetscape and public realm features</td> <td>Vehicular movement is dominated by The Parade, Fullarton Road, Rundle Street and Little Dew Street, which provides primary access for delivery, service, visitors' and residents' vehicles, in preference to access via the adjoining residential areas. Well established garden settings. High standard of landscaping incorporating large trees which reinforce the leafy avenue character of these roads.</td> </tr> <tr> <td>Representative Buildings</td> <td><i>[Not identified]</i></td> </tr> </table>	Eras, themes and context	Pre 1890s (Victorian). Offices, consulting rooms and residential dwellings, generally utilising historic buildings, with new residential apartments and townhouses.	Allotments, subdivision and built form patterns	<i>[Not stated]</i>	Architectural styles, detailing and built form features	Victorian villas, large two storey mansion buildings, double-fronted houses and groups of two storey row houses. Consistent set-backs. Buildings which address Fullarton Road and The Parade have deep set-backs.	Building height	Generally up to two storeys.	Materials	Sandstone or bluestone.	Fencing	<i>[Not stated]</i>	Setting, landscaping, streetscape and public realm features	Vehicular movement is dominated by The Parade, Fullarton Road, Rundle Street and Little Dew Street, which provides primary access for delivery, service, visitors' and residents' vehicles, in preference to access via the adjoining residential areas. Well established garden settings. High standard of landscaping incorporating large trees which reinforce the leafy avenue character of these roads.	Representative Buildings	<i>[Not identified]</i>
	Eras, themes and context	Pre 1890s (Victorian). Offices, consulting rooms and residential dwellings, generally utilising historic buildings, with new residential apartments and townhouses.															
	Allotments, subdivision and built form patterns	<i>[Not stated]</i>															
	Architectural styles, detailing and built form features	Victorian villas, large two storey mansion buildings, double-fronted houses and groups of two storey row houses. Consistent set-backs. Buildings which address Fullarton Road and The Parade have deep set-backs.															
	Building height	Generally up to two storeys.															
	Materials	Sandstone or bluestone.															
	Fencing	<i>[Not stated]</i>															
	Setting, landscaping, streetscape and public realm features	Vehicular movement is dominated by The Parade, Fullarton Road, Rundle Street and Little Dew Street, which provides primary access for delivery, service, visitors' and residents' vehicles, in preference to access via the adjoining residential areas. Well established garden settings. High standard of landscaping incorporating large trees which reinforce the leafy avenue character of these roads.															
	Representative Buildings	<i>[Not identified]</i>															

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Local Heritage Place Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development maintains the heritage and cultural values of Local Heritage Places through conservation, ongoing use and adaptive reuse.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 The form of new buildings and structures maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.1 None are applicable.
PO 1.2 Massing, scale and siting of development maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.2 None are applicable.
PO 1.3 Design and architectural detailing (including but not limited to roof pitch and form, openings, chimneys and verandahs) maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.3 None are applicable.
PO 1.4 Development is consistent with boundary setbacks and setting.	DTS/DPF 1.4 None are applicable.
PO 1.5	DTS/DPF 1.5

Materials and colours are either consistent with or complement the heritage values of the Local Heritage Place.	None are applicable.
PO 1.6 New buildings and structures are not placed or erected between the primary or secondary street boundaries and the façade of a Local Heritage Place.	DTS/DPF 1.6 None are applicable.
PO 1.7 Development of a Local Heritage Place retains features contributing to its heritage value.	DTS/DPF 1.7 None are applicable.
Alterations and Additions	
PO 2.1 Alterations and additions complement the subject building and are sited to be unobtrusive, not conceal or obstruct heritage elements and detailing, or dominate the Local Heritage Place or its setting.	DTS/DPF 2.1 None are applicable.
PO 2.2 Adaptive reuse and revitalisation of Local Heritage Places to support their retention in a manner that respects and references the original use of the Local Heritage Place.	DTS/DPF 2.2 None are applicable.
Landscape Context and Streetscape Amenity	
PO 5.1 Individually heritage listed trees, parks, historic gardens and memorial avenues are retained unless: (a) trees / plantings are, or have the potential to be, a danger to life or property or (b) trees / plantings are significantly diseased and their life expectancy is short.	DTS/DPF 5.1 None are applicable.
Conservation Works	
PO 7.1 Conservation works to the exterior of a Local Heritage Place (and other features identified in the extent of listing) match original materials to be repaired and utilise traditional work methods.	DTS/DPF 7.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Urban Transport Routes Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from Urban Transport Routes.

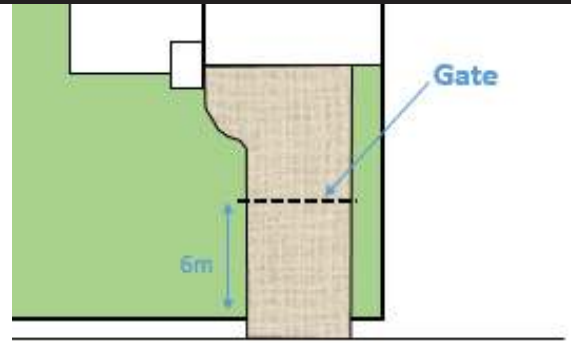
Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Access - Safe Entry and Exit (Traffic Flow)	
<p>PO 1.1</p> <p>Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State maintained roads.</p>	<p>DTS/DPF 1.1</p> <p>An access point satisfies (a), (b) or (c):</p> <p>(a) where servicing a single (1) dwelling / residential allotment:</p> <ul style="list-style-type: none"> (i) it will not result in more than one access point (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 3m and 4m (measured at the site boundary) <p>(b) where the development will result in 2 and up to 6 dwellings:</p> <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 5.8m to 6m (measured at the site boundary) and an access depth of 6m (measured from the site boundary into the site)

	<ul style="list-style-type: none"> (c) where the development will result in 7 or more dwellings, or is a non-residential land use: <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site using left turn only movements (iii) vehicles can enter and exit the site in a forward direction (iv) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (v) it will have a width of between 6m and 7m (measured at the site boundary), where the development is expected to accommodate vehicles with a length of 6.4m or less (vi) it will have a width of between 6m and 9m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 6.4m to 8.8m (vii) it will have a width of between 9m and 12m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 8.8m to 12.5m (viii) provides for simultaneous two-way vehicle movements at the access: <ul style="list-style-type: none"> A. with entry and exit movements for vehicles with a length up to 5.2m vehicles being fully within the kerbside lane of the road <li style="text-align: center;">and B. with entry movements of 8.8m vehicles (where relevant) being fully within the kerbside lane of the road and the exit movements of 8.8m vehicles do not cross the centreline of the road.
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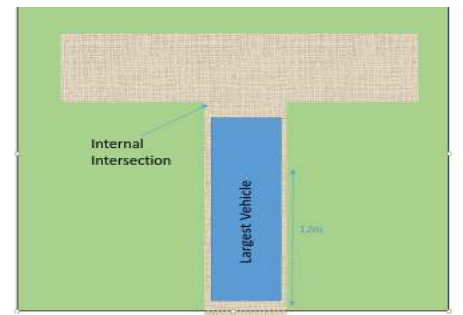
Access - On-Site Queuing

<p>PO 2.1</p> <p>Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption on the functional performance of the road and maintain safe vehicle movements.</p>	<p>DTS/DPF 2.1</p> <p>An access point in accordance with one of the following:</p> <ul style="list-style-type: none"> (a) will not service, or is not intended to service, more than 6 dwellings and there are no internal driveways, intersections, car parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) as shown in the following diagram:
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- (b) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
 - (i) is expected to be serviced by vehicles with a length no greater than 6.4m
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)

- (c) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
 - (i) is expected to be serviced by vehicles with a length greater than a 6.4m small rigid vehicle
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
 - (iii) any termination of or change in priority of movement within the main car park aisle is located far enough into the site so that the largest vehicle expected on-site can store fully within the site before being required to stop
 - (iv) all parking or manoeuvring areas for commercial vehicles are located a minimum of 12m or the length of the longest vehicle expected on site from the access (measured from the site boundary into the site) as shown in the following diagram:



Access - (Location Spacing) - Existing Access Point	
<p>PO 3.1</p> <p>Existing access points are designed to accommodate the type and volume of traffic likely to be generated by the development.</p>	<p>DTS/DPF 3.1</p> <p>An existing access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) it will not service, or is not intended to service, more than 6 dwellings (b) it is not located on a Controlled Access Road and will not service development that will result in a larger class of vehicle expected to access the site using the existing access (c) is not located on a Controlled Access Road and development constitutes: <ul style="list-style-type: none"> (i) a change of use between an office <500m² gross leasable floor area and a consulting room <500m² gross leasable floor area or vice versa (ii) a change in use from a shop to an office, consulting room or personal or domestic services establishment (iii) a change of use from a consulting room or office <250m² gross leasable floor area to shop <250m² gross leasable floor area (iv) a change of use from a shop <500m² gross leasable floor area to a warehouse <500m² gross leasable floor area (v) an office or consulting room with a <500m² gross leasable floor area (vi) a change of use from a residential dwelling to a shop, office, consulting room or personal or domestic services establishment with <250m² gross leasable floor area.

Access - Location (Spacing) - New Access Points	
<p>PO 4.1</p> <p>New access points are spaced apart from any existing access point or public road junction to manage impediments to traffic flow and maintain safe and efficient operating conditions on the road.</p>	<p>DTS/DPF 4.1</p> <p>A new access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where a development site is intended to serve between 1 and 6 dwellings, access to the site is from the local road network (not being a Controlled Access Road) and is located outside of the bold lines shown in the following diagram: <div style="text-align: center; margin: 10px 0;"> <p style="font-size: small; margin-top: 5px;">TP = Tangent point</p> </div> <p style="font-size: x-small; margin-top: 5px;">NOTE: The points marked X₁ and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension Y-Y extends to Point Y₁.</p> (b) where the development site is intended to serve between 1 and 6 dwellings, the new access:

- (i) is not located on a Controlled Access Road
 - (ii) is not located on a section of road affected by double barrier lines
 - (iii) will be on a road with a speed environment of 70km/h or less
 - (iv) is located outside of the bold lines on the diagram shown in the diagram following part (a)
 - (v) is located a minimum of 6m from a median opening or pedestrian crossing
- (c) where DTS/DPF 4.1 part (a) and (b) do not apply and access from an alternative local road at least 25m from the State Maintained Road is not available, and the access is not located on a Controlled Access Road, the new access is separated in accordance with the following:

Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes
50 km/h or less	No spacing requirement	20m
60 km/h	5m (for development intended to serve between 1 and 6 dwellings) and 10m for all other cases	73m
70 km/h	40m	92m
80 km/h	50m	114m
90 km/h	65m	139m
100 km/h	80m	165m
110 km/h	100m	193m

Access - Location (Sight Lines)

PO 5.1

Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.

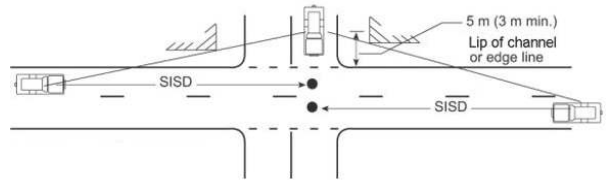
DTS/DPF 5.1

An access point satisfies (a) and (c) or (b) and (c):

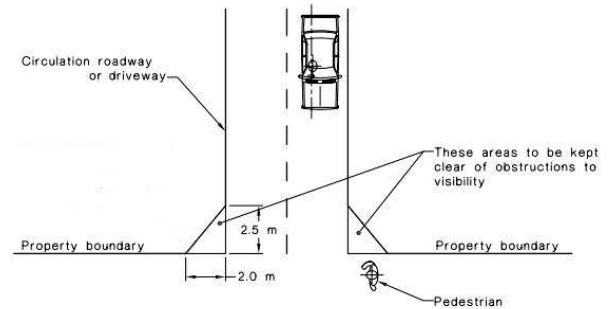
- (a) the development site does or is intended to serve between 1 and 6 dwellings and utilises an existing access point
or
- (b) drivers approaching or exiting an access point have an unobstructed line of sight in accordance with the following (measured at a height of 1.1m above the surface of the road):

Speed Limit	Access point serving 1-6 dwellings	Access point serving all other development
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40 km/h or less	47m	73m
50 km/h	63m	97m
60 km/h	81m	123m
70 km/h	100m	151m
80 km/h	121m	181m
90 km/h	144m	226m
100 km/h	169m	262m
110km/h	195m	300m



and
(c) pedestrian sightlines in accordance with the following diagram:



Access – Mud and Debris

PO 6.1
Access points constructed to minimise mud or other debris being carried or transferred onto the road to ensure safe road operating conditions.

DTS/DPF 6.1
Where the road has an unsealed shoulder and the road is not kerbed, the access way is sealed from the edge of seal on the road for a minimum of 10m or to the property boundary (whichever is closer).

Access - Stormwater

PO 7.1
Access points are designed to minimise negative impact on roadside drainage of water.

DTS/DPF 7.1
Development does not:

- (a) decrease the capacity of an existing drainage point
- (b) restrict or prevent the flow of stormwater through an existing drainage point and system
- (c) result in access points becoming stormwater flow paths directly onto the road.

Building on Road Reserve

PO 8.1
Buildings or structures that encroach onto, above or below road reserves are designed and sited to minimise impact on safe movements by all road users.

DTS/DPF 8.1
Buildings or structures are not located on, above or below the road reserve.

Public Road Junctions	
<p>PO 9.1</p> <p>New junctions with a public road (including the opening of unmade public road junctions) or modifications to existing road junctions are located and designed to ensure safe operating conditions are maintained on the State Maintained Road.</p>	<p>DTS/DPF 9.1</p> <p>Development does not comprise any of the following:</p> <ul style="list-style-type: none"> (a) creating a new junction with a public road (b) opening an unmade public road junction (c) modifying an existing public road junction.
Corner Cut-Offs	
<p>PO 10.1</p> <p>Development is located and designed to maintain sightlines for drivers turning into and out of public road junctions to contribute to driver safety.</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram:</p> <div style="text-align: center;"> </div>

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, development (including the division of land) that involves any of the following to/on a State Maintained Road or within 25 metres of an intersection with any such road:</p> <ul style="list-style-type: none"> (a) creation of a new access or junction (b) alterations to an existing access or public road junction (except where deemed to be minor in the opinion of the relevant authority) (c) development that changes the nature of vehicular movements or increase the number or frequency of movements through an existing access (except where deemed to be minor in the opinion of the relevant authority). 	<p>Commissioner of Highways.</p>	<p>To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.</p>	<p>Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>

Part 4 - General Development Policies

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.	DTS/DPF 1.1 One of the following is satisfied: <ul style="list-style-type: none"> (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i> (b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.

Design in Urban Areas

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development is: <ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
Earthworks and sloping land	
PO 8.1 Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 8.1 Development does not involve any of the following: <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
PO 8.2 Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.	DTS/DPF 8.2 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b): <ul style="list-style-type: none"> (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
PO 8.3 Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8): <ul style="list-style-type: none"> (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development (c) are designed to integrate with the natural topography of the land. 	DTS/DPF 8.3 None are applicable.
PO 8.4 Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.	DTS/DPF 8.4 None are applicable.
PO 8.5 Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.	DTS/DPF 8.5 None are applicable.
Overlooking / Visual Privacy (low rise buildings)	
PO 10.1 Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.	DTS/DPF 10.1 Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone: <ul style="list-style-type: none"> (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm

	<ul style="list-style-type: none"> (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.
<p>PO 10.2</p> <p>Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.</p>	<p>DTS/DPF 10.2</p> <p>One of the following is satisfied:</p> <ul style="list-style-type: none"> (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases
All residential development	
Front elevations and passive surveillance	
<p>PO 17.1</p> <p>Dwellings incorporate windows facing primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 17.1</p> <p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> (a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m (b) has an aggregate window area of at least 2m² facing the primary street.
<p>PO 17.2</p> <p>Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.</p>	<p>DTS/DPF 17.2</p> <p>Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.</p>
Outlook and Amenity	
<p>PO 18.1</p> <p>Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 18.1</p> <p>A living room of a dwelling incorporates a window with an external outlook of the street frontage, private open space, public open space, or waterfront areas.</p>
Residential Development - Low Rise	
External appearance	
<p>PO 20.1</p> <p>Garaging is designed to not detract from the streetscape or</p>	<p>DTS/DPF 20.1</p> <p>Garages and carports facing a street:</p>

appearance of a dwelling.	<ul style="list-style-type: none"> (a) are situated so that no part of the garage or carport will be in front of any part of the building line of the dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a garage door / opening width not exceeding 7m (d) have a garage door / opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.
<p>PO 20.2</p> <p>Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and the appearance of common driveway areas.</p>	<p>DTS/DPF 20.2</p> <p>Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway:</p> <ul style="list-style-type: none"> (a) a minimum of 30% of the building wall is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building wall (c) a balcony projects from the building wall (d) a verandah projects at least 1m from the building wall (e) eaves of a minimum 400mm width extend along the width of the front elevation (f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm (g) a minimum of two different materials or finishes are incorporated on the walls of the front building elevation, with a maximum of 80% of the building elevation in a single material or finish.
<p>PO 20.3</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 20.3</p> <p>None are applicable</p>
Private Open Space	
<p>PO 21.1</p> <p>Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.</p>	<p>DTS/DPF 21.1</p> <p>Private open space is provided in accordance with Design in Urban Areas Table 1 - Private Open Space.</p>
<p>PO 21.2</p> <p>Private open space is positioned to provide convenient access from internal living areas.</p>	<p>DTS/DPF 21.2</p> <p>Private open space is directly accessible from a habitable room.</p>
Landscaping	

<p>PO 22.1</p> <p>Soft landscaping is incorporated into development to:</p> <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) contribute shade and shelter (c) provide for stormwater infiltration and biodiversity (d) enhance the appearance of land and streetscapes. 	<p>DTS/DPF 22.1</p> <p>Residential development incorporates soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b):</p> <ul style="list-style-type: none"> (a) a total area for the entire development site, including any common property, as determined by the following table: <table border="1" style="margin: 10px auto; border-collapse: collapse; text-align: center;"> <thead> <tr style="background-color: #1a3d54; color: white;"> <th style="padding: 5px;">Site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="padding: 5px;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;"><150</td> <td style="padding: 5px;">10%</td> </tr> <tr> <td style="padding: 5px;">150-200</td> <td style="padding: 5px;">15%</td> </tr> <tr> <td style="padding: 5px;">>200-450</td> <td style="padding: 5px;">20%</td> </tr> <tr> <td style="padding: 5px;">>450</td> <td style="padding: 5px;">25%</td> </tr> </tbody> </table> (b) at least 30% of any land between the primary street boundary and the primary building line. 	Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
>200-450	20%										
>450	25%										

Car parking, access and manoeuvrability

<p>PO 23.1</p> <p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single width car parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m per space (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double width car parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.4m (iii) minimum garage door width of 2.4m per space.
<p>PO 23.2</p> <p>Uncovered car parking space are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.
<p>PO 23.3</p> <p>Driveways and access points are located and designed to</p>	<p>DTS/DPF 23.3</p> <p>Driveways and access points satisfy (a) or (b):</p>

<p>facilitate safe access and egress while maximising land available for street tree planting, pedestrian movement, domestic waste collection, landscaped street frontages and on-street parking.</p>	<ul style="list-style-type: none"> (a) sites with a frontage to a public road of 10m or less, have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site (b) sites with a frontage to a public road greater than 10m: <ul style="list-style-type: none"> (i) have a maximum width of 5m measured at the property boundary and are the only access point provided on the site; (ii) have a width between 3.0 metres and 3.2 metres measured at the property boundary and no more than two access points are provided on site, separated by no less than 1m.
<p>PO 23.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 23.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: <ul style="list-style-type: none"> (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
<p>PO 23.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 23.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient of the driveway does not exceed a grade of 1 in 4 and includes transitions to ensure a maximum grade change of 12.5% (1 in 8) for summit changes, and 15% (1 in 6.7) for sag changes, in accordance with AS 2890.1:2004 to prevent vehicles bottoming or scraping (b) the centreline of the driveway has an angle of no less than 70 degrees and no more than 110 degrees from the street boundary to which it takes its access as shown in the following diagram:

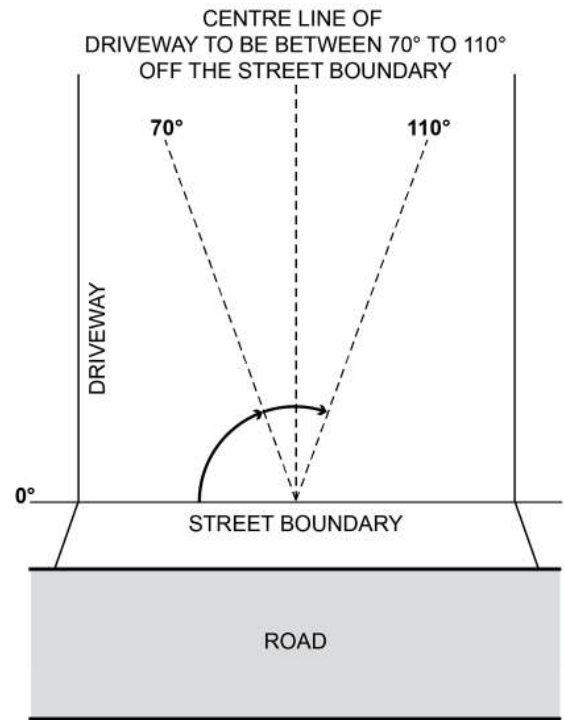
	 <p>(c) if located to provide access from an alley, lane or right of way - the alley, land or right of way is at least 6.2m wide along the boundary of the allotment / site.</p>
<p>PO 23.6</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 23.6</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> (a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
<p>Waste storage</p>	
<p>PO 24.1</p> <p>Provision is made for the convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 24.1</p> <p>Where dwellings abut both side boundaries a waste bin storage area is provided behind the building line of each dwelling that:</p> <ul style="list-style-type: none"> (a) has a minimum area of 2m² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space); and (b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.

Table 1 - Private Open Space

Dwelling Type	Dwelling / Site	Minimum Rate
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	Configuration	
Dwelling (at ground level, other than a residential flat building that includes above ground dwellings)		Total private open space area: (a) Site area <301m ² : 24m ² located behind the building line. (b) Site area ≥ 301m ² : 60m ² located behind the building line. Minimum directly accessible from a living room: 16m ² / with a minimum dimension 3m.
Cabin or caravan (permanently fixed to the ground) in a residential park or caravan and tourist park		Total area: 16m ² , which may be uses as second car parking space, provided on each site intended for residential occupation.
Dwelling in a residential flat building or mixed use building which incorporate above ground level dwellings	Dwellings at ground level:	15m ² / minimum dimension 3m
	Dwellings above ground level:	
	Studio (no separate bedroom)	4m ² / minimum dimension 1.8m
	One bedroom dwelling	8m ² / minimum dimension 2.1m
	Two bedroom dwelling	11m ² / minimum dimension 2.4m
	Three + bedroom dwelling	15 m ² / minimum dimension 2.6m

Infrastructure and Renewable Energy Facilities

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Wastewater Services	

PO 12.2 Effluent drainage fields and other wastewater disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.	DTS/DPF 12.2 Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.
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Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Overshadowing	
PO 3.1 Overshadowing of habitable room windows of adjacent residential land uses in: a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight.	DTS/DPF 3.1 North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.
PO 3.2 Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in: a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight b. other zones is managed to enable access to direct winter sunlight.	DTS/DPF 3.2 Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following: a. for ground level private open space, the smaller of the following: i. half the existing ground level open space or ii. 35m ² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m) b. for ground level communal open space, at least half of the existing ground level open space.

<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <ul style="list-style-type: none"> (a) the form of development contemplated in the zone (b) the orientation of the solar energy facilities (c) the extent to which the solar energy facilities are already overshadowed. 	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
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Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Vehicle Parking Rates	
<p>PO 5.1</p> <p>Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:</p> <ul style="list-style-type: none"> (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place. 	<p>DTS/DPF 5.1</p> <p>Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:</p> <ul style="list-style-type: none"> (a) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas if the development is a class of development listed in Table 2 and the site is in a Designated Area (b) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements where (a) does not apply (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.
Corner Cut-Offs	
<p>PO 10.1</p> <p>Development is located and designed to ensure drivers can safely turn into and out of public road junctions.</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:</p>

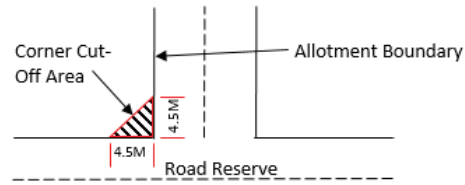


Table 1 - General Off-Street Car Parking Requirements

Class of Development	Car Parking Rate (unless varied by Table 2 onwards) Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.
Residential Development	
Detached Dwelling	Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Group Dwelling	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered. 0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.
Residential Flat Building	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered. 0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.
Row Dwelling where vehicle access is from the primary street	Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.

Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Semi-Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>

Table 2 - Off-Street Car Parking Requirements in Designated Areas

Class of Development	Car Parking Rate		Designated Areas
	Minimum number of spaces	Maximum number of spaces	
Development generally			
All classes of development	No minimum.	<p>No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is:</p> <p>1 space for each dwelling with a total floor area less than 75 square metres</p> <p>2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres</p> <p>3 spaces for each dwelling with a total floor area greater than 150 square metres.</p> <p>Residential flat building or Residential component of a multi-storey building: 1 visitor space for each 6 dwellings.</p>	<p>Capital City Zone</p> <p>City Main Street Zone</p> <p>City Riverbank Zone</p> <p>Adelaide Park Lands Zone</p> <p>Business Neighbourhood Zone (within the City of Adelaide)</p> <p>The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone</p>

120 RUNDLE ST KENT TOWN SA 5067

Address:

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Property Zoning Details

- Zone** Business Neighbourhood
- Overlay**
 - Airport Building Heights (Regulated) *(All structures over 110 metres)*
 - Future Road Widening
 - Historic Area *(NPSP25)*
 - Heritage Adjacency
 - Hazards (Flooding - General)
 - Local Heritage Place *(5907)*
 - Prescribed Wells Area
 - Regulated and Significant Tree
 - Traffic Generating Development
 - Urban Transport Routes
- Local Variation (TNV)**
 - Maximum Building Height (Levels) *(Maximum building height is 2 levels)*

Selected Development(s)

Consulting room

This development may be subject to multiple assessment pathways. Please review the document below to determine which pathway may be applicable based on the proposed development compliances to standards. If no assessment pathway is shown this mean the proposed development will default to performance assessed. Please contact your local council in this instance. Refer to Part 1 - Rules of Interpretation - Determination of Classes of Development

Property Policy Information for above selection

Consulting room - Code Assessed - Performance Assessed

Part 2 - Zones and Sub Zones

Business Neighbourhood Zone

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	A variety of housing and accommodation types and compatible employment-generating land uses in an environment characterised by primarily low-rise buildings

DO 2	Buildings of a scale and design that complements surrounding built form, streetscapes and local character and provide for landscaping and open space.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Housing and accommodation types appropriate to the locality complemented by shops, offices, consulting rooms and other non-residential uses that do not materially impact residential amenity.	DTS/DPF 1.1 Development comprises one or more of the following: <ul style="list-style-type: none"> (a) Community facility (b) Consulting room (c) Dwelling (d) Office (e) Residential flat building (f) Shop
PO 1.2 Business and commercial land uses complement and enhance the prevailing or emerging neighbourhood character.	DTS/DPF 1.2 Shops, offices and consulting rooms (or any combination thereof) do not exceed 250m ² in gross leasable floor area.
PO 1.3 Changes in the use of land between similar businesses encourages the efficient reuse of commercial premises and supports continued local access to a range of services compatible to the locality.	DTS/DPF 1.3 A change of use to a shop, office or consulting room or any combination of these uses where all of the following are achieved: <ul style="list-style-type: none"> (a) the area to be occupied by the proposed development is in an existing building and is currently used as a shop, office, consulting room or any combination of these uses (b) if the proposed change of use is for a shop: <ul style="list-style-type: none"> (i) the total gross leasable floor area of the shop will not exceed 250m² (ii) if primarily involving the handling and sale of foodstuffs, areas used for the storage and collection of refuse are sited at least 10m from the site of a dwelling (other than a dwelling directly associated with the proposed shop) (iii) if primarily involving heating and cooking of foodstuffs in a commercial kitchen and is within 30m of any residential allotment within a neighbourhood-type zone or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions (c) off-street vehicular parking exists in accordance with the rate(s) specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas to the nearest whole number, except where: <ul style="list-style-type: none"> (i) the required contribution will be made into a relevant car parking offset scheme (other than where a relevant contribution has previously been made) or (ii) the building is a local heritage place.
Built Form and Character	
PO 2.1 Buildings are of a scale and design that complements surrounding built form, streetscapes and local character.	DTS/DPF 2.1 None are applicable.
PO 2.2 Development provides attractive landscaping to the primary street frontage.	DTS/DPF 2.2 None are applicable.
PO 2.3	DTS/DPF 2.3

Site coverage is limited to provide space for landscaping, open space and pervious areas.	Development does not result in site coverage exceeding 60%.
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Building height and setbacks

<p>PO 3.1</p> <p>Buildings are generally of low-rise construction, with taller buildings positioned towards the centre of the zone and away from any adjoining neighbourhood-type zone to positively contribute to the built form character of the locality.</p>	<p>DTS/DPF 3.1</p> <p>Except on a Catalyst site in the Melbourne Street West Subzone, Building height (excluding garages, carports and outbuildings) is no greater than:</p> <p>(a) the following:</p> <table border="1" style="width: 100%; margin: 10px 0;"> <tr> <th style="text-align: center;">Maximum Building Height (Levels)</th> </tr> <tr> <td>Maximum building height is 2 levels</td> </tr> </table> <p>(b) in all other cases (ie there is a blank field for both values):</p> <ul style="list-style-type: none"> (i) 2 building levels or 9m where the development is located adjoining a different zone that primarily envisages residential development (ii) 3 building levels or 12m in all other cases. <p>In relation to DTS/DPF 3.1, in instances where:</p> <p>(c) more than one value is returned in the same field:</p> <ul style="list-style-type: none"> (i) for the purpose of DTS/DPF 3.1(a), refer to the Maximum Building Height (Metres) Technical and Numeric Variation layer or Maximum Building Height (Levels) Technical and Numeric Variation layer in the SA planning database to determine the applicable value relevant to the site of the proposed development (ii) only one value is returned for DTS/DPF 3.1(a), (i.e. there is one blank field), then the relevant height in metres or building levels applies with no criteria for the other. 	Maximum Building Height (Levels)	Maximum building height is 2 levels
Maximum Building Height (Levels)			
Maximum building height is 2 levels			

<p>PO 3.2</p> <p>Buildings are set back from primary street boundaries consistent with the existing streetscape.</p>	<p>DTS/DPF 3.2</p> <p>Buildings setback from the primary street boundary in accordance with the following table:</p> <table border="1" style="width: 100%; margin: 10px 0;"> <thead> <tr> <th style="text-align: center;">Development Context</th> <th style="text-align: center;">Minimum setback</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;">There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.</td> <td style="padding: 5px;">The average setback of the existing buildings.</td> </tr> <tr> <td style="padding: 5px;">There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is not on a corner site.</td> <td style="padding: 5px;">The setback of the existing building.</td> </tr> <tr> <td style="padding: 5px;">There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is on a corner site.</td> <td style="padding: 5px;"> <ul style="list-style-type: none"> (a) Where the existing building shares the same primary street frontage - the setback of the existing building (b) Where the existing building has a different primary street frontage - 5m </td> </tr> <tr> <td style="padding: 5px;">There is no existing building on either of the abutting sites sharing the same street frontage as the site of the proposed building.</td> <td style="padding: 5px;">5m</td> </tr> </tbody> </table> <p>For the purposes of DTS/DPF 3.2:</p> <ul style="list-style-type: none"> (a) the setback of an existing building on an abutting site to the street boundary that it shares with the site of the proposed building is to be measured from the closest building wall to that street boundary at its closest point to the building wall and any existing projection from the building such as a verandah, porch, balcony, awning or bay window is not taken to form part of the building for the purposes of determining its setback (b) any proposed projections such as a verandah, porch, balcony, awning or bay window may encroach not more than 1.5 metres into the minimum setback prescribed in the table 	Development Context	Minimum setback	There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.	The average setback of the existing buildings.	There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is not on a corner site.	The setback of the existing building.	There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is on a corner site.	<ul style="list-style-type: none"> (a) Where the existing building shares the same primary street frontage - the setback of the existing building (b) Where the existing building has a different primary street frontage - 5m 	There is no existing building on either of the abutting sites sharing the same street frontage as the site of the proposed building.	5m
Development Context	Minimum setback										
There is an existing building on both abutting sites sharing the same street frontage as the site of the proposed building.	The average setback of the existing buildings.										
There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is not on a corner site.	The setback of the existing building.										
There is an existing building on only one abutting site sharing the same street frontage as the site of the proposed building and the existing building is on a corner site.	<ul style="list-style-type: none"> (a) Where the existing building shares the same primary street frontage - the setback of the existing building (b) Where the existing building has a different primary street frontage - 5m 										
There is no existing building on either of the abutting sites sharing the same street frontage as the site of the proposed building.	5m										

<p>PO 3.3</p> <p>Buildings set back from secondary street boundaries (other than rear laneways) contribute to a consistent streetscape.</p>	<p>DTS/DPF 3.3</p> <p>Building walls are set back from the secondary street frontage:</p> <ul style="list-style-type: none"> (a) the average of any existing buildings on adjoining sites having frontage to the same street or (b) not less than 900mm where no building exists on an adjoining site.
<p>PO 3.4</p> <p>Walls on boundaries are limited in height and length to manage visual and overshadowing impacts on adjoining residential properties.</p>	<p>DTS/DPF 3.4</p> <p>Except where the building is a dwelling and is located on a central site within a row dwelling or terrace arrangement, side boundary walls occur only on one side boundary and satisfy (a) or (b) below:</p> <ul style="list-style-type: none"> (a) side boundary walls adjoin or abut a boundary wall of a building on adjoining land for the same or lesser length and height (b) side boundary walls do not: <ul style="list-style-type: none"> (i) exceed 3.2m in height from the lower of the natural or finished ground level (ii) exceed 11.5m in length (iii) when combined with other walls on the boundary of the subject development site, exceed a maximum 45% of the length of the boundary (iv) encroach within 3m of any other existing or proposed boundary walls on the subject land.
<p>PO 3.5</p> <p>Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a suburban streetscape character.</p>	<p>DTS/DPF 3.5</p> <p>Dwelling walls of dwellings in a semi-detached or row arrangement are set back at least 900mm from side boundaries shared with allotments outside the development site.</p>
<p>PO 3.6</p> <p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between buildings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours. 	<p>DTS/DPF 3.6</p> <p>Other than walls located on a side boundary, building walls are set back at least 900mm from side boundaries.</p>
<p>PO 3.7</p> <p>Buildings are set back from rear boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between buildings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours (c) open space recreational opportunities (d) space for landscaping and vegetation. 	<p>DTS/DPF 3.7</p> <p>Buildings walls are set back from the rear boundary at least:</p> <ul style="list-style-type: none"> (a) 3m for the first building level (b) 5m for any second building level.
<p>Concept Plans</p>	
<p>PO 6.1</p> <p>Development is compatible with the outcomes sought by any relevant Concept Plan contained within Part 12 - Concept Plans of the Planning and Design Code to support the orderly development of land through staging of development and provision of infrastructure.</p>	<p>DTS/DPF 6.1</p> <p>The site of the development is wholly located outside any relevant Concept Plan boundary. The following Concept Plans are relevant:</p> <p>In relation to DTS/DPF 6.1, in instances where:</p> <ul style="list-style-type: none"> (a) one or more Concept Plan is returned, refer to Part 12 - Concept Plans in the Planning and Design Code to determine if a Concept Plan is relevant to the site of the proposed development. Note: multiple concept plans may be relevant. (b) in instances where 'no value' is returned, there is no relevant concept plan and DTS/DPF 6.1 is met.

Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

Notification tables exclude the classes of development listed in Column A from notification provided that they do not fall within a corresponding exclusion prescribed in Column B.

Where a development or an element of a development falls within more than one class of development listed in Column A, it will be excluded from notification if it is excluded (in its entirety) under any of those classes of development. It need not be excluded under all applicable classes of development.

Where a development involves multiple performance assessed elements, all performance assessed elements will require notification (regardless of whether one or more elements are excluded in the applicable notification table) unless every performance assessed element of the application is excluded in the applicable notification table, in which case the application will not require notification.

A relevant authority may determine that a variation to 1 or more corresponding exclusions prescribed in Column B is minor in nature and does not require notification.

Class of Development (Column A)	Exceptions (Column B)
1. Development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. All development undertaken by: <ul style="list-style-type: none"> (a) the South Australian Housing Trust either individually or jointly with other persons or bodies or (b) a provider registered under the Community Housing National Law participating in a program relating to the renewal of housing endorsed by the South Australian Housing Trust. 	Except development involving any of the following: <ul style="list-style-type: none"> 1. residential flat building(s) of 3 or more building levels 2. the demolition (or partial demolition) of a State or Local Heritage Place (other than an excluded building) 3. the demolition (or partial demolition) of a building in a Historic Area Overlay (other than an excluded building).
3. Any development involving any of the following (or of any combination of any of the following): <ul style="list-style-type: none"> (a) advertisement (b) ancillary accommodation (c) community facility (d) dwelling (e) dwelling addition (f) residential flat building (g) student accommodation. 	Except development that: <ul style="list-style-type: none"> 1. exceeds the maximum building height specified in Business Neighbourhood Zone DTS/DPF 3.1 or 2. is on a Catalyst Site that exceeds the maximum building height in Business Neighbourhood Zone DTS/DPF 3.1 that applies to development not on a Catalyst Site or 3. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ul style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 11.5m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or (b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
4. Any development involving any of the following (or of any combination of any of the following): <ul style="list-style-type: none"> (a) consulting room (b) office (c) shop. 	Except development that: <ul style="list-style-type: none"> 1. does not satisfy Business Neighbourhood Zone DTS/DPF 1.2 or 2. exceeds the maximum building height specified in Business Neighbourhood Zone DTS/DPF 3.1 or 3. is on a Catalyst Site that exceeds the maximum building height in Business Neighbourhood Zone DTS/DPF 3.1 that applies to development not on a Catalyst Site or 4. involves a building wall (or structure) that is proposed to be situated on (or abut) an allotment boundary (not being a boundary with a primary street or secondary street or an excluded boundary) and: <ul style="list-style-type: none"> (a) the length of the proposed wall (or structure) exceeds 11.5m (other than where the proposed wall abuts an existing wall or structure of greater length on the adjoining allotment) or

	(b) the height of the proposed wall (or post height) exceeds 3.2m measured from the lower of the natural or finished ground level (other than where the proposed wall abuts an existing wall or structure of greater height on the adjoining allotment).
5. Any development involving any of the following (or of any combination of any of the following): (a) air handling unit, air conditioning system or exhaust fan (b) carport (c) deck (d) fence (e) internal building work (f) land division (g) outbuilding (h) pergola (i) private bushfire shelter (j) replacement building (k) retaining wall (l) shade sail (m) solar photovoltaic panels (roof mounted) (n) swimming pool or spa pool and associated swimming pool safety features (o) temporary accommodation in an area affected by bushfire (p) tree damaging activity (q) verandah (r) water tank.	None specified.
6. Demolition.	Except any of the following: 1. the demolition (or partial demolition) of a State or Local Heritage Place (other than an excluded building) 2. the demolition (or partial demolition) of a building in a Historic Area Overlay (other than an excluded building).
7. Railway line.	Except where located outside of a rail corridor or rail reserve.

Placement of Notices - Exemptions for Performance Assessed Development

None specified.

Placement of Notices - Exemptions for Restricted Development

None specified.

Part 3 - Overlays

Airport Building Heights (Regulated) Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Management of potential impacts of buildings and generated emissions to maintain operational and safety requirements of registered and certified commercial and military airfields, airports, airstrips and helicopter landing sites.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Building height does not pose a hazard to the operation of a certified or registered aerodrome.	DTS/DPF 1.1 Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas. In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Any of the following classes of development: (a) building located in an area identified as 'All structures' (no height limit is prescribed) or will exceed the height specified in the <i>Airport Building Heights (Regulated) Overlay</i> (b) building comprising exhaust stacks that generates plumes, or may cause plumes to be generated, above a height specified in the <i>Airport Building Heights (Regulated) Overlay</i> .	The airport-operator company for the relevant airport within the meaning of the <i>Airports Act 1996</i> of the Commonwealth or, if there is no airport-operator company, the Secretary of the Minister responsible for the administration of the <i>Airports Act 1996</i> of the Commonwealth.	To provide expert assessment and direction to the relevant authority on potential impacts on the safety and operation of aviation activities.	Development of a class to which Schedule 9 clause 3 item 1 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Future Road Widening Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development which is consistent with and will not compromise efficient delivery of future road widening requirements.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Future Road Widening	
PO 1.1 Development does not compromise or is located and designed to minimise its impact on future road widening requirements.	DTS/DPF 1.1 Development does not involve building work, or building work is located wholly outside the land subject to the 6m Consent Area, the C Type Requirement or the Strip Requirement of the Metropolitan Adelaide Road Widening Plan.

Procedural Matters (PM)

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
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<p>Other than where all deemed-to-satisfy criteria for all policies relevant to this referral are met, development (including the division of land) that is within or may encroach within a Future Road Widening Area.</p>	<p>Commissioner of Highways.</p>	<p>To provide expert technical assessment and direction to the relevant authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.</p>	<p>Development of a class to which Schedule 9 clause 3 item 4 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>
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Hazards (Flooding – General) Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Impacts on people, property, infrastructure and the environment from general flood risk are minimised through the appropriate siting and design of development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Flood Resilience	
<p>PO 2.1</p> <p>Development is sited, designed and constructed to prevent the entry of floodwaters where the entry of flood waters is likely to result in undue damage to or compromise ongoing activities within buildings.</p>	<p>DTS/DPF 2.1</p> <p>Habitable buildings, commercial and industrial buildings, and buildings used for animal keeping incorporate a finished ground and floor level not less than:</p> <p>In instances where no finished floor level value is specified, a building incorporates a finished floor level at least 300mm above the height of a 1% AEP flood event.</p>
Environmental Protection	
<p>PO 3.1</p> <p>Buildings and structures used either partly or wholly to contain or store hazardous materials are designed to prevent spills or leaks leaving the confines of the building during a 1% AEP flood event to avoid potential environmental harm.</p>	<p>DTS/DPF 3.1</p> <p>Development involving the storage or disposal of hazardous materials is wholly located outside of the 1% AEP flood plain or flow path.</p>

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Heritage Adjacency Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development adjacent to State and Local Heritage Places maintains the heritage and cultural values of those Places.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Development adjacent to a State or Local Heritage Place does not dominate, encroach on or unduly impact on the setting of the Place.	DTS/DPF 1.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development which in the opinion of the relevant authority materially affects the context within which the State Heritage Place is situated.	Minister responsible for the administration of the <i>Heritage Places Act 1993</i> .	To provide expert assessment and direction to the relevant authority on the potential impacts of development adjacent State Heritage Places.	Development of a class to which Schedule 9 clause 3 item 17 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Historic Area Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Historic themes and characteristics are reinforced through conservation and contextually responsive development, design and adaptive reuse that responds to existing coherent patterns of land division, site configuration, streetscapes, building siting and built scale, form and features as exhibited in the Historic Area and expressed in the Historic Area Statement.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
PO 1.1 All development is undertaken having consideration to the historic streetscapes and built form as expressed in the Historic Area Statement.	DTS/DPF 1.1 None are applicable.
Built Form	
PO 2.1 The form and scale of new buildings and structures that are visible from the public realm are consistent with the prevailing historic characteristics of the historic area.	DTS/DPF 2.1 None are applicable.
PO 2.2 Development is consistent with the prevailing building and wall heights in the historic area.	DTS/DPF 2.2 None are applicable.
PO 2.3 Design and architectural detailing of street-facing buildings (including but not limited to roof pitch and form, openings, chimneys and verandahs) complement the prevailing characteristics in the historic area.	DTS/DPF 2.3 None are applicable.
PO 2.4 Development is consistent with the prevailing front and side boundary setback pattern in the historic area.	DTS/DPF 2.4 None are applicable.
PO 2.5 Materials are either consistent with or complement those within the historic area.	DTS/DPF 2.5 None are applicable.
Context and Streetscape Amenity	
PO 6.1 The width of driveways and other vehicle access ways are consistent with the prevailing width of existing driveways of the historic area.	DTS/DPF 6.1 None are applicable.
PO 6.2 Development maintains the valued landscape patterns and characteristics that contribute to the historic area, except where they compromise safety, create nuisance, or impact adversely on buildings or infrastructure.	DTS/DPF 6.2 None are applicable.
Ruins	
PO 8.1 Development conserves and complements features and ruins associated with former activities of significance.	DTS/DPF 8.1 None are applicable.

Historic Area Statements

Statement#	Statement
Historic Areas affecting City of Norwood, Payneham and St Peters	
	<p>The Parade / Fullarton Road Historic Area Statement (NPSP25)</p> <p>The Historic Area Overlay identifies localities that comprise characteristics of an identifiable historic, economic and / or social theme of recognised importance. They can comprise land divisions, development patterns, built form characteristics and natural features that provide a legible connection to the historic development of a locality.</p> <p>These attributes have been identified in the below table. In some cases State and / or Local Heritage Places within the locality contribute to the attributes of an Historic Area.</p> <p>The preparation of an Historic Impact Statement can assist in determining potential additional attributes of an Historic Area where these are</p>

Statement#	Statement	
NPSP25	not stated in the below table.	
	Eras, themes and context	Pre 1890s (Victorian). Offices, consulting rooms and residential dwellings, generally utilising historic buildings, with new residential apartments and townhouses.
	Allotments, subdivision and built form patterns	[Not stated]
	Architectural styles, detailing and built form features	Victorian villas, large two storey mansion buildings, double-fronted houses and groups of two storey row houses. Consistent set-backs. Buildings which address Fullarton Road and The Parade have deep set-backs.
	Building height	Generally up to two storeys.
	Materials	Sandstone or bluestone.
	Fencing	[Not stated]
	Setting, landscaping, streetscape and public realm features	Vehicular movement is dominated by The Parade, Fullarton Road, Rundle Street and Little Dew Street, which provides primary access for delivery, service, visitors' and residents' vehicles, in preference to access via the adjoining residential areas. Well established garden settings. High standard of landscaping incorporating large trees which reinforce the leafy avenue character of these roads.
	Representative Buildings	[Not identified]

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Local Heritage Place Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development maintains the heritage and cultural values of Local Heritage Places through conservation, ongoing use and adaptive reuse.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 The form of new buildings and structures maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.1 None are applicable.
PO 1.2 Massing, scale and siting of development maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.2 None are applicable.
PO 1.3 Design and architectural detailing (including but not limited to roof pitch and form, openings, chimneys and verandahs) maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.3 None are applicable.
PO 1.4 Development is consistent with boundary setbacks and setting.	DTS/DPF 1.4 None are applicable.
PO 1.5 Materials and colours are either consistent with or complement the heritage values of the Local Heritage Place.	DTS/DPF 1.5 None are applicable.
PO 1.6 New buildings and structures are not placed or erected between the primary or secondary street boundaries and the façade of a Local Heritage Place.	DTS/DPF 1.6 None are applicable.
PO 1.7 Development of a Local Heritage Place retains features contributing to its heritage value.	DTS/DPF 1.7 None are applicable.
Landscape Context and Streetscape Amenity	
PO 5.1 Individually heritage listed trees, parks, historic gardens and memorial avenues are retained unless: (a) trees / plantings are, or have the potential to be, a danger to life or property or (b) trees / plantings are significantly diseased and their life expectancy is short.	DTS/DPF 5.1 None are applicable.
Conservation Works	
PO 7.1 Conservation works to the exterior of a Local Heritage Place (and other features identified in the extent of listing) match original materials to be repaired and utilise traditional work methods.	DTS/DPF 7.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Traffic Generating Development Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes and Major Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from urban transport routes and major urban transport routes.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Traffic Generating Development	
PO 1.1 Development designed to minimise its potential impact on the safety, efficiency and functional performance of the State Maintained Road network.	DTS/DPF 1.1 Access is obtained directly from a State Maintained Road where it involves any of the following types of development: <ul style="list-style-type: none"> (a) building, or buildings, containing in excess of 50 dwellings (b) land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more.
PO 1.2 Access points sited and designed to accommodate the type and volume of traffic likely to be generated by development.	DTS/DPF 1.2 Access is obtained directly from a State Maintained Road where it involves any of the following types of development: <ul style="list-style-type: none"> (a) building, or buildings, containing in excess of 50 dwellings (b) land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more.
PO 1.3 Sufficient accessible on-site queuing provided to meet the needs of the development so that queues do not impact on the State Maintained Road network.	DTS/DPF 1.3 Access is obtained directly from a State Maintained Road where it involves any of the following types of development: <ul style="list-style-type: none"> (a) building, or buildings, containing in excess of 50 dwellings (b) land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, any of the following classes of development that are proposed within 250m of a State Maintained Road:</p> <ul style="list-style-type: none"> (a) except where a proposed development has previously been referred under clause (b) - a building, or buildings, containing in excess of 50 dwellings (b) except where a proposed development has previously been referred under clause (a) - land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more. 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Urban Transport Routes Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from Urban Transport Routes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)


Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Access - Safe Entry and Exit (Traffic Flow)	
<p>PO 1.1</p> <p>Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State maintained roads.</p>	<p>DTS/DPF 1.1</p> <p>An access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where servicing a single (1) dwelling / residential allotment: <ul style="list-style-type: none"> (i) it will not result in more than one access point (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 3m and 4m (measured at the site boundary) (b) where the development will result in 2 and up to 6 dwellings: <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site in a forward direction

	<ul style="list-style-type: none"> (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 5.8m to 6m (measured at the site boundary) and an access depth of 6m (measured from the site boundary into the site) <p>(c) where the development will result in 7 or more dwellings, or is a non-residential land use:</p> <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site using left turn only movements (iii) vehicles can enter and exit the site in a forward direction (iv) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (v) it will have a width of between 6m and 7m (measured at the site boundary), where the development is expected to accommodate vehicles with a length of 6.4m or less (vi) it will have a width of between 6m and 9m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 6.4m to 8.8m (vii) it will have a width of between 9m and 12m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 8.8m to 12.5m (viii) provides for simultaneous two-way vehicle movements at the access: <ul style="list-style-type: none"> A. with entry and exit movements for vehicles with a length up to 5.2m vehicles being fully within the kerbside lane of the road and B. with entry movements of 8.8m vehicles (where relevant) being fully within the kerbside lane of the road and the exit movements of 8.8m vehicles do not cross the centreline of the road.
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Access - On-Site Queuing

<p>PO 2.1</p> <p>Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption on the functional performance of the road and maintain safe vehicle movements.</p>
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<p>DTS/DPF 2.1</p> <p>An access point in accordance with one of the following:</p> <ul style="list-style-type: none"> (a) will not service, or is not intended to service, more than 6 dwellings and there are no internal driveways, intersections, car parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) as shown in the following diagram: <div style="text-align: center;"> </div> <ul style="list-style-type: none"> (b) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
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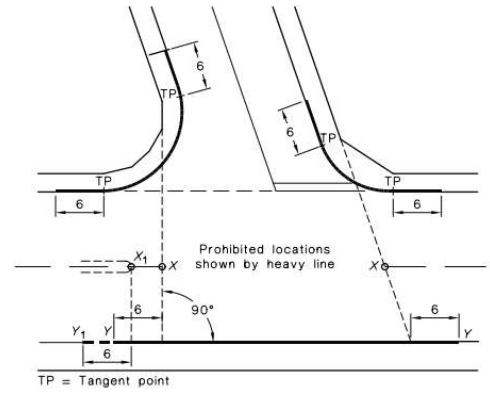
	<ul style="list-style-type: none"> (i) is expected to be serviced by vehicles with a length no greater than 6.4m (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) <p>(c) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:</p> <ul style="list-style-type: none"> (i) is expected to be serviced by vehicles with a length greater than a 6.4m small rigid vehicle (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) (iii) any termination of or change in priority of movement within the main car park aisle is located far enough into the site so that the largest vehicle expected on-site can store fully within the site before being required to stop (iv) all parking or manoeuvring areas for commercial vehicles are located a minimum of 12m or the length of the longest vehicle expected on site from the access (measured from the site boundary into the site) as shown in the following diagram: <div style="text-align: center;">  </div>
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Access - (Location Spacing) - Existing Access Point

<p>PO 3.1</p> <p>Existing access points are designed to accommodate the type and volume of traffic likely to be generated by the development.</p>	<p>DTS/DPF 3.1</p> <p>An existing access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) it will not service, or is not intended to service, more than 6 dwellings (b) it is not located on a Controlled Access Road and will not service development that will result in a larger class of vehicle expected to access the site using the existing access (c) is not located on a Controlled Access Road and development constitutes: <ul style="list-style-type: none"> (i) a change of use between an office <500m² gross leasable floor area and a consulting room <500m² gross leasable floor area or vice versa (ii) a change in use from a shop to an office, consulting room or personal or domestic services establishment (iii) a change of use from a consulting room or office <250m² gross leasable floor area to shop <250m² gross leasable floor area (iv) a change of use from a shop <500m² gross leasable floor area to a warehouse <500m² gross leasable floor area (v) an office or consulting room with a <500m² gross leasable floor area (vi) a change of use from a residential dwelling to a shop, office, consulting room or personal or domestic services establishment with <250m² gross leasable floor area.
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Access - Location (Spacing) - New Access Points

<p>PO 4.1</p> <p>New access points are spaced apart from any existing access point or public road junction to manage impediments to traffic flow and maintain safe and efficient operating conditions on the road.</p>	<p>DTS/DPF 4.1</p> <p>A new access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where a development site is intended to serve between 1 and 6 dwellings, access to the site is from the local road network (not being a Controlled Access Road) and is located outside of the bold lines shown in the following diagram:
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NOTE:
The points marked X_1 and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension $Y-Y_1$ extends to Point Y_1 .

- (b) where the development site is intended to serve between 1 and 6 dwellings, the new access:
 - (i) is not located on a Controlled Access Road
 - (ii) is not located on a section of road affected by double barrier lines
 - (iii) will be on a road with a speed environment of 70km/h or less
 - (iv) is located outside of the bold lines on the diagram shown in the diagram following part (a)
 - (v) is located a minimum of 6m from a median opening or pedestrian crossing
- (c) where DTS/DPF 4.1 part (a) and (b) do not apply and access from an alternative local road at least 25m from the State Maintained Road is not available, and the access is not located on a Controlled Access Road, the new access is separated in accordance with the following:

Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes
50 km/h or less	No spacing requirement	20m
60 km/h	5m (for development intended to serve between 1 and 6 dwellings) and 10m for all other cases	73m
70 km/h	40m	92m
80 km/h	50m	114m
90 km/h	65m	139m
100 km/h	80m	165m
110 km/h	100m	193m

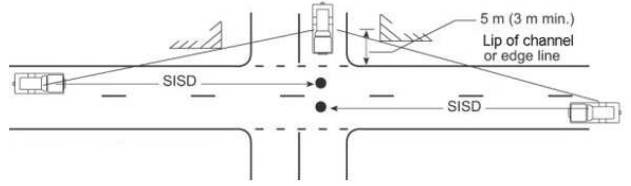
Access - Location (Sight Lines)

PO 5.1
Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.

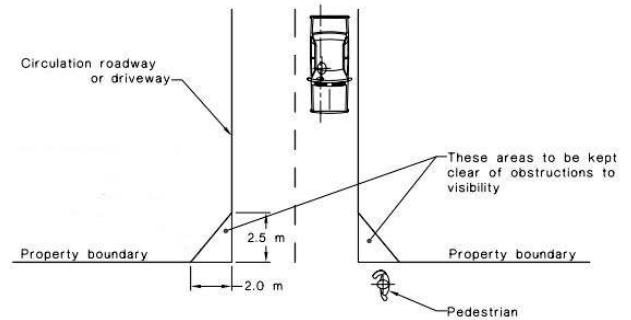
- DTS/DPF 5.1
An access point satisfies (a) and (c) or (b) and (c):
- (a) the development site does or is intended to serve between 1 and 6 dwellings and utilises an existing access point or
 - (b) drivers approaching or exiting an access point have an unobstructed line of sight in accordance with the following (measured at a height of 1.1m above the surface of the road):

Speed Limit	Access point serving 1-6 dwellings	Access point serving all other development
40 km/h or less	47m	73m
50 km/h	63m	97m

60 km/h	81m	123m
70 km/h	100m	151m
80 km/h	121m	181m
90 km/h	144m	226m
100 km/h	169m	262m
110km/h	195m	300m



and
(c) pedestrian sightlines in accordance with the following diagram:



Access - Mud and Debris

<p>PO 6.1</p> <p>Access points constructed to minimise mud or other debris being carried or transferred onto the road to ensure safe road operating conditions.</p>	<p>DTS/DPF 6.1</p> <p>Where the road has an unsealed shoulder and the road is not kerbed, the access way is sealed from the edge of seal on the road for a minimum of 10m or to the property boundary (whichever is closer).</p>
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Access - Stormwater

<p>PO 7.1</p> <p>Access points are designed to minimise negative impact on roadside drainage of water.</p>	<p>DTS/DPF 7.1</p> <p>Development does not:</p> <ul style="list-style-type: none"> (a) decrease the capacity of an existing drainage point (b) restrict or prevent the flow of stormwater through an existing drainage point and system (c) result in access points becoming stormwater flow paths directly onto the road.
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Building on Road Reserve

<p>PO 8.1</p> <p>Buildings or structures that encroach onto, above or below road reserves are designed and sited to minimise impact on safe movements by all road users.</p>	<p>DTS/DPF 8.1</p> <p>Buildings or structures are not located on, above or below the road reserve.</p>
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Public Road Junctions

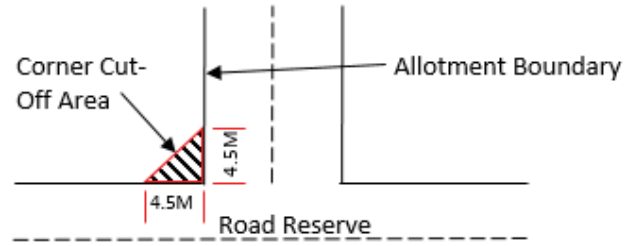
<p>PO 9.1</p> <p>New junctions with a public road (including the opening of unmade public road junctions) or modifications to existing road junctions are located and designed to ensure safe operating conditions are maintained on the State Maintained Road.</p>	<p>DTS/DPF 9.1</p> <p>Development does not comprise any of the following:</p> <ul style="list-style-type: none"> (a) creating a new junction with a public road (b) opening an unmade public road junction (c) modifying an existing public road junction.
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Corner Cut-Offs

<p>PO 10.1</p> <p>Development is located and designed to maintain sightlines for drivers</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located</p>
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turning into and out of public road junctions to contribute to driver safety.

wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram:



Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, development (including the division of land) that involves any of the following to/on a State Maintained Road or within 25 metres of an intersection with any such road:</p> <ul style="list-style-type: none"> (a) creation of a new access or junction (b) alterations to an existing access or public road junction (except where deemed to be minor in the opinion of the relevant authority) (c) development that changes the nature of vehicular movements or increase the number or frequency of movements through an existing access (except where deemed to be minor in the opinion of the relevant authority). 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Part 4 - General Development Policies

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.</p>	<p>DTS/DPF 1.1</p> <p>One of the following is satisfied:</p> <ul style="list-style-type: none"> (a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i>

- (b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.

Design in Urban Areas

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	<p>Development is:</p> <ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
External Appearance	
PO 1.1 Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).	DTS/DPF 1.1 None are applicable.
PO 1.2 Where zero or minor setbacks are desirable, development provides shelter over footpaths (in the form of verandahs, awnings, canopies and the like, with adequate lighting) to positively contribute to the walkability, comfort and safety of the public realm.	DTS/DPF 1.2 None are applicable.
PO 1.3 Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.	DTS/DPF 1.3 None are applicable.
PO 1.4 Plant, exhaust and intake vents and other technical equipment are integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by: <ul style="list-style-type: none"> (a) positioning plant and equipment discretely, in unobtrusive locations as viewed from public roads and spaces (b) screening rooftop plant and equipment from view (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses. 	DTS/DPF 1.4 Development does not incorporate any structures that protrude beyond the roofline.
PO 1.5 The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built	DTS/DPF 1.5 None are applicable.

form), taking into account the form of development contemplated in the relevant zone.	
Safety	
PO 2.1 Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.	DTS/DPF 2.1 None are applicable.
PO 2.2 Development is designed to differentiate public, communal and private areas.	DTS/DPF 2.2 None are applicable.
PO 2.3 Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.	DTS/DPF 2.3 None are applicable.
PO 2.4 Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.	DTS/DPF 2.4 None are applicable.
PO 2.5 Common areas and entry points of buildings (such as the foyer areas of residential buildings) and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.	DTS/DPF 2.5 None are applicable.
Landscaping	
PO 3.1 Soft landscaping and tree planting are incorporated to: (a) minimise heat absorption and reflection (b) maximise shade and shelter (c) maximise stormwater infiltration (d) enhance the appearance of land and streetscapes.	DTS/DPF 3.1 None are applicable.
Environmental Performance	
PO 4.1 Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.	DTS/DPF 4.1 None are applicable.
PO 4.2 Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.	DTS/DPF 4.2 None are applicable.
PO 4.3 Buildings incorporate climate responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.	DTS/DPF 4.3 None are applicable.
On-site Waste Treatment Systems	
PO 6.1 Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.	DTS/DPF 6.1 Effluent disposal drainage areas do not: (a) encroach within an area used as private open space or result in less private open space than that specified in Design in Urban Areas Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.

Car parking appearance	
<p>PO 7.1</p> <p>Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on streetscapes through techniques such as:</p> <ul style="list-style-type: none"> (a) limiting protrusion above finished ground level (b) screening through appropriate planting, fencing and mounding (c) limiting the width of openings and integrating them into the building structure. 	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
<p>PO 7.2</p> <p>Vehicle parking areas appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.</p>	<p>DTS/DPF 7.2</p> <p>None are applicable.</p>
<p>PO 7.3</p> <p>Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.</p>	<p>DTS/DPF 7.3</p> <p>None are applicable.</p>
<p>PO 7.4</p> <p>Street-level vehicle parking areas incorporate tree planting to provide shade, reduce solar heat absorption and reflection.</p>	<p>DTS/DPF 7.4</p> <p>Vehicle parking areas that are open to the sky and comprise 10 or more car parking spaces include a shade tree with a mature canopy of 4m diameter spaced for each 10 car parking spaces provided and a landscaped strip on any road frontage of a minimum dimension of 1m.</p>
<p>PO 7.5</p> <p>Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.</p>	<p>DTS/DPF 7.5</p> <p>Vehicle parking areas comprising 10 or more car parking spaces include soft landscaping with a minimum dimension of:</p> <ul style="list-style-type: none"> (a) 1m along all public road frontages and allotment boundaries (b) 1m between double rows of car parking spaces.
<p>PO 7.6</p> <p>Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.</p>	<p>DTS/DPF 7.6</p> <p>None are applicable.</p>
<p>PO 7.7</p> <p>Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.</p>	<p>DTS/DPF 7.7</p> <p>None are applicable.</p>
Earthworks and sloping land	
<p>PO 8.1</p> <p>Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.</p>	<p>DTS/DPF 8.1</p> <p>Development does not involve any of the following:</p> <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m (c) a total combined excavation and filling vertical height of 2m or more.
<p>PO 8.2</p> <p>Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.</p>	<p>DTS/DPF 8.2</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):</p> <ul style="list-style-type: none"> (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway (b) are constructed with an all-weather trafficable surface.
<p>PO 8.3</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):</p> <ul style="list-style-type: none"> (a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development 	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>

(c) are designed to integrate with the natural topography of the land.	
PO 8.4 Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.	DTS/DPF 8.4 None are applicable.
PO 8.5 Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.	DTS/DPF 8.5 None are applicable.
Overlooking / Visual Privacy (low rise buildings)	
PO 10.1 Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.	DTS/DPF 10.1 Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone: (a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.
PO 10.2 Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.	DTS/DPF 10.2 One of the following is satisfied: (a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases
Site Facilities / Waste Storage (excluding low rise residential development)	
PO 11.1 Development provides a dedicated area for on-site collection and sorting of recyclable materials and refuse, green organic waste and wash bay facilities for the ongoing maintenance of bins that is adequate in size considering the number and nature of the activities they will serve and the frequency of collection.	DTS/DPF 11.1 None are applicable.
PO 11.2 Communal waste storage and collection areas are located, enclosed and designed to be screened from view from the public domain, open space and dwellings.	DTS/DPF 11.2 None are applicable.
PO 11.3 Communal waste storage and collection areas are designed to be well ventilated and located away from habitable rooms.	DTS/DPF 11.3 None are applicable.
PO 11.4 Communal waste storage and collection areas are designed to allow waste and recycling collection vehicles to enter and leave the site without reversing.	DTS/DPF 11.4 None are applicable.
PO 11.5 For mixed use developments, non-residential waste and recycling storage areas and access provide opportunities for on-site management of food waste through composting or other waste recovery as appropriate.	DTS/DPF 11.5 None are applicable.
All Development - Medium and High Rise	
External Appearance	
PO 12.1 Buildings positively contribute to the character of the local area by responding to local context.	DTS/DPF 12.1 None are applicable.

<p>PO 12.2</p> <p>Architectural detail at street level and a mixture of materials at lower building levels near the public interface are provided to reinforce a human scale.</p>	<p>DTS/DPF 12.2</p> <p>None are applicable.</p>								
<p>PO 12.3</p> <p>Buildings are designed to reduce visual mass by breaking up building elevations into distinct elements.</p>	<p>DTS/DPF 12.3</p> <p>None are applicable.</p>								
<p>PO 12.4</p> <p>Boundary walls visible from public land include visually interesting treatments to break up large blank elevations.</p>	<p>DTS/DPF 12.4</p> <p>None are applicable.</p>								
<p>PO 12.5</p> <p>External materials and finishes are durable and age well to minimise ongoing maintenance requirements.</p>	<p>DTS/DPF 12.5</p> <p>Buildings utilise a combination of the following external materials and finishes:</p> <ul style="list-style-type: none"> (a) masonry (b) natural stone (c) pre-finished materials that minimise staining, discolouring or deterioration. 								
<p>PO 12.6</p> <p>Street-facing building elevations are designed to provide attractive, high quality and pedestrian-friendly street frontages.</p>	<p>DTS/DPF 12.6</p> <p>Building street frontages incorporate:</p> <ul style="list-style-type: none"> (a) active uses such as shops or offices (b) prominent entry areas for multi-storey buildings (where it is a common entry) (c) habitable rooms of dwellings (d) areas of communal public realm with public art or the like, where consistent with the zone and/or subzone provisions. 								
<p>PO 12.7</p> <p>Entrances to multi-storey buildings are safe, attractive, welcoming, functional and contribute to streetscape character.</p>	<p>DTS/DPF 12.7</p> <p>Entrances to multi-storey buildings are:</p> <ul style="list-style-type: none"> (a) oriented towards the street (b) clearly visible and easily identifiable from the street and vehicle parking areas (c) designed to be prominent, accentuated and a welcoming feature if there are no active or occupied ground floor uses (d) designed to provide shelter, a sense of personal address and transitional space around the entry (e) located as close as practicable to the lift and / or lobby access to minimise the need for long access corridors (f) designed to avoid the creation of potential areas of entrapment. 								
<p>PO 12.8</p> <p>Building services, plant and mechanical equipment are screened from the public realm.</p>	<p>DTS/DPF 12.8</p> <p>None are applicable.</p>								
<p>Landscaping</p>									
<p>PO 13.1</p> <p>Development facing a street provides a well landscaped area that contains a deep soil space to accommodate a tree of a species and size adequate to provide shade, contribute to tree canopy targets and soften the appearance of buildings.</p>	<p>DTS/DPF 13.1</p> <p>Buildings provide a 4m by 4m deep soil space in front of the building that accommodates a medium to large tree, except where no building setback from front property boundaries is desired.</p>								
<p>PO 13.2</p> <p>Deep soil zones are provided to retain existing vegetation or provide areas that can accommodate new deep root vegetation, including tall trees with large canopies to provide shade and soften the appearance of multi-storey buildings.</p>	<p>DTS/DPF 13.2</p> <p>Multi-storey development provides deep soil zones and incorporates trees at not less than the following rates, except in a location or zone where full site coverage is desired.</p> <table border="1" data-bbox="823 2007 1544 2123"> <thead> <tr> <th>Site area</th> <th>Minimum deep soil area</th> <th>Minimum dimension</th> <th>Tree / deep soil zones</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	Site area	Minimum deep soil area	Minimum dimension	Tree / deep soil zones				
Site area	Minimum deep soil area	Minimum dimension	Tree / deep soil zones						

	<300 m ²	10 m ²	1.5m	1 small tree / 10 m ²
	300-1500 m ²	7% site area	3m	1 medium tree / 30 m ²
	>1500 m ²	7% site area	6m	1 large or medium tree / 60 m ²
Tree size and site area definitions				
	Small tree	4-6m mature height and 2-4m canopy spread		
	Medium tree	6-12m mature height and 4-8m canopy spread		
	Large tree	12m mature height and >8m canopy spread		
	Site area	The total area for development site, not average area per dwelling		
PO 13.3 Deep soil zones with access to natural light are provided to assist in maintaining vegetation health.	DTS/DPF 13.3 None are applicable.			
PO 13.4 Unless separated by a public road or reserve, development sites adjacent to any zone that has a primary purpose of accommodating low-rise residential development incorporate a deep soil zone along the common boundary to enable medium to large trees to be retained or established to assist in screening new buildings of 3 or more building levels in height.	DTS/DPF 13.4 Building elements of 3 or more building levels in height are set back at least 6m from a zone boundary in which a deep soil zone area is incorporated.			
Environmental				
PO 14.1 Development minimises detrimental micro-climatic impacts on adjacent land and buildings.	DTS/DPF 14.1 None are applicable.			
PO 14.2 Development incorporates sustainable design techniques and features such as window orientation, eaves and shading structures, water harvesting and use, green walls and roof designs that enable the provision of rain water tanks (where they are not provided elsewhere on site), green roofs and photovoltaic cells.	DTS/DPF 14.2 None are applicable.			
PO 14.3 Development of 5 or more building levels, or 21m or more in height (as measured from natural ground level and excluding roof-mounted mechanical plant and equipment) is designed to minimise the impacts of wind through measures such as: <ul style="list-style-type: none"> (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street (b) substantial verandahs around a building to deflect downward travelling wind flows over pedestrian areas (c) the placement of buildings and use of setbacks to deflect the wind at ground level (d) avoiding tall shear elevations that create windy conditions at street level. 	DTS/DPF 14.3 None are applicable.			
Car Parking				
PO 15.1 Multi-level vehicle parking structures are designed to contribute to active	DTS/DPF 15.1 Multi-level vehicle parking structures within buildings:			

<p>street frontages and complement neighbouring buildings.</p>	<ul style="list-style-type: none"> (a) provide land uses such as commercial, retail or other non-car parking uses along ground floor street frontages (b) incorporate facade treatments in building elevations facing along major street frontages that are sufficiently enclosed and detailed to complement adjacent buildings.
<p>PO 15.2 Multi-level vehicle parking structures within buildings complement the surrounding built form in terms of height, massing and scale.</p>	<p>DTS/DPF 15.2 None are applicable.</p>
<p>Overlooking/Visual Privacy</p>	
<p>PO 16.1 Development mitigates direct overlooking of habitable rooms and private open spaces of adjacent residential uses in neighbourhood-type zones through measures such as:</p> <ul style="list-style-type: none"> (a) appropriate site layout and building orientation (b) off-setting the location of balconies and windows of habitable rooms or areas with those of other buildings so that views are oblique rather than direct to avoid direct line of sight (c) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms (d) screening devices that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity. 	<p>DTS/DPF 16.1 None are applicable.</p>
<p>All non-residential development</p>	
<p>Water Sensitive Design</p>	
<p>PO 42.1 Development likely to result in risk of export of sediment, suspended solids, organic matter, nutrients, oil and grease include stormwater management systems designed to minimise pollutants entering stormwater.</p>	<p>DTS/DPF 42.1 None are applicable.</p>
<p>PO 42.2 Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.</p>	<p>DTS/DPF 42.2 None are applicable.</p>
<p>PO 42.3 Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that development does not increase peak flows in downstream systems.</p>	<p>DTS/DPF 42.3 None are applicable.</p>
<p>Wash-down and Waste Loading and Unloading</p>	
<p>PO 43.1 Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, plant or equipment are:</p> <ul style="list-style-type: none"> (a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off (b) paved with an impervious material to facilitate wastewater collection (c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area (d) are designed to drain wastewater to either: <ul style="list-style-type: none"> (i) a treatment device such as a sediment trap and coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme or (ii) a holding tank and its subsequent removal off-site on a regular basis. 	<p>DTS/DPF 43.1 None are applicable.</p>
<p>Laneway Development</p>	
<p>Infrastructure and Access</p>	

<p>PO 44.1</p> <p>Development with a primary street comprising a laneway, alley, lane, right of way or similar minor thoroughfare only occurs where:</p> <ul style="list-style-type: none"> (a) existing utility infrastructure and services are capable of accommodating the development (b) the primary street can support access by emergency and regular service vehicles (such as waste collection) (c) it does not require the provision or upgrading of infrastructure on public land (such as footpaths and stormwater management systems) (d) safety of pedestrians or vehicle movement is maintained (e) any necessary grade transition is accommodated within the site of the development to support an appropriate development intensity and orderly development of land fronting minor thoroughfares. 	<p>DTS/DPF 44.1</p> <p>Development with a primary street frontage that is not an alley, lane, right of way or similar public thoroughfare.</p>
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Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature								
Hours of Operation									
<p>PO 2.1</p> <p>Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:</p> <ul style="list-style-type: none"> (a) the nature of the development (b) measures to mitigate off-site impacts (c) the extent to which the development is desired in the zone (d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land. 	<p>DTS/DPF 2.1</p> <p>Development operating within the following hours:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="width: 50%; text-align: center; padding: 5px;">Class of Development</th> <th style="width: 50%; text-align: center; padding: 5px;">Hours of operation</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;">Consulting room</td> <td style="padding: 5px;">7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td style="padding: 5px;">Office</td> <td style="padding: 5px;">7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td style="padding: 5px;">Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</td> <td style="padding: 5px;">7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday</td> </tr> </tbody> </table>	Class of Development	Hours of operation	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday
Class of Development	Hours of operation								
Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday								
Shop, other than any one or combination of the following: (a) restaurant (b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone	7am to 9pm, Monday to Friday 8am to 5pm, Saturday and Sunday								
Overshadowing									
<p>PO 3.1</p> <p>Overshadowing of habitable room windows of adjacent residential land uses in:</p>	<p>DTS/DPF 3.1</p> <p>North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight</p>								

<p>a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>between 9.00am and 3.00pm on 21 June.</p>				
<p>PO 3.2</p> <p>Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in:</p> <p>a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>DTS/DPF 3.2</p> <p>Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following:</p> <p>a. for ground level private open space, the smaller of the following:</p> <p>i. half the existing ground level open space</p> <p>or</p> <p>ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m)</p> <p>b. for ground level communal open space, at least half of the existing ground level open space.</p>				
<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <p>(a) the form of development contemplated in the zone</p> <p>(b) the orientation of the solar energy facilities</p> <p>(c) the extent to which the solar energy facilities are already overshadowed.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>				
<p>Activities Generating Noise or Vibration</p>					
<p>PO 4.1</p> <p>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.1</p> <p>Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.</p>				
<p>PO 4.2</p> <p>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</p> <p>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</p> <p>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</p> <p>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</p> <p>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>				
<p>PO 4.5</p> <p>Outdoor areas associated with licensed premises (such as beer gardens or dining areas) are designed and/or sited to not cause unreasonable noise impact on existing adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.5</p> <p>None are applicable.</p>				
<p>PO 4.6</p> <p>Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 4.6</p> <p>Development incorporating music includes noise attenuation measures that will achieve the following noise levels:</p> <table border="1" style="width: 100%; margin-top: 10px;"> <thead> <tr> <th style="width: 50%;">Assessment location</th> <th style="width: 50%;">Music noise level</th> </tr> </thead> <tbody> <tr> <td>Externally at the nearest</td> <td>Less than 8dB above the level of</td> </tr> </tbody> </table>	Assessment location	Music noise level	Externally at the nearest	Less than 8dB above the level of
Assessment location	Music noise level				
Externally at the nearest	Less than 8dB above the level of				

	existing or envisaged noise sensitive location	background noise (L _{90,15min}) in any octave band of the sound spectrum (LOCT _{10,15} < LOCT _{90,15} + 8dB)
Air Quality		
PO 5.2 Development that includes chimneys or exhaust flues (including cafes, restaurants and fast food outlets) is designed to minimise nuisance or adverse health impacts to sensitive receivers (or lawfully approved sensitive receivers) by: (a) incorporating appropriate treatment technology before exhaust emissions are released (b) locating and designing chimneys or exhaust flues to maximise the dispersion of exhaust emissions, taking into account the location of sensitive receivers.	DTS/DPF 5.2 None are applicable.	
Light Spill		
PO 6.1 External lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or lawfully approved sensitive receivers).	DTS/DPF 6.1 None are applicable.	
Solar Reflectivity / Glare		
PO 7.1 Development is designed and comprised of materials and finishes that do not unreasonably cause a distraction to adjacent road users and pedestrian areas or unreasonably cause heat loading and micro-climatic impacts on adjacent buildings and land uses as a result of reflective solar glare.	DTS/DPF 7.1 None are applicable.	

Out of Activity Centre Development

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO1	The role of Activity Centres in contributing to the form and pattern of development and enabling equitable and convenient access to a range of shopping, administrative, cultural, entertainment and other facilities in a single trip is maintained and reinforced.

Performance Outcomes and Deemed to Satisfy / Designated Performance Outcome Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Non-residential development outside Activity Centres of a scale and type that does not diminish the role of Activity Centres: (a) as primary locations for shopping, administrative, cultural, entertainment and community services (b) as a focus for regular social and business gatherings (c) in contributing to or maintaining a pattern of development that supports equitable community access to services and facilities.	DTS/DPF 1.1 None are applicable.
PO 1.2 Out-of-activity centre non-residential development complements Activity Centres through the provision of services and facilities: (a) that support the needs of local residents and workers, particularly in underserved locations	DTS/DPF 1.2 None are applicable.

(b) at the edge of Activities Centres where they cannot readily be accommodated within an existing Activity Centre to expand the range of services on offer and support the role of the Activity Centre.	
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Site Contamination

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Ensure land is suitable for the proposed use in circumstances where it is, or may have been, subject to site contamination.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 Ensure land is suitable for use when land use changes to a more sensitive use.	DTS/DPF 1.1 Development satisfies (a), (b), (c) or (d): <ul style="list-style-type: none"> (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a more sensitive use (c) involves a change in the use of land to a more sensitive use on land at which site contamination is unlikely to exist (as demonstrated in a site contamination declaration form) (d) involves a change in the use of land to a more sensitive use on land at which site contamination exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: <ul style="list-style-type: none"> (i) a site contamination audit report has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that- <ul style="list-style-type: none"> A. site contamination does not exist (or no longer exists) at the land or B. the land is suitable for the proposed use or range of uses (without the need for any further remediation) or C. where remediation is, or remains, necessary for the proposed use (or range of uses), remediation work has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development) and (ii) no other class 1 activity or class 2 activity has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a site contamination declaration form).

Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome

DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Movement Systems	
PO 1.2 Development is designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.4 Development is sited and designed so that loading, unloading and turning of all traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.	DTS/DPF 1.4 All vehicle manoeuvring occurs onsite.
Sightlines	
PO 2.1 Sightlines at intersections, pedestrian and cycle crossings, and crossovers to allotments for motorists, cyclists and pedestrians are maintained or enhanced to ensure safety for all road users and pedestrians.	DTS/DPF 2.1 None are applicable.
PO 2.2 Walls, fencing and landscaping adjacent to driveways and corner sites are designed to provide adequate sightlines between vehicles and pedestrians.	DTS/DPF 2.2 None are applicable.
Vehicle Access	
PO 3.1 Safe and convenient access minimises impact or interruption on the operation of public roads.	DTS/DPF 3.1 The access is: (a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land or (b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.
PO 3.2 Development incorporating vehicular access ramps ensures vehicles can enter and exit a site safely and without creating a hazard to pedestrians and other vehicular traffic.	DTS/DPF 3.2 None are applicable.
PO 3.3 Access points are sited and designed to accommodate the type and volume of traffic likely to be generated by the development or land use.	DTS/DPF 3.3 None are applicable.
PO 3.4 Access points are sited and designed to minimise any adverse impacts on neighbouring properties.	DTS/DPF 3.4 None are applicable.
PO 3.5	DTS/DPF 3.5

<p>Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.</p>	<p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: <ul style="list-style-type: none"> (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
<p>PO 3.6</p> <p>Driveways and access points are separated and minimised in number to optimise the provision of on-street visitor parking (where on-street parking is appropriate).</p>	<p>DTS/DPF 3.6</p> <p>Driveways and access points:</p> <ul style="list-style-type: none"> (a) for sites with a frontage to a public road of 20m or less, one access point no greater than 3.5m in width is provided (b) for sites with a frontage to a public road greater than 20m: <ul style="list-style-type: none"> (i) a single access point no greater than 6m in width is provided or (ii) not more than two access points with a width of 3.5m each are provided.
<p>PO 3.7</p> <p>Access points are appropriately separated from level crossings to avoid interference and ensure their safe ongoing operation.</p>	<p>DTS/DPF 3.7</p> <p>Development does not involve a new or modified access or cause an increase in traffic through an existing access that is located within the following distance from a railway crossing:</p> <ul style="list-style-type: none"> (a) 80 km/h road - 110m (b) 70 km/h road - 90m (c) 60 km/h road - 70m (d) 50km/h or less road - 50m.
<p>PO 3.8</p> <p>Driveways, access points, access tracks and parking areas are designed and constructed to allow adequate movement and manoeuvrability having regard to the types of vehicles that are reasonably anticipated.</p>	<p>DTS/DPF 3.8</p> <p>None are applicable.</p>
<p>PO 3.9</p> <p>Development is designed to ensure vehicle circulation between activity areas occurs within the site without the need to use public roads.</p>	<p>DTS/DPF 3.9</p> <p>None are applicable.</p>
<p>Access for People with Disabilities</p>	
<p>PO 4.1</p> <p>Development is sited and designed to provide safe, dignified and convenient access for people with a disability.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>Vehicle Parking Rates</p>	
<p>PO 5.1</p> <p>Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:</p> <ul style="list-style-type: none"> (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place. 	<p>DTS/DPF 5.1</p> <p>Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:</p> <ul style="list-style-type: none"> (a) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas if the development is a class of development listed in Table 2 and the site is in a Designated Area (b) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements where (a) does not apply (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.

Vehicle Parking Areas	
PO 6.1 Vehicle parking areas are sited and designed to minimise impact on the operation of public roads by avoiding the use of public roads when moving from one part of a parking area to another.	DTS/DPF 6.1 Movement between vehicle parking areas within the site can occur without the need to use a public road.
PO 6.2 Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced, and the like.	DTS/DPF 6.2 None are applicable.
PO 6.3 Vehicle parking areas are designed to provide opportunity for integration and shared-use of adjacent car parking areas to reduce the total extent of vehicle parking areas and access points.	DTS/DPF 6.3 None are applicable.
PO 6.4 Pedestrian linkages between parking areas and the development are provided and are safe and convenient.	DTS/DPF 6.4 None are applicable.
PO 6.5 Vehicle parking areas that are likely to be used during non-daylight hours are provided with sufficient lighting to entry and exit points to ensure clear visibility to users.	DTS/DPF 6.5 None are applicable.
PO 6.6 Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.	DTS/DPF 6.6 Loading areas and designated parking spaces are wholly located within the site.
Undercroft and Below Ground Garaging and Parking of Vehicles	
PO 7.1 Undercroft and below ground garaging of vehicles is designed to enable safe entry and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles.	DTS/DPF 7.1 None are applicable.
Bicycle Parking in Designated Areas	
PO 9.1 The provision of adequately sized on-site bicycle parking facilities encourages cycling as an active transport mode.	DTS/DPF 9.1 Areas and / or fixtures are provided for the parking and storage of bicycles at a rate not less than the amount calculated using Transport, Access and Parking Table 3 - Off Street Bicycle Parking Requirements.
PO 9.2 Bicycle parking facilities provide for the secure storage and tethering of bicycles in a place where casual surveillance is possible, is well lit and signed for the safety and convenience of cyclists and deters property theft.	DTS/DPF 9.2 None are applicable.
PO 9.3 Non-residential development incorporates end-of-journey facilities for employees such as showers, changing facilities and secure lockers, and signage indicating the location of the facilities to encourage cycling as a mode of journey-to-work transport.	DTS/DPF 9.3 None are applicable.
Corner Cut-Offs	
PO 10.1 Development is located and designed to ensure drivers can safely turn into and out of public road junctions.	DTS/DPF 10.1 Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:

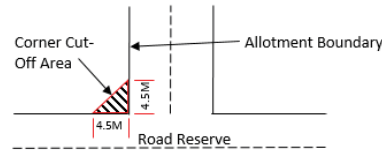


Table 1 - General Off-Street Car Parking Requirements

Class of Development	Car Parking Rate (unless varied by Table 2 onwards)
Health Related Uses	
Consulting room	4 spaces per consulting room excluding ancillary facilities.

Table 2 - Off-Street Car Parking Requirements in Designated Areas

Class of Development	Car Parking Rate		Designated Areas
	Minimum number of spaces	Maximum number of spaces	
Non-residential development			
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	6 spaces per 100m ² of gross leasable floor area.	Strategic Innovation Zone in the City of Burnside, City of Marion or City of Mitcham Strategic Innovation Zone outside the City of Burnside, City of Marion or City of Mitcham when the site is also in a high frequency public transit area Suburban Activity Centre Zone when the site is also in a high frequency public transit area Suburban Business Zone when the site is also in a high frequency public

			<p>transit area</p> <p>Business Neighbourhood Zone in the City of Adelaide</p> <p>Business Neighbourhood Zone outside of the City of Adelaide when the site is also in a high frequency public transit area</p> <p>Suburban Main Street Zone when the site is also in a high frequency public transit area</p> <p>Urban Activity Centre Zone</p>
--	--	--	--

Table 3 - Off-Street Bicycle Parking Requirements

Class of Development	Bicycle Parking Rate	
	<p>Where a development comprises more than one development type, then the overall bicycle parking rate will be taken to be the sum of the bicycle parking rates for each development type.</p>	
Consulting room	1 space per 20 employees plus 1 space per 20 consulting rooms for customers.	
Schedule to Table 3	Designated Area	Relevant part of the State
		The bicycle parking rate applies to a designated area located in a relevant part of the State described below.
	All zones	City of Adelaide
	Business Neighbourhood Zone	Metropolitan Adelaide
	Strategic Innovation Zone	
	Suburban Activity Centre Zone	
	Suburban Business Zone	
	Suburban Main Street Zone	
	Urban Activity Centre Zone	
	Urban Corridor (Boulevard) Zone	
	Urban Corridor (Business) Zone	
Urban Corridor (Living) Zone		
Urban Corridor (Main Street) Zone		
Urban Neighbourhood Zone		



120 Rundle Street Kent Town
Presentation: Planning Application
October 05th Revision: 2743_PA.06

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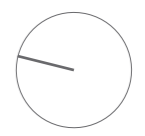
Alana & Andrew Campbell-Lloyd

120 Rundle Street, Kent Town

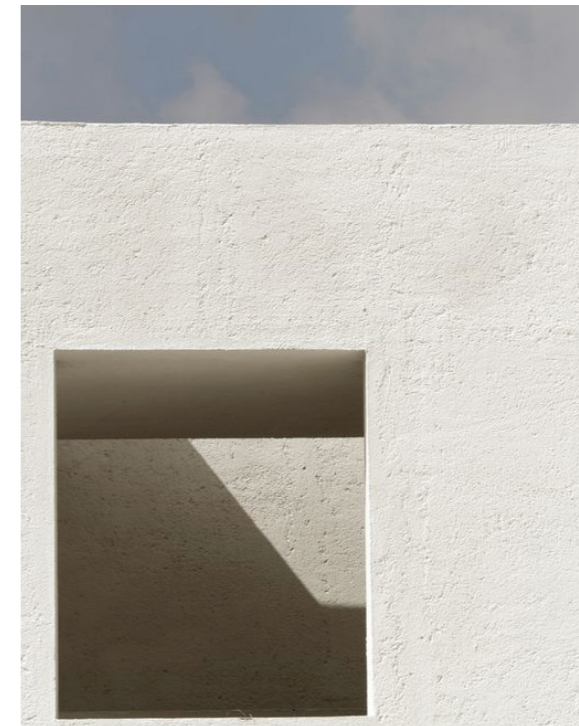
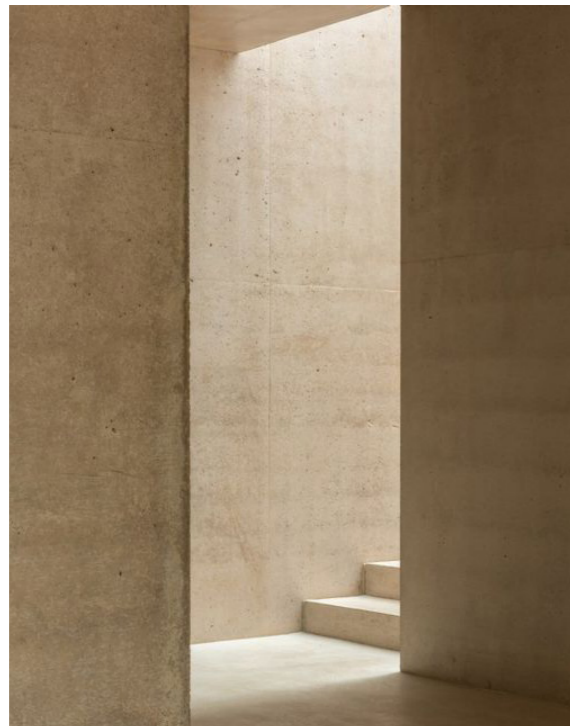
Presentation: Planning Application

05th October 2023

Revision: 2743_Planning Application

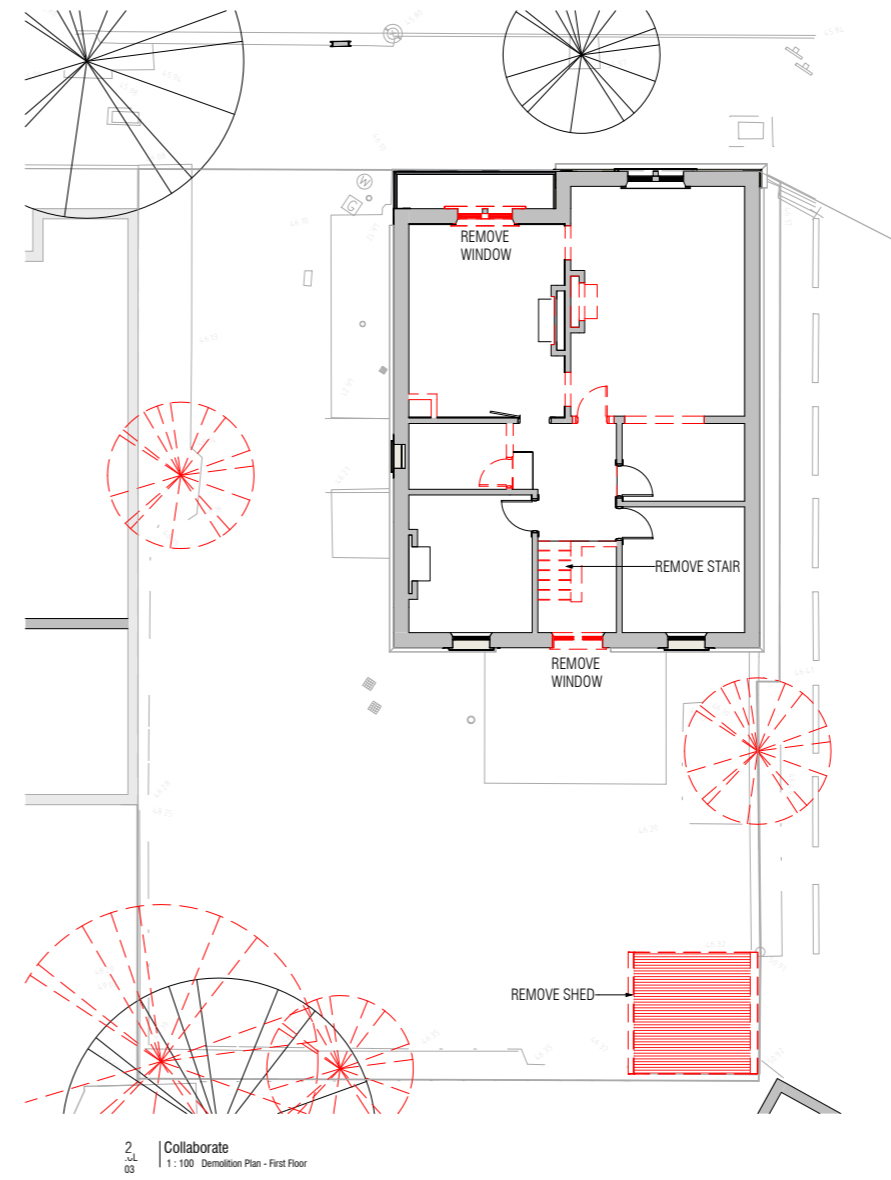
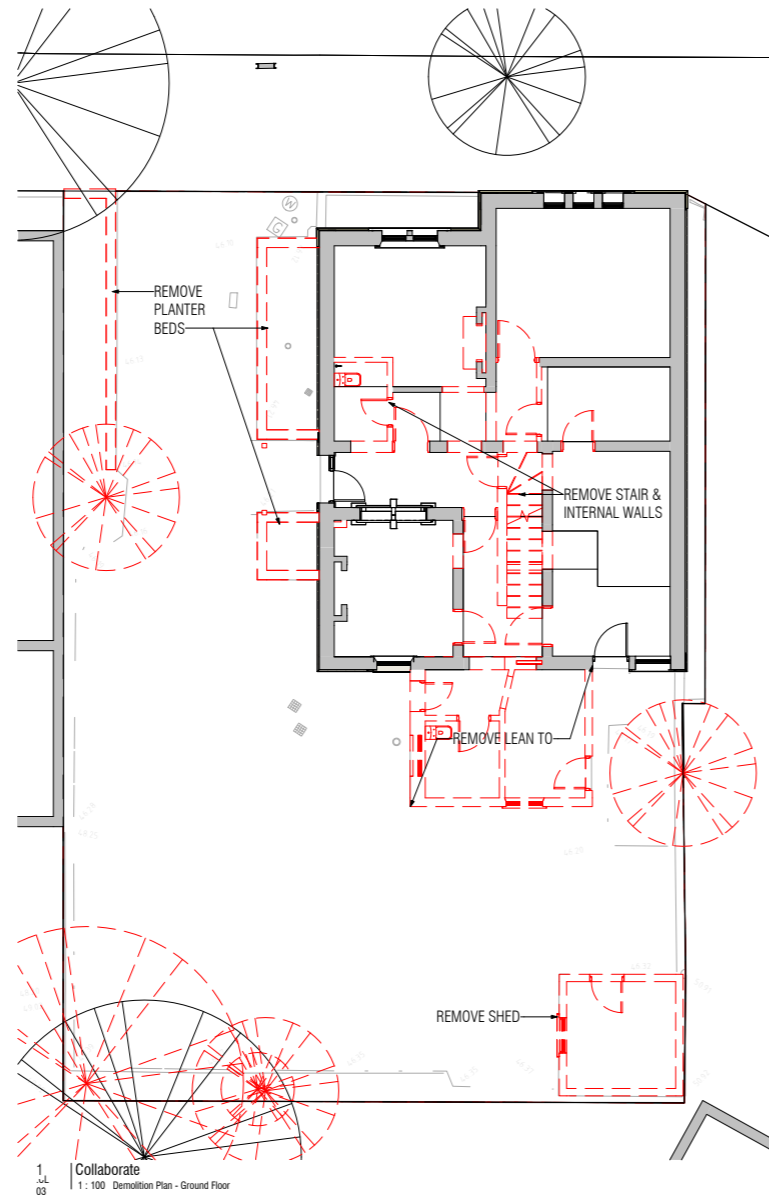


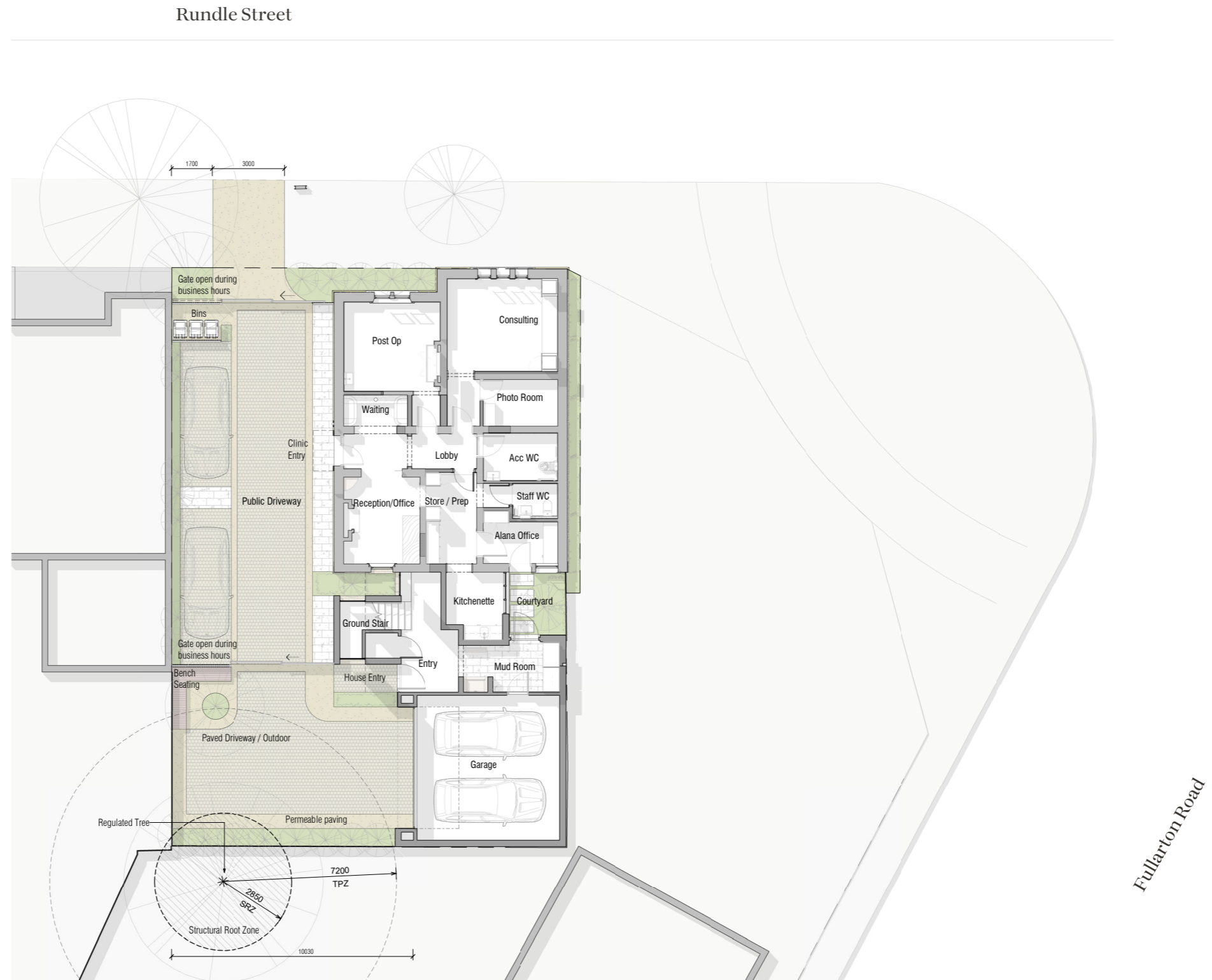
Our Design Narrative is informed by the overarching principles of how you want it to feel and what you wish your practice and home to be. From our conversations, we believe the fundamental principles are; A home that champions efficient planning and environmental principles. A non-conventional medical practice that embodies your values and progressive approach to work. There will be a clear delineation between work and home, but they will both utilise functional and utilitarian materials. Below are the guiding principles that will help us represent those values.

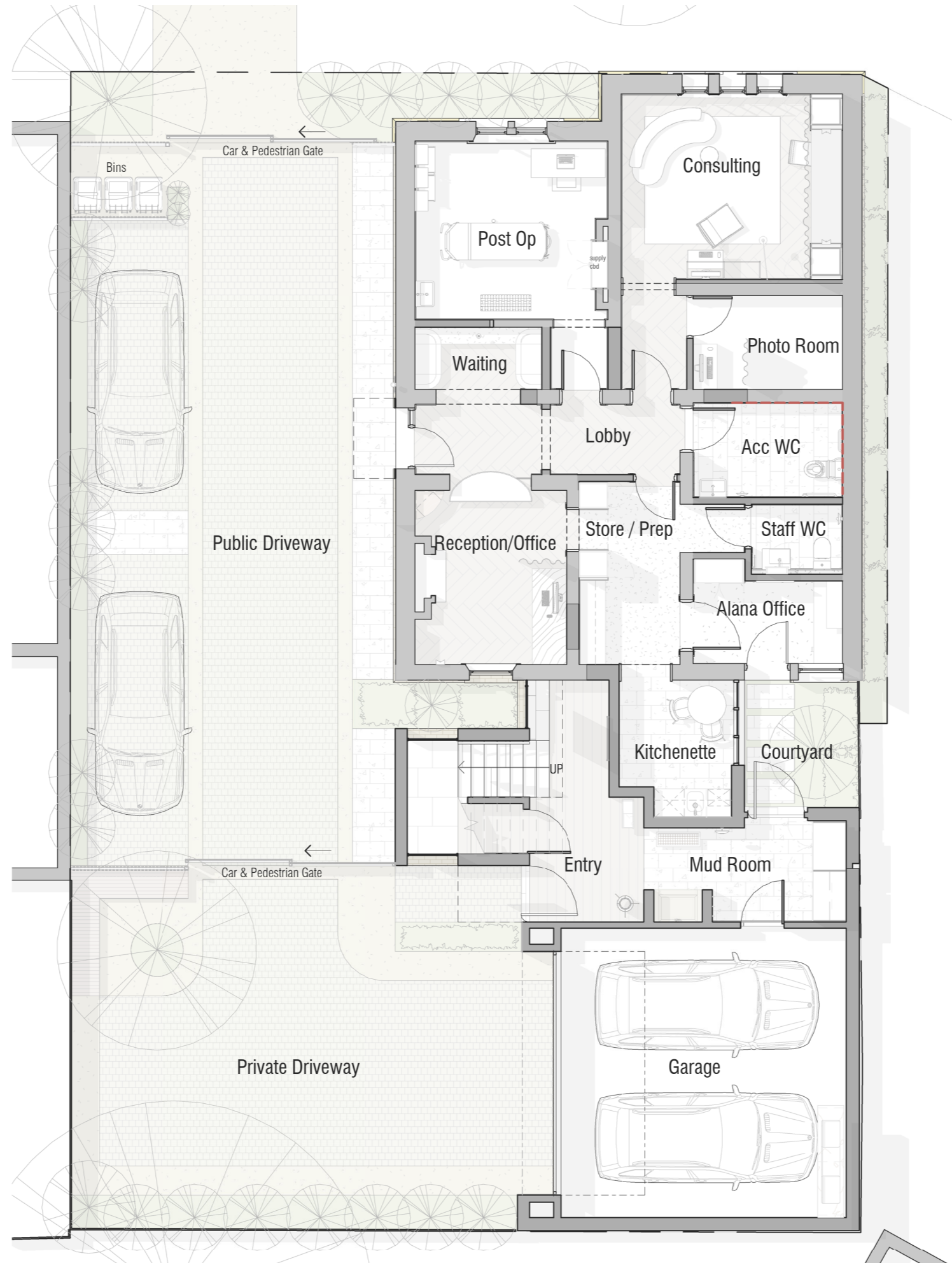


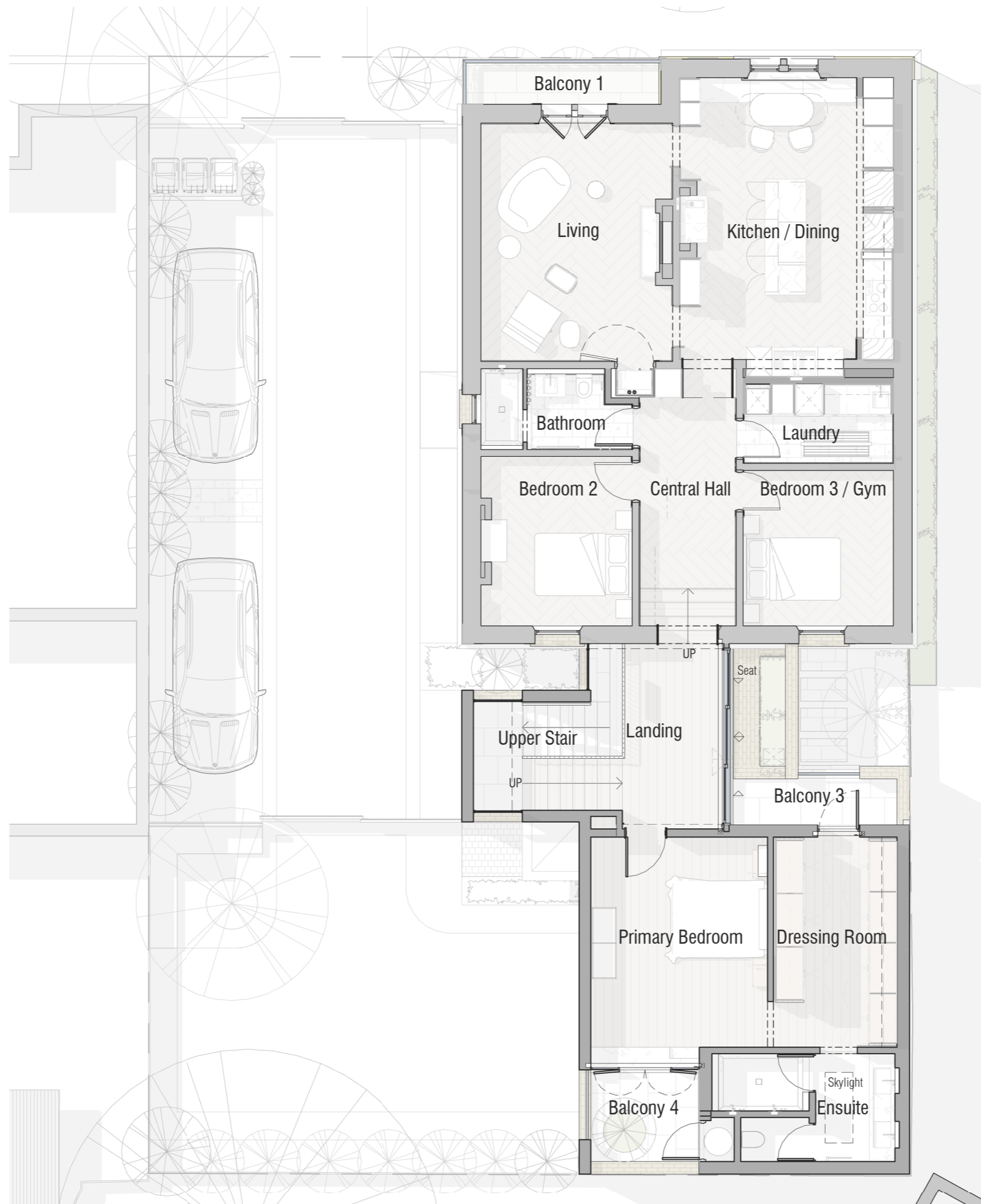
Order & Calm

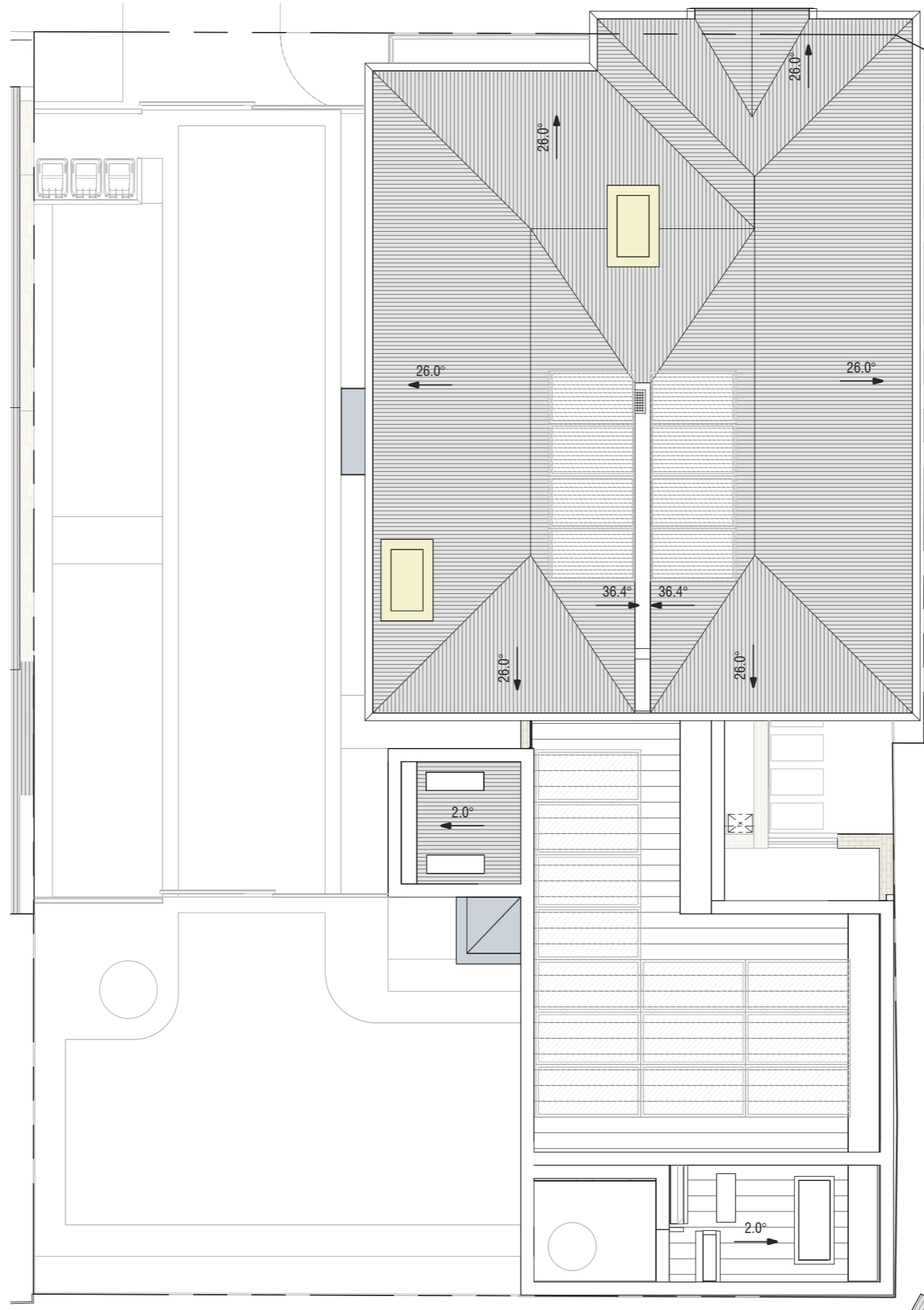
A re-imagined footprint that prioritises cohesion in its planning and forms to counter the natural unpredictability of life & work. The cohesion will provide an overall sense of everything important coming elegantly together.







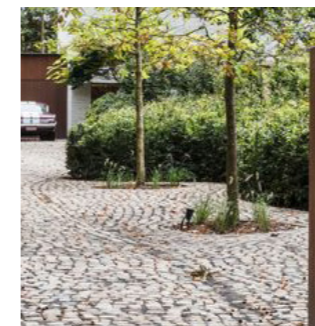




Attachment 1



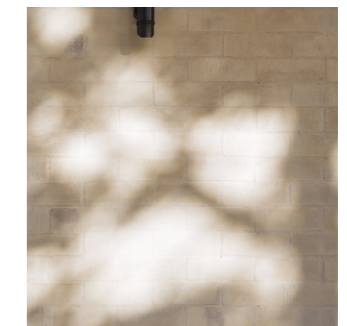
Garage Doors —
Hardwood timber
cladding



Driveway —
Cobblestone & planting



Open balustrade — Fine
open steel

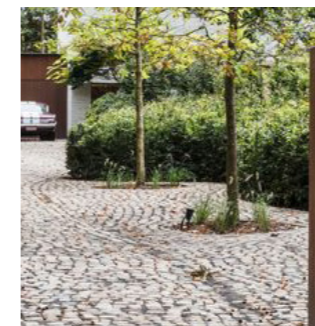


New walls — Sealed
Bagged Brick

Attachment 1



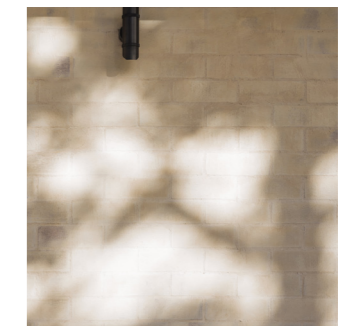
Garage Doors —
Hardwood timber
cladding



Driveway —
Cobblestone & planting



Open balustrade — Fine
open steel



New walls — Sealed
Bagged Brick

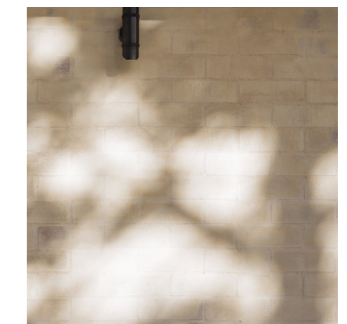
Attachment 1



Balcony — Hanging plants to courtyard



Open balustrade — Fine open steel



New walls — Sealed Bagged Brick



West Elevation



South Elevation



North Elevation



East Elevation



01 / BAGGED BRICK EXTERNAL WALLS



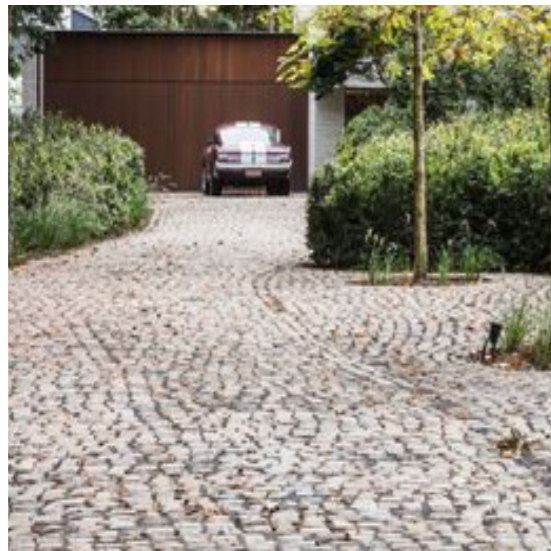
02 / BRONZE PAINTED STEEL WINDOWS



03 / RENDERED MASONRY PARAPET



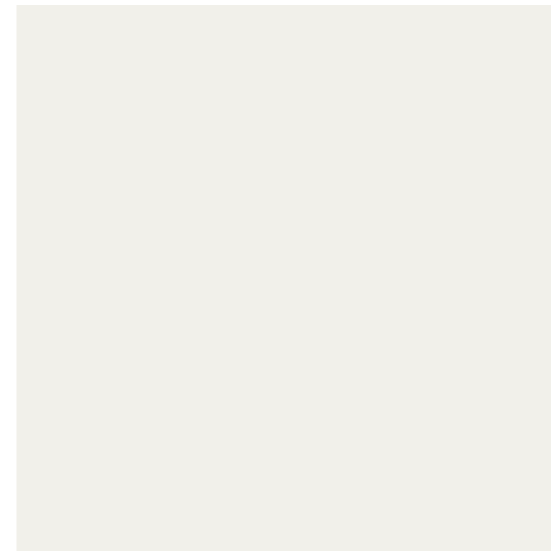
04 / BRONZE PAINTED STEEL BALUSTRADE



05 / COBBLE LIMESTONE PAVED DRIVEWAY



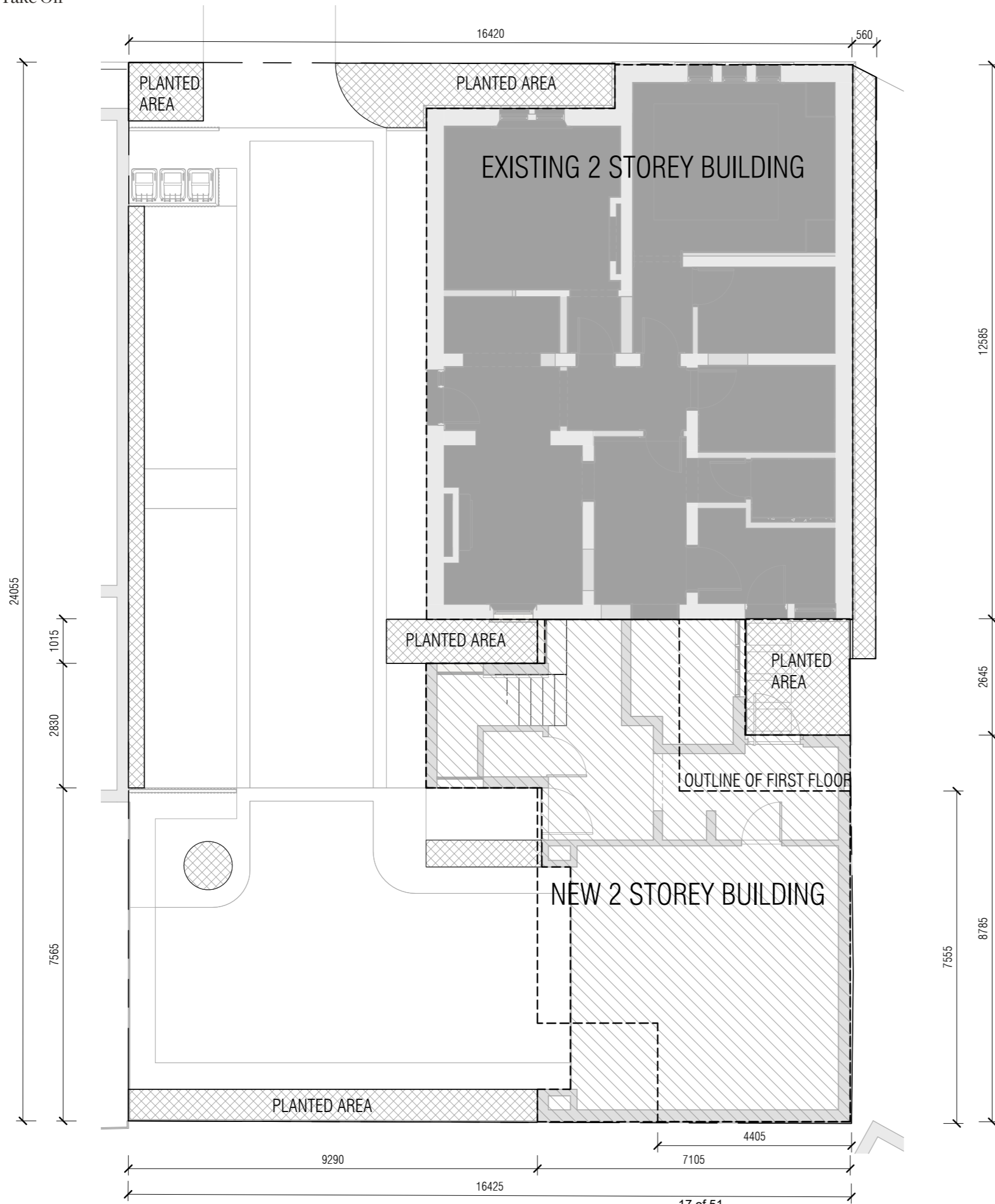
06 / HARDWOOD CLADDING TO GARAGE



07 / LIGHT GREY PAINT TO FASCIAS



08 / COLORBOND SURFMIST ROOF DECKING



SITE AREAS

SITE:		406sqm
EXISTING BUILDING:	(117 x 2sqm)	234sqm
NEW BUILDING:	(81 + 67sqm)	148sqm
OVERALL BUILDING:		384sqm
SITE COVERAGE:	(198/406sqm)	49%
OPEN AREA:	(208/406sqm)	51%
PLANTED AREA:		40sqm

Carparking Diagrams 01 scale - 1:150

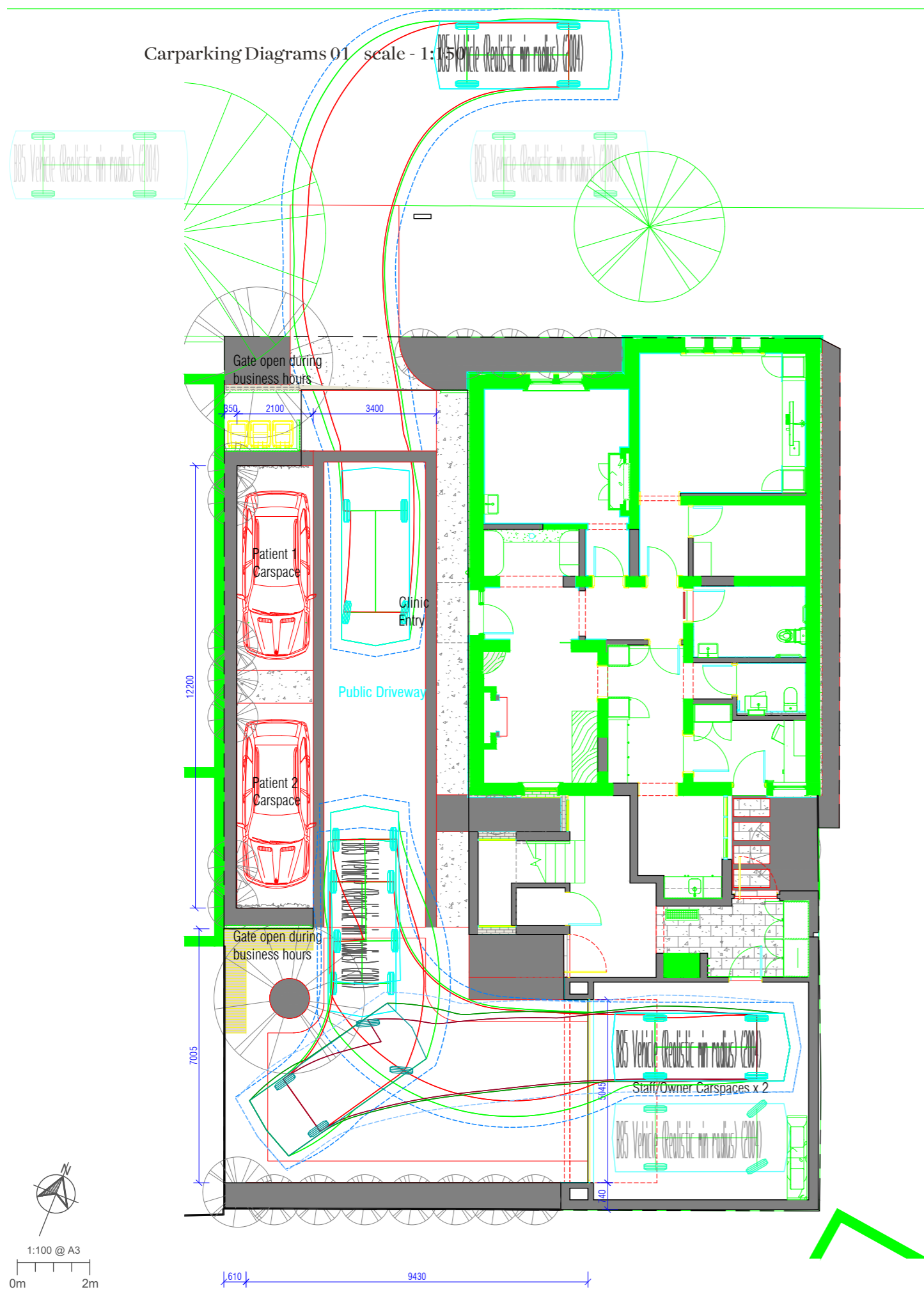


FIGURE A - Site entry and northern garage space accessibility

120 Rundle Street, Kent Town - File: 23-232 - 19/09/23

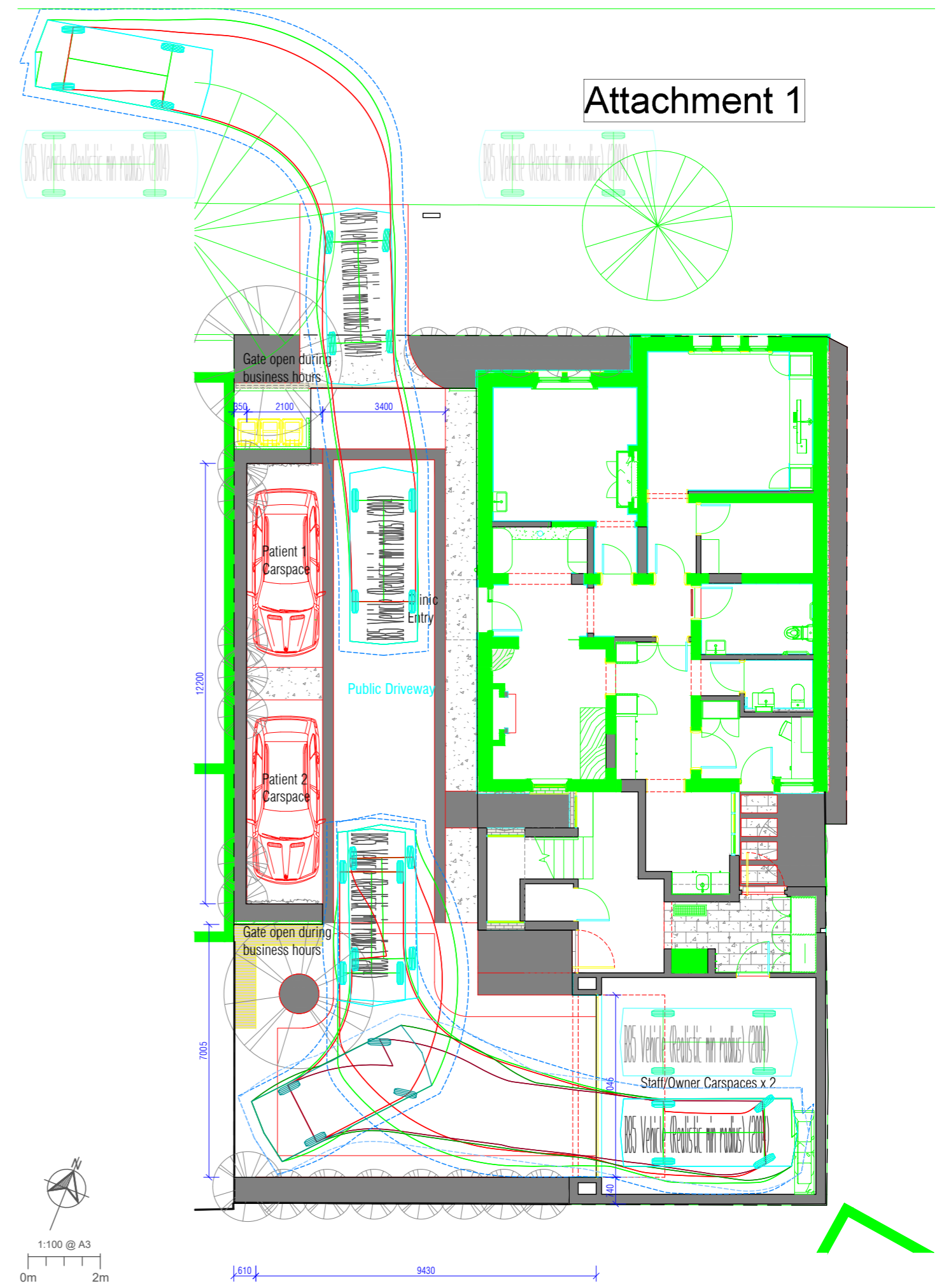


FIGURE B - Site exit and southern garage space accessibility

120 Rundle Street, Kent Town - File: 23-232 - 19/09/23

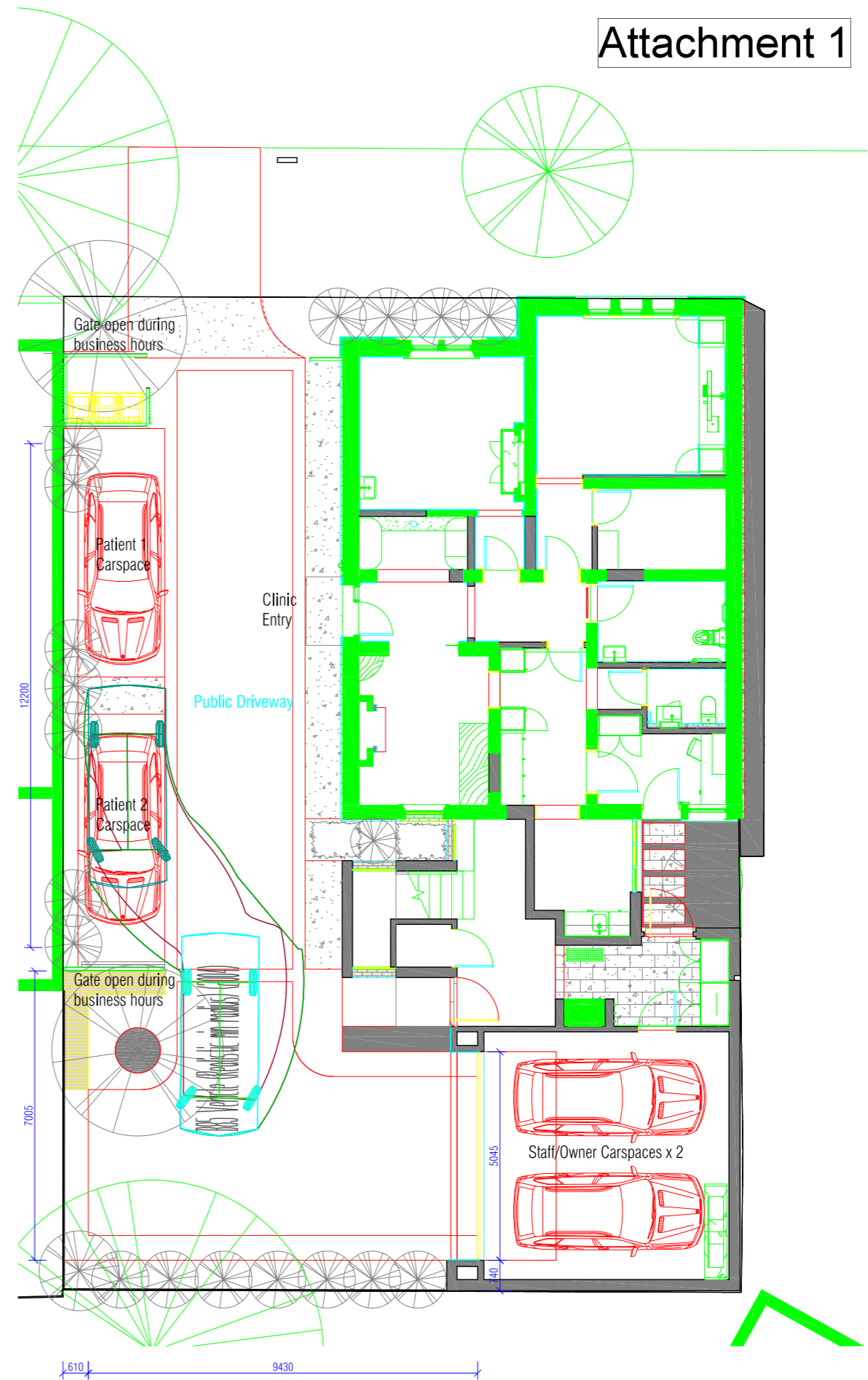
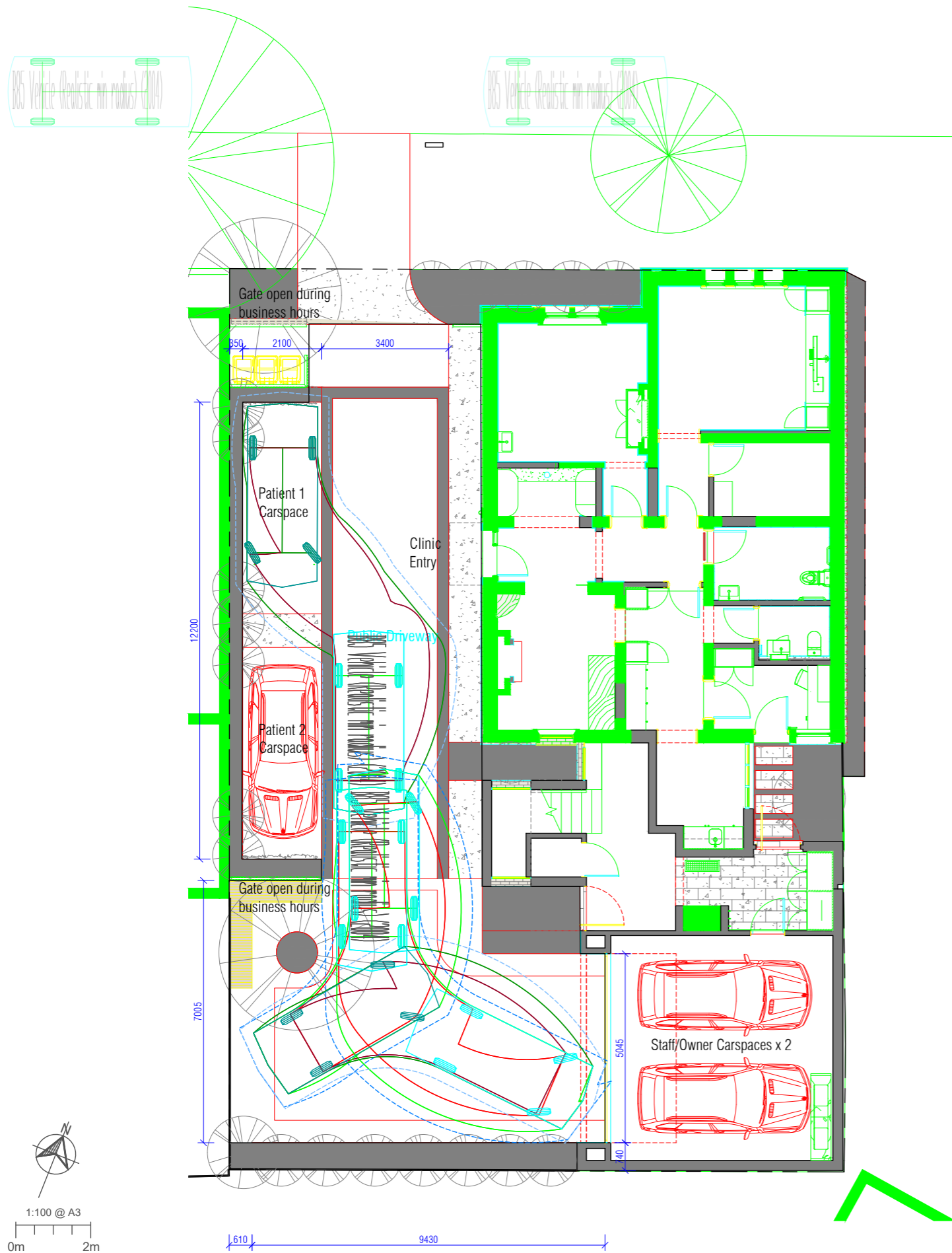


FIGURE C - Parallel space accessibility and on-site turnaround

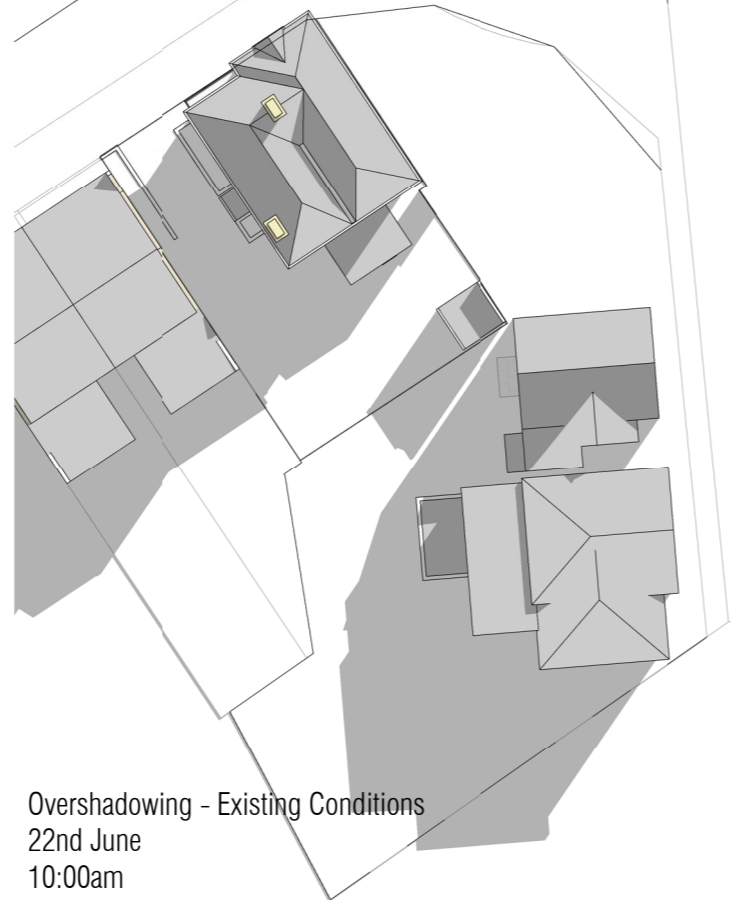
120 Rundle Street, Kent Town - File: 23-232 - 19/09/23

2ND PATIENT SPACE

Overshadowing Diagrams - Existing Conditions - 22nd June



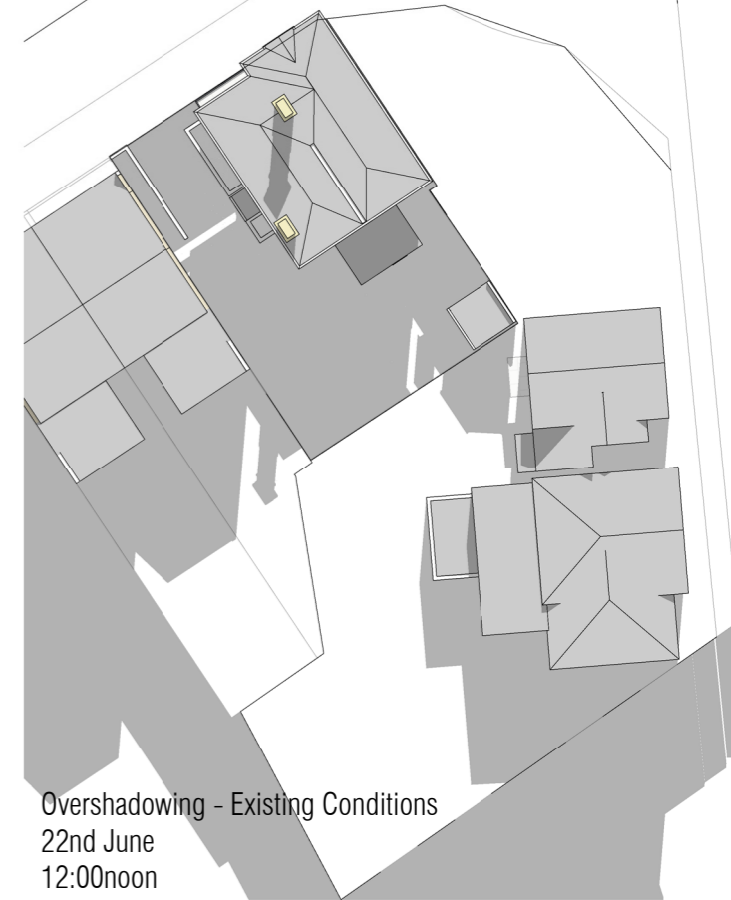
Overshadowing - Existing Conditions
22nd June
9:00am



Overshadowing - Existing Conditions
22nd June
10:00am



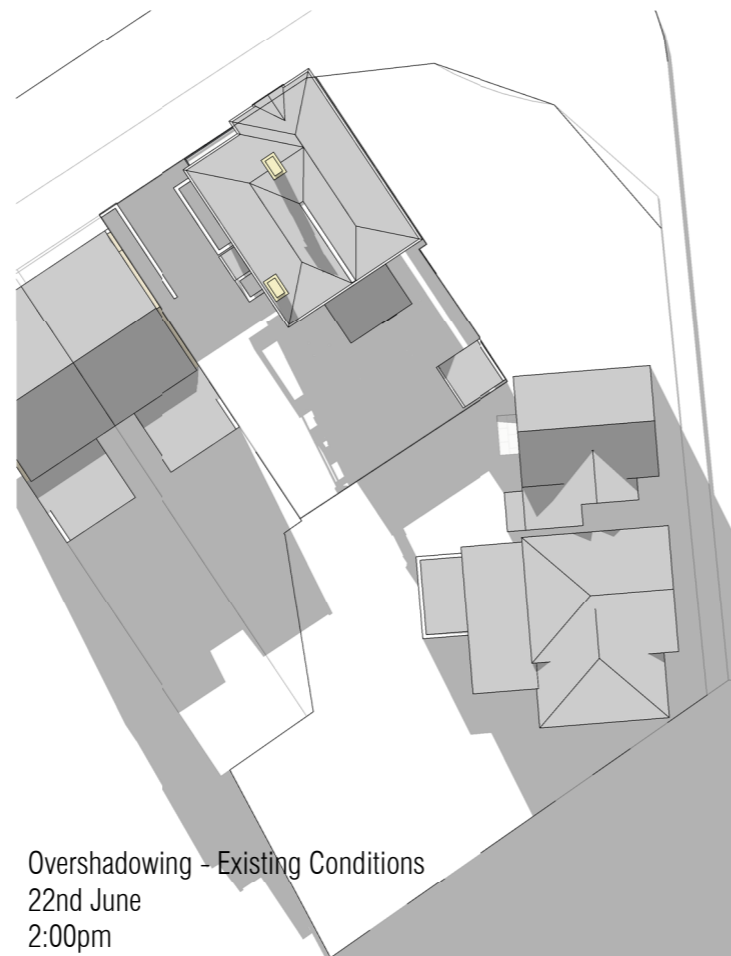
Overshadowing - Existing Conditions
22nd June
11:00am



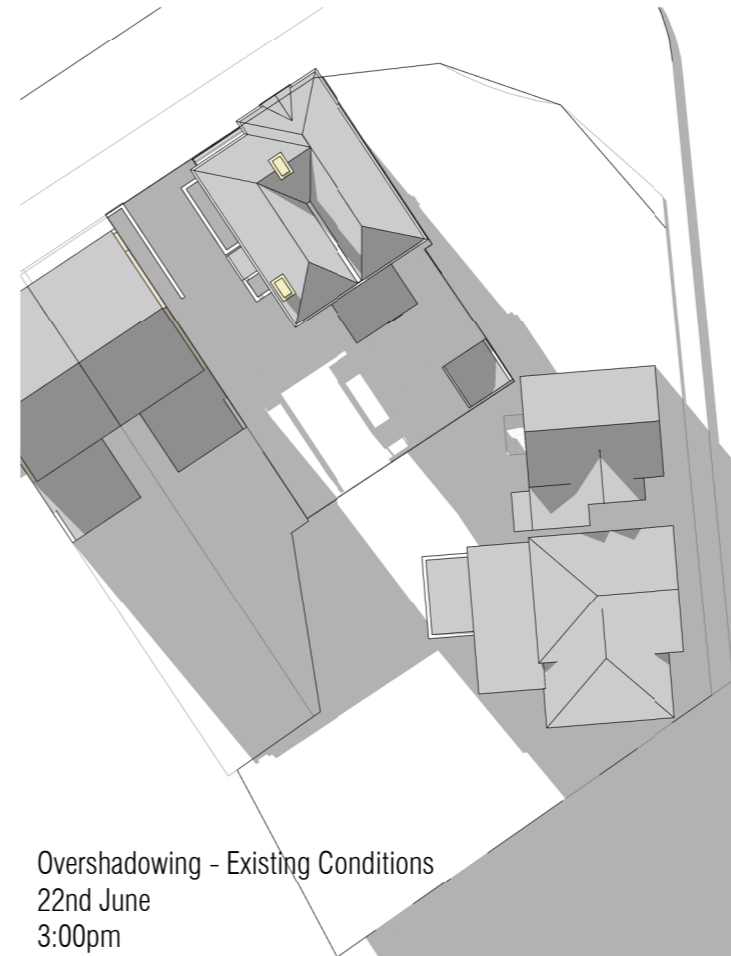
Overshadowing - Existing Conditions
22nd June
12:00noon



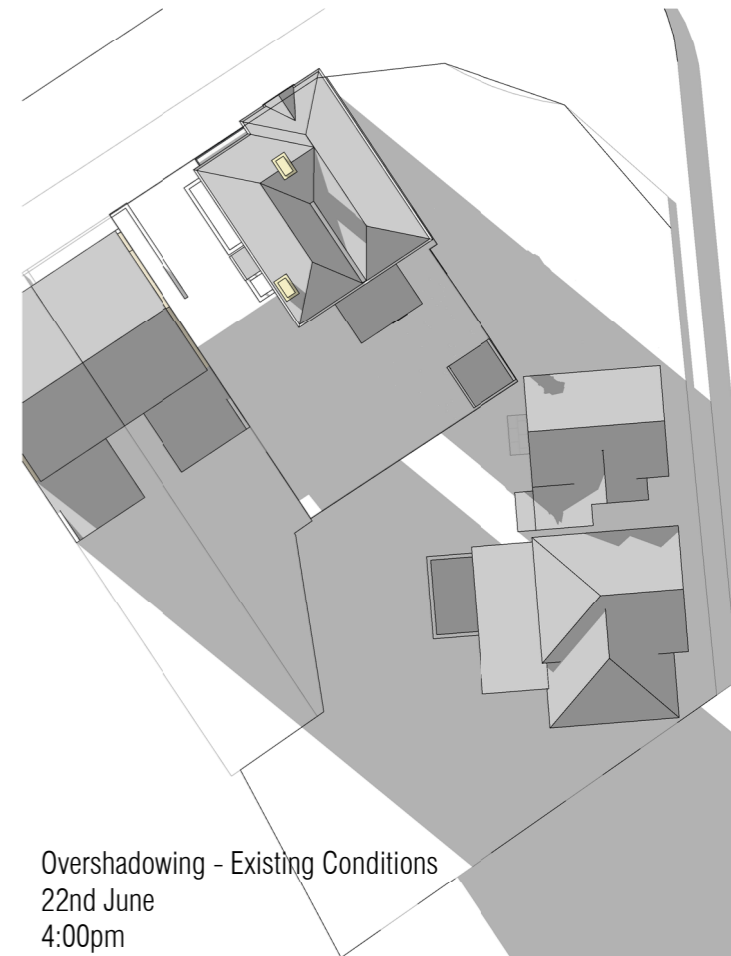
Overshadowing - Existing Conditions
22nd June
1:00pm



Overshadowing - Existing Conditions
22nd June
2:00pm

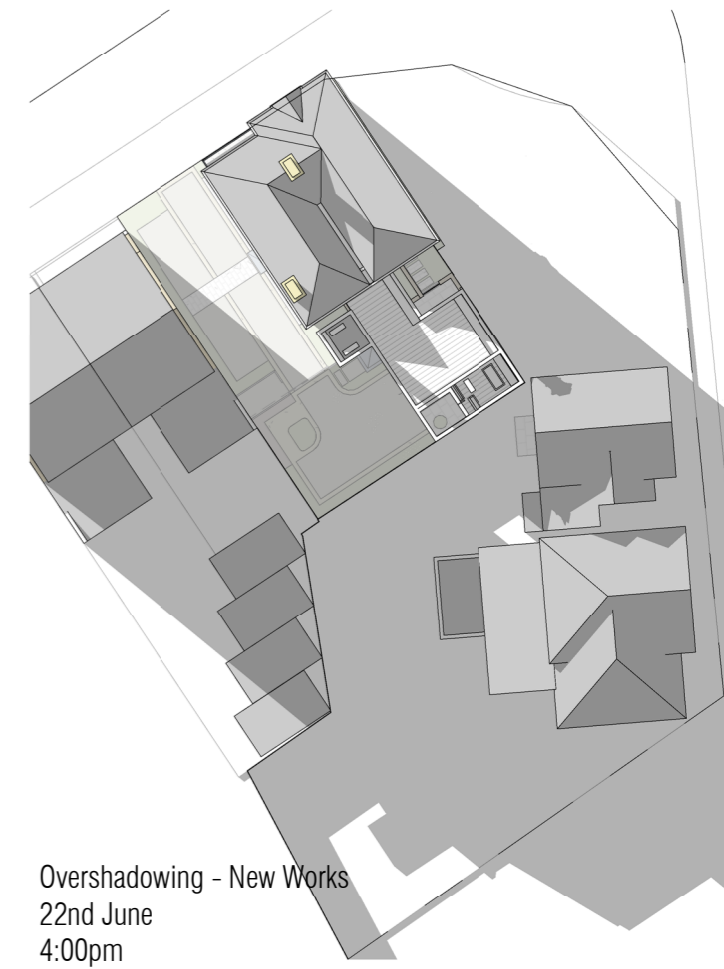
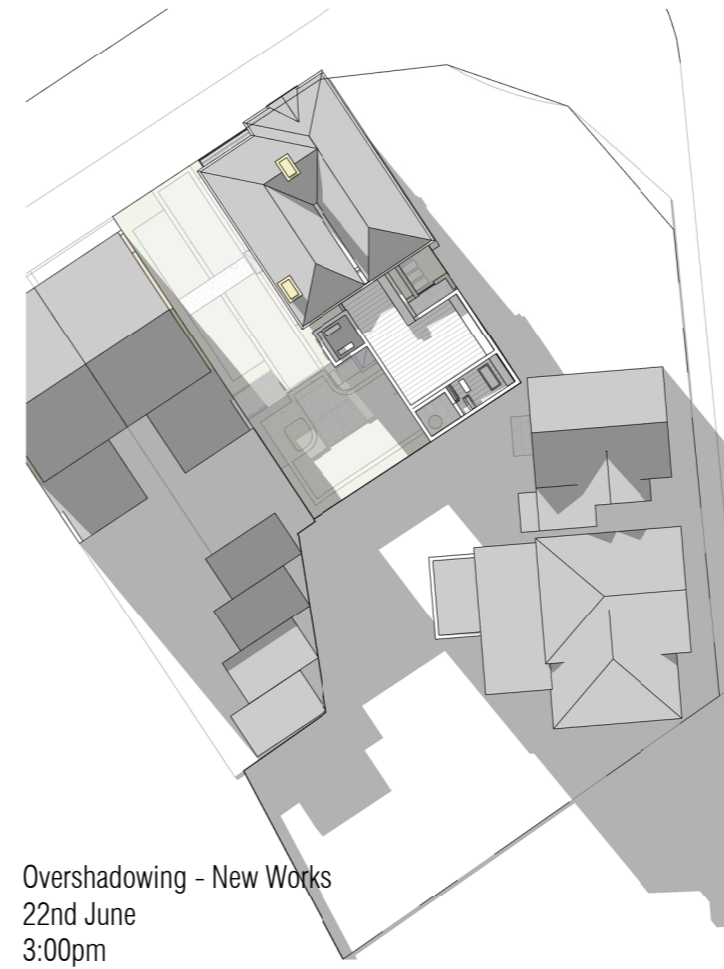
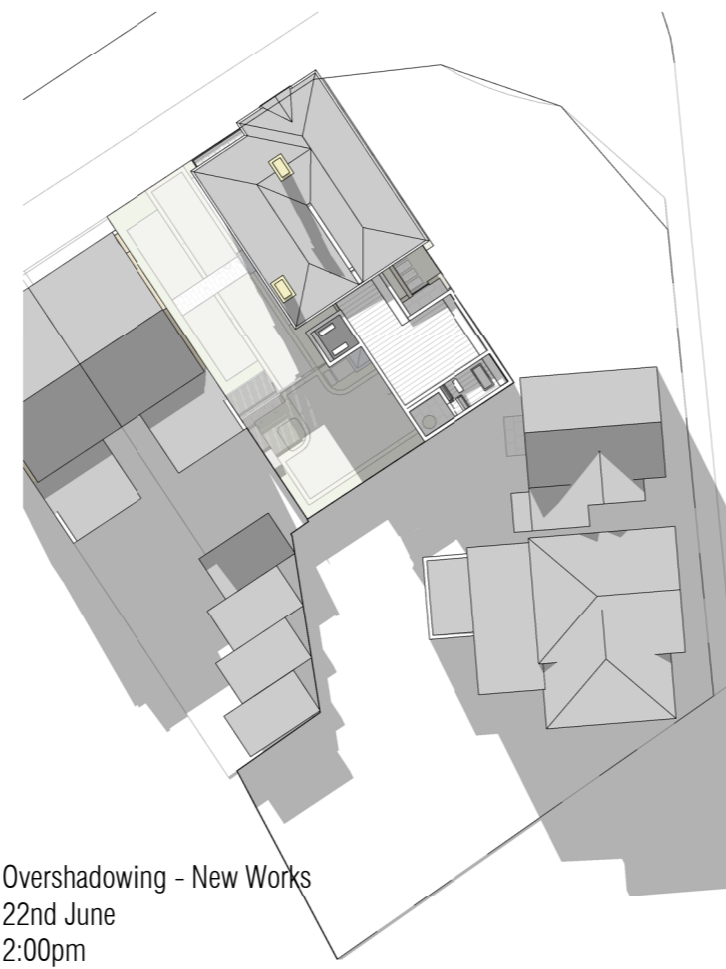
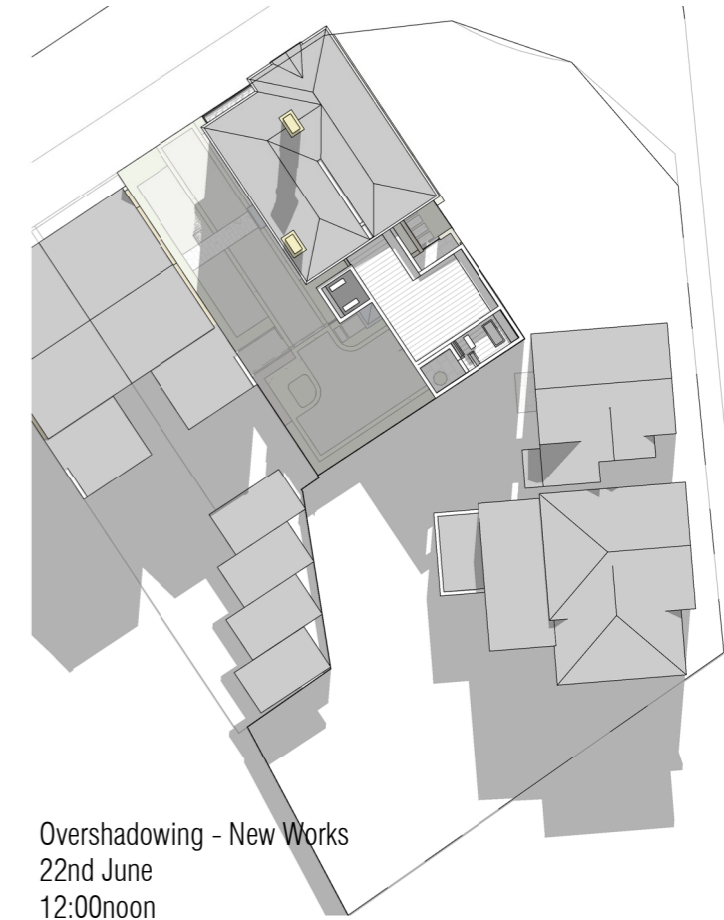
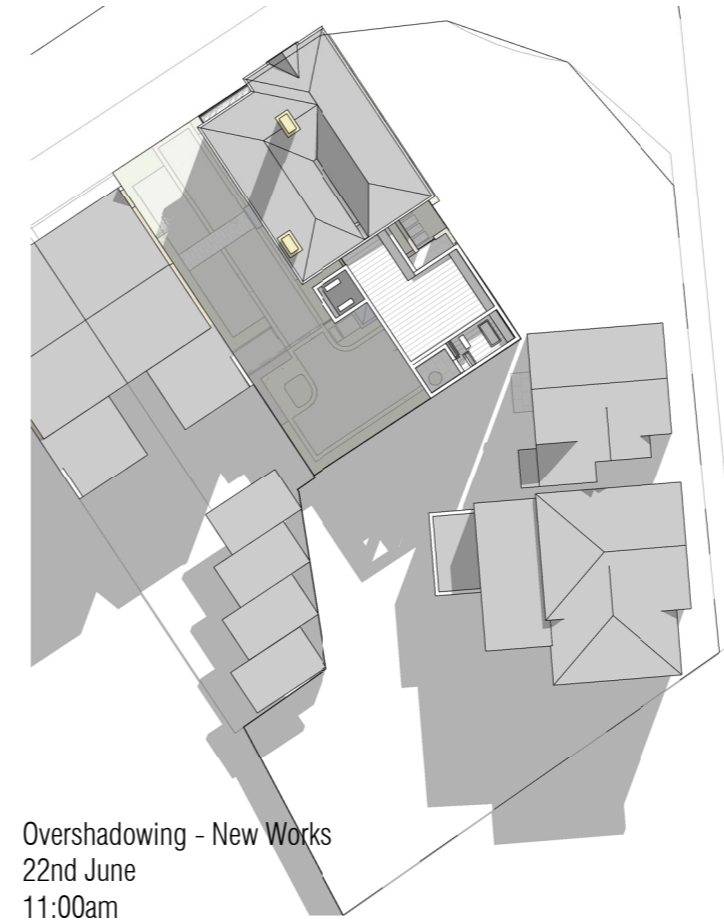
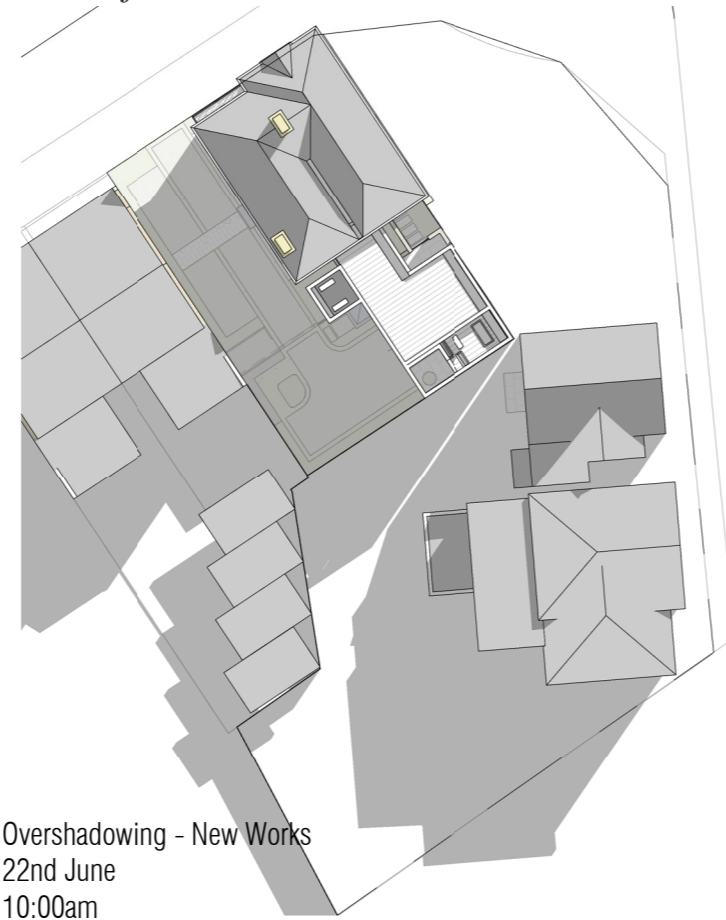


Overshadowing - Existing Conditions
22nd June
3:00pm



Overshadowing - Existing Conditions
22nd June
4:00pm

Overshadowing Diagrams - New Works - 22nd June





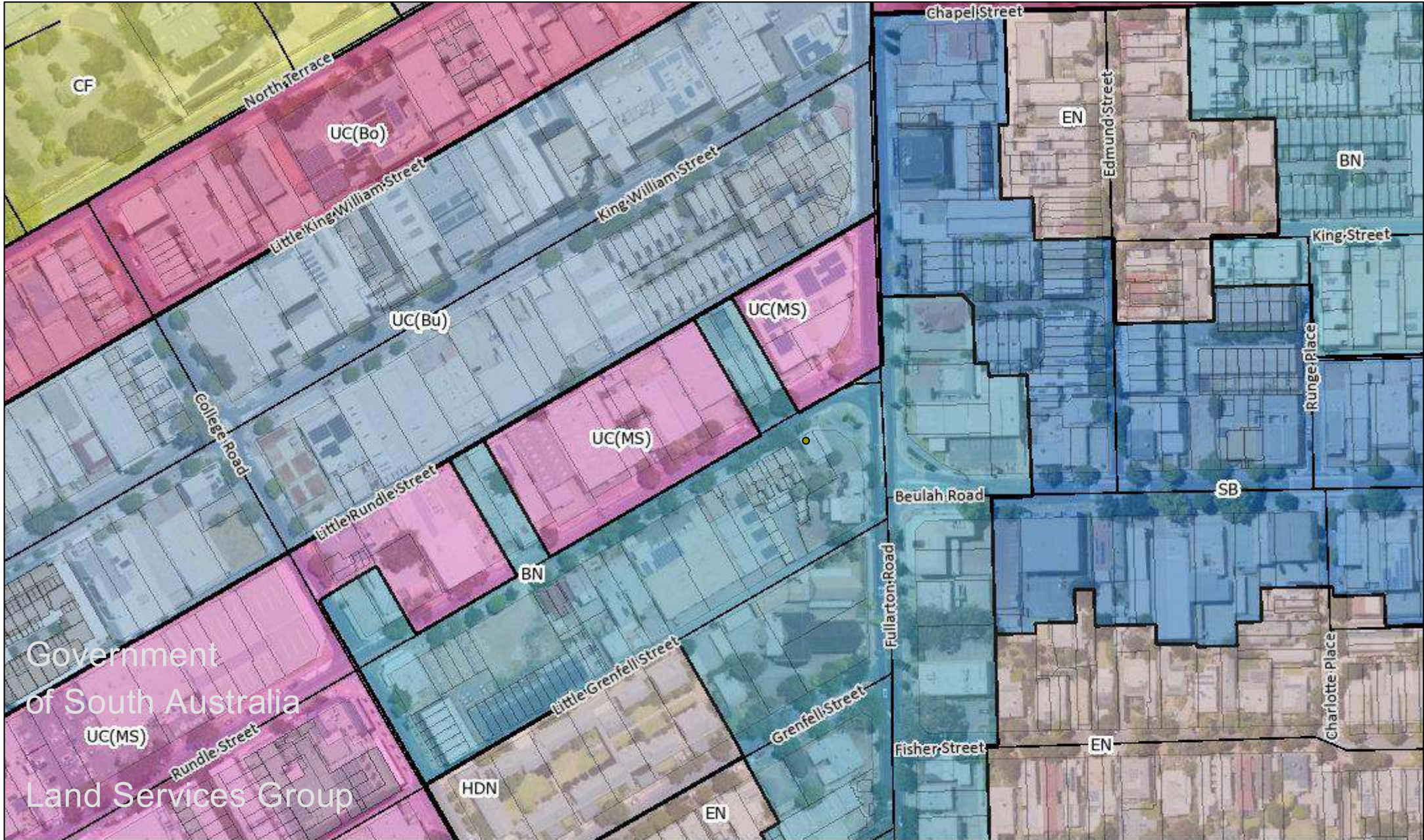
Contact Details

175 The Parade, Norwood
South Australia 5007
P: 08 8366 4555 F: 08 8332 6338
E: townhall@npsp.sa.gov.au

Disclaimer

This map is a representation of the information current held by The City of Norwood, Payneham & St Peters. While every effort has been made to ensure the accuracy of the product, Council accepts no responsibility for any errors or omissions. Any feedback on omissions or errors would be appreciated. Data Acknowledgement: Property, Road & Administrator Boundaries - Supplied by Department Environment & Heritage (DEH)

Attachment 3 - Zoning Map



Government
of South Australia
Land Services Group



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Contact Details

175 The Parade, Norwood
South Australia 5067
P: 08 8366 4555 F: 08 8332 6338
E: townhall@npsp.sa.gov.au

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Application Summary

Application ID	23014875
Proposal	Two-level alterations and additions to an existing Local Heritage Place (comprising the expansion of existing dwelling and proposed consulting rooms)
Location	120 RUNDLE ST KENT TOWN SA 5067

Representations

Representor 1 - George Samaras

Name	George Samaras
Address	PO Box 7434 HALIFAX STREET SA, 5000 Australia
Submission Date	01/08/2023 03:35 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	Yes
My position is	I oppose the development
Reasons refer to attached letter	

Attached Documents

Representation-120-Rundle-St,-Kent-Town-August-2023-1255915.pdf



1 August 2023

PO Box 7434

Halifax Street SA 5000

0402 832 226

andrew@humbyconsulting.com.au

humbyconsulting.com.au

Norwood, Payneham and St Peters Council
PO Box 204
KENT TOWN SA 5071

Via: PlanSA Portal

DEVELOPMENT APPLICATION 23014875

120 Rundle Street, Kent Town

Two – Level Alterations and Additions to an existing Local Heritage Place (comprising the expansion of existing dwelling and proposed consulting rooms)

LETTER OF REPRESENTATION

We acknowledge receipt of your letter regarding the Development Application for the property at 120 Rundle Street, Kent Town, described by the Norwood, Payneham and St Peters Council as '*Two – Level Alterations and Additions to an existing Local Heritage Place (comprising the expansion of existing dwelling and proposed consulting rooms)*'.

Humby Consulting represents the interests of 31 Fullarton Pty Ltd ATF Fullarton Road Unit Trust, the adjoining landowner at 31 Fullarton Road, Kent Town. We hereby present the following representation on behalf of 31 Fullarton Pty Ltd ATF Fullarton Road Unit Trust.

At present, the proposal is deemed to be sited, designed, and scaled in a manner that fails to preserve the character and amenity of the locality. In light of this, we earnestly request that the Council Assessment Panel carefully considers not supporting the proposal in its current form.

Building Setbacks

We acknowledge that the subject land falls within the Business Neighbourhood Zone, as indicated in the Planning and Design Code. The Zone provisions allow for the consideration of various housing and accommodation types, along with compatible employment-generating land uses. However, our evaluation leads us to believe that the current proposal fails to meet several criteria within the Business Neighbourhood Zone and, therefore, should not be granted Planning Consent in its present form.

The Business Neighbourhood Zone prescribes a suite of policies that assist to provide guidance on the suitable setbacks from side and rear boundaries in order to provide:

- Separation between buildings in a way that complements the established character of the locality
- Access to natural light and ventilation for neighbours



Performance Outcomes 3.6 and 3.7 of the Business Neighbourhood Zone state:

<p>PO 3.6</p> <p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between buildings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours. 	<p>DTS/DPF 3.6</p> <p>Other than walls located on a side boundary, building walls are set back at least 900mm from side boundaries.</p>
<p>PO 3.7</p> <p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between buildings in a way that complements the established character of the locality (b) access to natural light and ventilation for neighbours (c) open space recreational opportunities (d) space for landscaping and vegetation. 	<p>DTS/DPF 3.7</p> <p>Building walls are set back from rear boundaries at least:</p> <ul style="list-style-type: none"> (a) 3m for the first building level (b) 5m for any second building level.

The proposed development includes a sizable 'dwelling' constructed at the rear of the subject land, featuring the following parameters:

- 8.765m side boundary wall with no setback
- 7.555m side boundary wall with no setback
- 7.105m rear boundary wall with no setback

The proposed dwellings 7.2m high (approx.) boundary walls on both the side and rear boundaries appear to deviate significantly from with the intent of both Performance Outcome 3.6 and 3.7 of the Business Neighbourhood Zone.

The positioning of the proposed development leads to minimal separation between buildings, resulting in a significant reduction in access to natural light and ventilation for the occupants of 31 Fullarton Road.

Regrettably, the submitted documentation lacks shadow diagrams, making it challenging to assess the extent of overshadowing on the southern allotment. Nonetheless, considering the orientation of the allotments, any built form placed on the rear boundary is likely to cause excessive overshadowing, particularly during the winter solstice.

It is essential to note that the area immediately adjacent to the boundary, where the rear boundary wall is proposed, serves as an outdoor courtyard for the occupants of the adjoining commercial operations. This space, complete with a small garden and outdoor table/chairs, enjoys direct access to sunlight. Furthermore, the west-facing windows on the lower level of the building will also be impacted by the proposed development, leading to a loss of direct sunlight into the internal areas.



In light of the combination of the proposed dwelling's wall height and siting on the side and rear boundaries, a detrimental visual impact and a significant reduction of natural light are expected. Consequently, the siting and design of the proposed development are deemed inappropriate and, to some degree, an overdevelopment of the site.

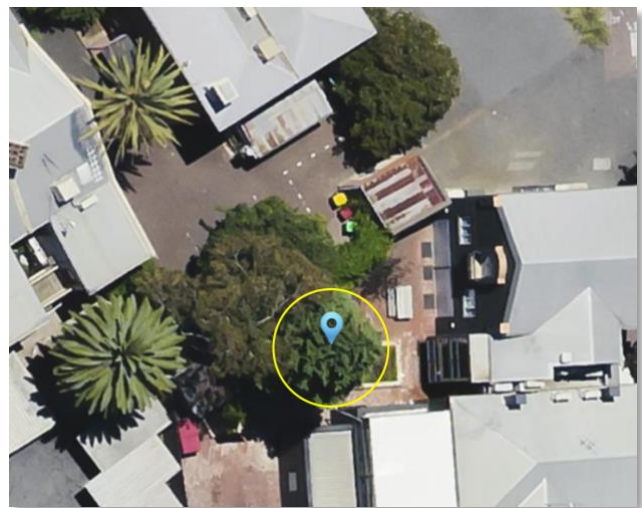
Addressing these concerns and reassessing the design is imperative to align the proposal with the intent of the Business Neighbourhood Zone and to minimise the impacts upon the adjoining property.

Impacts upon Regulated Tree

Unfortunately, the submitted documentation lacks acknowledgment of the presence of a Regulated Tree situated in the adjacent allotment to the south, which belongs to our client.

This sizeable tree, measuring 2.1 meters in circumference, satisfies the criteria for being classified as a Regulated Tree. Notably, it is positioned a mere 1.2 meters away from the boundary of the subject land.

Our clients have raised legitimate concerns regarding the proposed development, specifically the location of the proposed dwelling and installation of the driveway paving, which is planned within the confines of the 'Tree Protection Zone' of the aforementioned Regulated Tree. This circumstance poses a tangible risk of adversely affecting the health and long-term viability of the tree.



To address these concerns and mitigate potential impacts on the existing tree, it is recommended to increase the setbacks of the proposed structure from the rear boundary.

Moreover, it is imperative to furnish supplementary supporting documentation from a qualified arborist or horticulturalist. This documentation should provide expert insights and assurances to alleviate the aforementioned concerns.

Carparking and Access

The submitted plans lack sufficient detail about the existing operations, leaving uncertainty about whether the proposed development includes additional consulting rooms or realigns the internal areas on the ground floor consulting rooms.

In the event that the proposed development aims to introduce additional consulting rooms, our client expresses concerns regarding the following matters:

- Inadequate on-site car parking provision on the subject land

This results in an inconsistency with Performance Outcome 5.1 of the Transport, Access and Parking General Development Policies



- The layout of the proposed on-site car parking does not comply with the requirements stipulated in the Planning and Design Code, particularly in regards to:

- *Vehicular access, where an existing stobie pole poses access challenges into the site*

This results in an inconsistency with Performance Outcome 3.5 of the Transport, Access and Parking General Development Policies

- The inability of vehicles to exit the site in a forward direction

This results in an inconsistency with Performance Outcome 3.8 of the Transport, Access and Parking General Development Policies

- Insufficient on-street car parking available in the immediate vicinity, raising the likelihood that future clientele may utilise the neighbouring car parking area designated for clients of 31 Fullarton Road, Kent Town.

This results in an inconsistency with Performance Outcome 5.1 of the Transport, Access and Parking General Development Policies

There are also concerns regarding the proposed 'visitor' car parks designated for the occupants of the dwelling. While the intention is to offer additional parking spaces, it is evident that the driveway area lacks sufficient dimensions to accommodate the turning areas required for a standard vehicle.

Addressing these concerns is of importance to ensure the proposed development aligns with the minimum car parking requirements of the Planning and Design Code. Additionally, it is essential to ensure adequate manoeuvrability is provided, thereby minimising any potential adverse impact on the surrounding area, including issues related to parking availability and traffic flow.

Further documentation and modifications may be required to resolve these issues effectively.

Summary

The submitted proposal raises several significant concerns, primarily related to the siting, design, and scale of the proposed dwelling. These aspects seem to deviate significantly from the intent outlined in Performance Outcome 3.6 and 3.7 of the Business Neighbourhood Zone. Notably, the lack of adequate setbacks from the side and rear boundaries results in minimal separation between buildings, leading to a reduction in access to natural light and ventilation for the occupants of 31 Fullarton Road.

Furthermore, the presence of a Regulated Tree near the subject land raises additional concerns. The proposed development's location and driveway paving within the 'Tree Protection Zone' risk detrimental impacts on the tree's health and longevity.

Additionally, the inadequate provision of on-site car parking, non-compliance with the Planning and Design Code requirements for vehicular access, and insufficient on-street parking availability further compound the objections.



To address these concerns, it is essential to reassess the design, increase setbacks, and provide additional supporting documentation from qualified experts. Failure to do so may jeopardise the proposed development's alignment with the Planning and Design Code and would have adverse effects on the adjoining occupants.

We seek the opportunity to review any further information provided by the applicant, and depending on the nature of the information, this representation may be withdrawn.

We welcome the opportunity to discuss our concerns with the Council's representative and request to be heard before the Council Assessment Panel.

For any further communication, please feel free to contact us at 0402 832 226.

Kind regards

A handwritten signature in blue ink, appearing to read 'Andrew Humby', with a long horizontal stroke extending to the right.

Andrew Humby
Director
Humby Consulting
andrew@humbyconsulting.com.au

Kent Town 2332 001A

5 September 2023

Mr Nenad Milasinovic
City of Norwood Payneham & St Peters
Via the Plan SA Portal



Town Planning
Development Advice
Strategic Management

Dear Nenad,

APPLICATION ID: 23014875 – RESPONSE TO REPRESENTATIONS

I refer to the abovementioned development application by Dr Andrew and Mrs Alana Campbell-Lloyd that seeks planning consent to construct a two level addition to the rear of an existing building used as a residence and consulting rooms (a Local Heritage Place) located at 120 Rundle Street, Kent Town.

I have been engaged by Andrew and Alana, to review and respond to the representations received as a result of public notification procedures, noting that by Dr David Baker of 123 Rundle Street was received out of time. I shall reply to the matters raised by Dr Baker in any event.

The representation made by Humby Consulting on behalf of George Samaras, presumably a director of the company that holds 31 Fullarton Road, the adjoining property to the south and east, the building on which is used as offices and studio associated with a creative agency trading under the name Kojo.

In providing my response to the matters raised, I have had regard to the existing condition of the land and the nature of development on surrounding land, the function and role of Rundle Street passing through Kent Town, and the relevant provisions of the Planning & Design Code. In this regard, I note:

- the building at 120 Rundle Street is currently used as consulting rooms and a residence having been the subject of an approval granted in October 2022;
- prior to this 120 Rundle Street was used wholly as consulting rooms for up to 5 practitioners and associated support staff;
- adjoining properties to the west and south east are non-residential in nature used for consulting rooms or offices with associated activities including car parking;
- access to 120 Rundle Street is from Rundle Street on which kerb side parking is permissible other than between 7 AM and 9 AM given its use as a bike lane;
- Rundle Street as it passes through Kent Town serves a sub arterial road function between Fullarton Road and Dequetteville Terrace;
- 120 Rundle Street is located within the Business Neighbourhood Zone which provides for a range of land uses including consulting room and dwelling;

Phillip Brunning & Associates

ABN 40 118 903 021

Level 1, 27 Halifax Street
Adelaide SA 5000
Mobile 0407 019 748
phil@phillipbrunning.com
www.phillipbrunning.com

- consulting rooms are specifically provided for within existing buildings, with an upper limit of 250 m² in floor area;
- policy guidance is provided in relation to site coverage (60%), building height (2 levels) and set backs (discussed below); and
- car parking is sought by the code at a rate of 4 spaces per consulting room and 2 spaces per dwelling.

With this in mind, I provide the following for your consideration.

Land Use

As noted above, there has been a previous consent issued for use of this premises as consulting room and dwelling. As outlined in that submission, Dr Andrew Cambell Lloyd is a plastic surgeon specialising in breast reconstruction.

Andrew is assisted by one staff member with consulting hours being between 9 AM and 4 PM Monday to Friday by appointment only. Appointments are typically no less than 30 minutes in duration.

Andrew and Alana currently reside within the premises, with the proposed works providing enhanced accommodation for their family. The consulting room use will continue to be solely for Andrew's practice.

The arrangement of the premises at ground floor provides rooms for different functions associated with Andrew's practice. At the upper level, 3 bedrooms are provided with the usual living and utility spaces.

This is a low intensity use of land that would have minimal, if any appreciable impact on the character and amenity of the surrounding locality. The synergy of the two land uses also need to be considered and the benefits arising from such.

Siting & Built Form

I am sure you would agree that this proposal exhibits a high quality of design, that has been thoughtfully composed and expressed. The resultant composition would in my view make a positive contribution to the built form fabric of Kent Town.

Some issue has been taken with respect to the adequacy of building set backs to side and rear property boundaries. It is also suggested that the proposal will result in excessive overshadowing of the adjoining property to the south.

As you will be no doubt aware, Designated Performance Features (such as that in relation to building set backs) are provided by the Code as one way of satisfying the overarching Performance Outcome and in this sense are not mandatory in nature.

The relevant Performance Outcomes make it clear that appropriate reference and weight needs to be given to the *established character of the locality* when applying these tests in relation to access to natural light and ventilation.

On any objective review it is apparent that this locality is characterised by robust two level buildings dating back to the late 19th and early 20th century, with walls on property boundaries and high site utilisation.

This locality is not characterised by buildings set within landscaped grounds as may the case in other locations within Kent Town, with the space around buildings more often than not used for car parking and garaging of vehicles.

In so far as the proposal does not provide the building set backs sought by DPFs 3.6 and 3.7, I am of the view that this will not result in an unacceptable built form outcome that would significantly prejudice the enjoyment of adjoining properties.

As shown on the diagrams provided, shadow that would be cast by the proposed additions falls predominantly over the car parking area to the rear of the adjoining property in a manner and to an extent not dissimilar to that which typical of this locality.

The adjoining property to the south is not residential in nature. Can I respectfully suggest that the owner of this property may not reasonably expect unfettered sunlight to a commercial premises in an inner urban context such as this.

Heritage & Character

Appropriate regard and respect have given by the Architect to the heritage value of the existing building and the context in which these additions are proposed, both with respect to form and detailing of facades.

The additions are proposed to the rear of this local heritage place such that would not alter its street presence and contribution that it makes to the character of this locality. the addition is subservient to the existing building form which will remain dominant.

Appropriate junctions between the two preserve the integrity of the original building, with the new work clearly discernible yet respectful. Proportions and alignments are complementary, with a sympathetic palette of materials and finishes.

I expect that Council's heritage advisor will take a similar view of this proposal.

You will note that there has been some revision of the detailing of the work proposed to the front facade of this heritage building, more particularly in relation to the balcony window and the balustrades to the balcony.

The existing balcony window is being replaced with door panels now configured to allow all existing quoin & post details to be retained. The window railing & panel breakup is being maintained on the new door panels.

The existing balcony balustrade does not comply with current safety codes, but the existing wrought iron panels are being salvaged to be installed within the new raised balustrade, allowing the original detailing to be maintained.

Regulated Tree

Arboricultural advice has been taken by the Applicant in relation to trees on adjoining land. A copy of the expert opinion of Mark Elliott of The Adelaide Tree Surgery is provided as an attachment to this letter for your consideration.

Mark confirms that that this tree (a Conifer) is a Regulated Tree having a stem circumference of between 2 and 3 metres when measured 1 metre above ground level, with an overall height of approximately 17 metres.

A Structural Root Zone (SRZ) of 2.85 metres has been determined with a Tree Protection Zone (TPZ) of 7.2 metres. Mark advises notes that no major excavation works are proposed within the TPZ, with the proposed building beyond this extent.

While Mark confirms that this tree will not be adversely effected by the proposed development, he does recommend the use of permeable paving to allow air and water to the tree's root system. The proposal plans have been amended accordingly.

Parking & Access

Some issue is taken by the representors with respect to access arrangements and parking provision. In light of this commentary, the Architect has reviewed and amended the proposed arrangement in the following manner:

- repositioning of the driveway location relative to the power pole to the front of the land within the verge area so as to achieve suitable clearance from this obstruction and the street tree further to the west;
- repositioning of the two parallel parking spaces to be used by patients attending these premises during business hours, together with the provision of a screened bin enclosure – gates to be kept open during business hours; and
- provision of a turn around area in front of the owners garage that would provide for a 3 point turn manoeuvre allowing vehicles to exit the land and enter Rundle Street in a forward direction - gates to be kept open during business hours.

I am of the view that these amendments suitably remedy the concerns raised by representors with respect to the movement of vehicles to and from the land, minimising the potential for conflict with passing motorists and cyclists.

This arrangement is far preferable and safer than many of the other examples further along Rundle Street where vehicles either have to reverse into parking spaces on arrival or out onto the carriageway when leaving.

In terms of the adequacy of on-site parking, I again point out that the proprietors of the consulting room is also residents such that reduces the demand from that which may otherwise be the case if there were two sperate user groups.

This is a low intensity consulting room use with long duration consultations (typically 30 minutes or more) such that the frequency of movement and potential for overlap between appointments is limited.

Whereas specific parking rates are identified in Table 1, the overarching performance consideration is that sufficient on site vehicle parking is provided to meet the needs of the development, and that a lesser provision may be acceptable having regard to:

- *availability of on-street car parking;*
- *shared use of other parking areas;*

- *in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared; and*
- *the adaptive reuse of a State or Local Heritage Place.*

I say that these considerations are applicable and will be satisfied by the proposed development such that the planning authority may exercise its discretion to accept a slightly lesser provision from the standard rates identified in Table 1.

The planning authority may have confidence that adequate parking will be provided.

In conclusion, I am of the view that the proposal is in suitable conformity with the relevant provisions of the Planning & Design Code. to the extent that it may depart from certain provision, not serious planning impacts are anticipated.

This is a highly desirable adaptive reuse of this heritage building that will ensure its longevity and relevance as a valued place of work and residence by its owners. The Applicant looks forward to your support of this proposal.

As one of the representors has requested to be heard by Council's Assessment Panel, I will attend the scheduled meeting to respond to any matters arising including questions from Panel members. Thank you.

Yours faithfully

PHILLIP BRUNNING & ASSOCIATES PTY LTD



PHILLIP BRUNNING RPIA

Registered Planner

Accredited Professional – Planning Level 1



THE Adelaide
TREE SURGERY
A division of **tree aware**

3 Ellemsea Circuit
LONSDALE SA 5160
ABN: 33 099 478 994

Phone: 8371 5955 Mobile: 0408 086 774 Fax: 8297 6885 Email: mark@adelaidetreesurgery.com

Pre - Arboricultural Impact Assessment and Report

Prepared for
Williams Burton Leopardi
David Handsaker
Level 3, Darling Building
28 Franklin Street
Adelaide SA 5000

Site Address:
120 Rundle Street
Kent Town SA

In Regard to
1 x "Regulated" *Cupressus sp* (Conifer)



Prepared By:
Mark Elliott Consulting Arborist/Diploma Arboriculture

Arborist Report for: Williams Burton Leopardi (Mr David Handsaker) SITE ADDRESS: 120 Rundle Street, Kent Town SA 5067
©The Adelaide Tree Surgery Pty Ltd – A Division of Tree Aware
Ref: TATSME002355 4/09/2023.



3 Ellemsea Circuit
LONSDALE SA 5160
ABN: 33 099 478 994

Phone: 8371 5955 **Mobile:** 0408 086 774 **Fax:** 8297 6885 **Email:** mark@adelaidetreesurgery.com

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1.0 INTRODUCTION

- 1.1 This report has been prepared at the request of **Mr David Handsaker** from **Williams Burton Leopardi** is in relation to one ***Cupressus sp* (Conifer)** which is located at **31 Fullarton Road, Kent Town SA 5067** which is the rear neighboring property to 120 Rundle Street, Kent Town SA 5067 where it is proposed to undertake the development.
- 1.2 The subject tree is classified as “**Regulated Tree**” as per the ***Planning Development. and Infrastructure March 2016.***
- 1.3 It is proposed to construct a new addition to the existing building, garage and paving area at **120 Rundle Street, Kent Town SA 5067 (refer Figure 2).**

2.0 TERMS OF REFERENCE

- 2.1 Verbal instructions were received in **August 2023.**
- 2.2 The instructions received for an arborist report for a “**Regulated**” ***Cupressus sp* (Conifer)** which is located at **31 Fullarton Road, Kent Town SA 5067** and the proposed development at **120 Rundle Street, Kent Town SA 5067.**
- 2.3 The report also provides comment on the subject tree to ensure the tree is not compromised by the proposed development at **120 Rundle Street, Kent Town SA 5067.**

3.0 CAVEAT EMPTOR

- 3.1 This is a stage 1 '**Ground Report**'. The tree was inspected from the ground only.
- 3.2 The report is limited by the time of the inspection.
- 3.3 The report reflects the tree as found on the day of inspection. Any changes to site conditions or surroundings, such as construction works, landscape works or further failures or pruning, may alter the findings of the report.
- 3.4 The inspection period to which this report applies is three months from the date of the report.

4.0 THE SITE

- 4.1 The subject tree is growing in the rear car parking area of the property at **31 Fullarton Road, Kent Town SA 5067** which is located on the southern side of the property at **120 Rundle Street, Kent Town SA 5067** where the proposed development is to be undertaken.
- 4.2 **Kent Town** is located with the council boundaries of the **City of Norwood, Payneham & St Peters** which is located approximately 1 – 6 km east from the **Central Business District (CBD).**

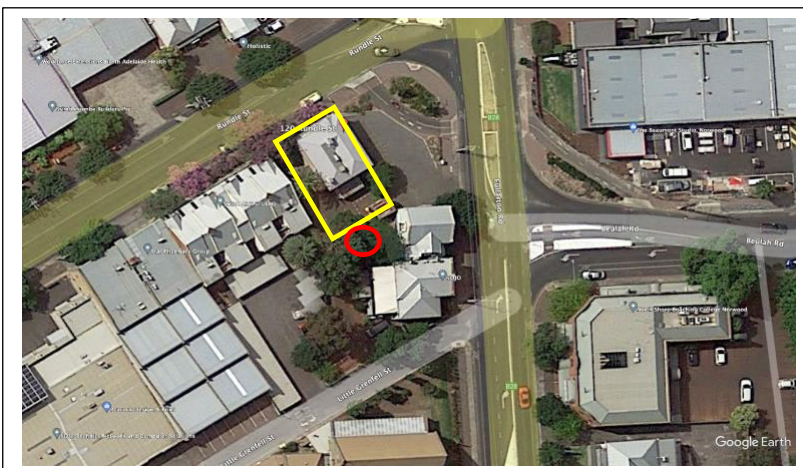


Figure 1 shows an aerial image of the property at 120 Rundle Street, Kent Town and the subject tree located in the neighbouring property is highlighted in red.

5.0 PROPOSED DEVELOPMENT

- 5.1 It is proposed **construct** a new addition to the existing building which includes a garage and paved area for driveway/car parking at **120 Rundle Street, Kent Town SA 5067** (refer Figure 2).
- 5.2 There is no major encroachment in to the **Tree Protection Zone (TPZ)** of the subject tree other than the paved driveway/car parking area. This paved area does have some minor encroachment into the trees **Structural Root Zone (SRZ)**.

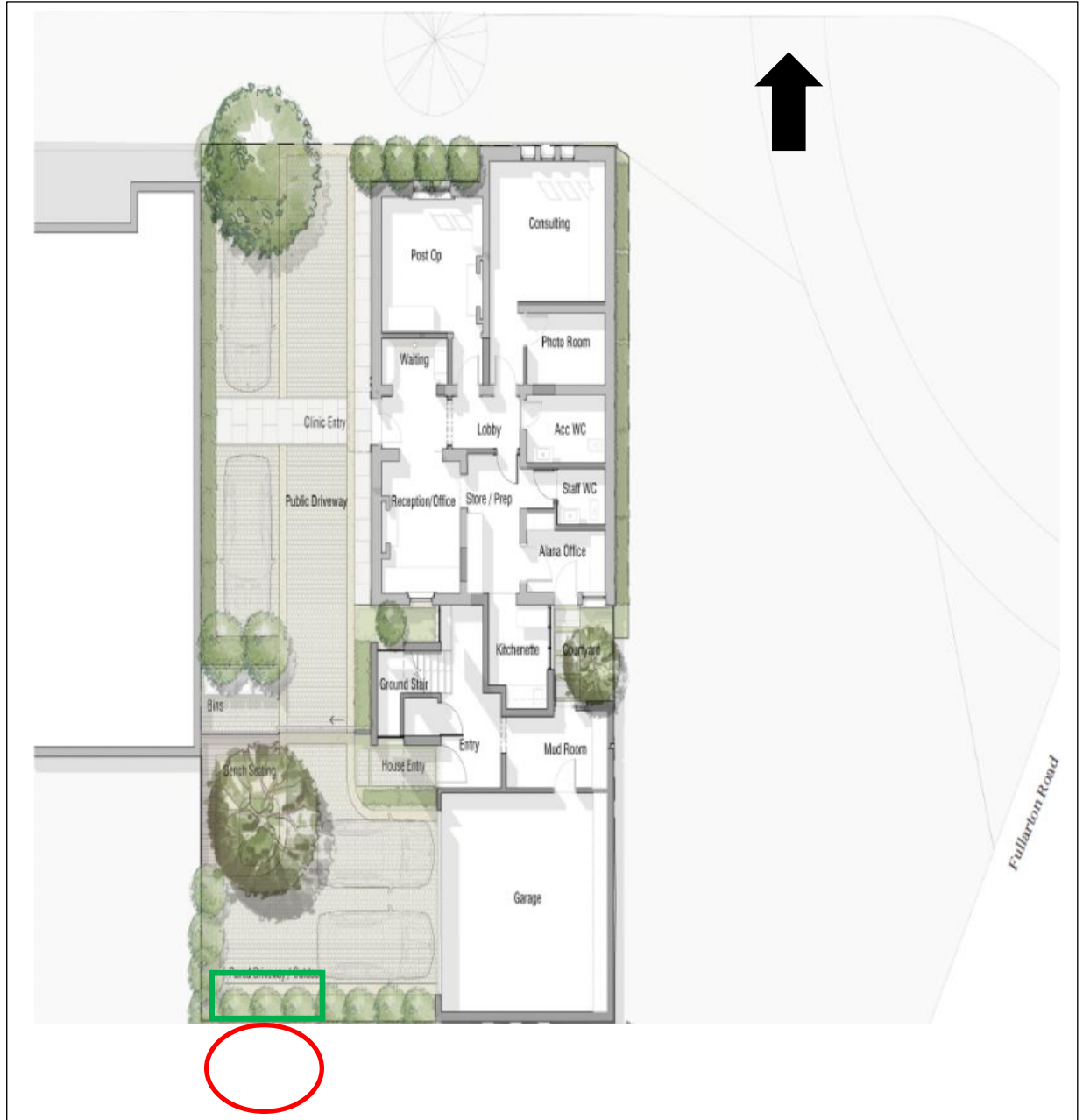


Figure 2 shows the plans of the proposed development at 120 Rundle Street, Kent Town SA 5067 and the subject tree is located in the rear neighbouring property at 31 Fullarton Road, Kent Town SA 5067.

6.0 THE TREE - One "Regulated" *Cupressus sp* (Conifer)

6.1 The circumference of the subject tree when measured at one metre above natural ground level of greater than 2 meters and less than 3 metres, therefore the tree can be classified as a "Regulated Tree" as per the *Development, Transport, and Infrastructure March 2016*.

6.2 Tree Information:

Tree Name	Significant or Regulated	Tree Condition
<i>Cupressus sp</i> (Conifer)	<p>Regulated Tree</p> <p>Stem circumference between 2 meters and 3 meters when measured at one meter above natural ground level</p>	<p>This Conifer is a large tree growing in the rear car parking area of the neighbouring property at 31 Fullarton Road, Kent Town SA 5067.</p> <p>The subject tree is located approximately one meter from the dividing boundary line.</p> <p>The tree is approximately 17 metres in height.</p> <p>The crown dimensions are:</p> <p>4 meters to the west 4 meters to the south 3.5 meters to the east 4 meters to the north.</p> <p>At the time of the inspection, the subject tree was showing good health and condition and there were no signs of any pests of diseases.</p> <p>Following are the calculated Tree Protection Zone (TPZ) and Structural Root Zone (SRZ) for the subject tree.</p> <ul style="list-style-type: none"> - Tree Protection Zone (TPZ) = 7.20 meters - Structural Root Zone (SRZ) = 2.85 meters <p>PLEASE NOTE: The above measurements are estimates only.</p>



Figure 3 show the subject Conifer Tree growing in the property at 31 Fullarton Road, Kent Town SA 5067 which is located on the southern side of 120 Rundle Street, Kent Town SA 5067.

7.0 POTENTIAL IMPACTS

7.1 The aim of this report is to provide guidelines for best practise tree protection measures in accord with **Australian Standard AS4970-2009 'Protection of Trees on Development Sites'**

7.2 The preamble of the standard provides a brief outline of why it is important to retain and protect trees on development sites and the following Section: The Tree Protection Zone details the zones around a tree that are required to protect it.

7.3 AUSTRALIAN STANDARD: AS4970-2009 'Protection of Trees on Development Sites'

7.3.1 *A living tree is a dynamic organism that needs specific environmental conditions to continue healthy, stable growth. It is rarely possible to repair stressed and injured trees, so substantial injury needs to be avoided during all stages of development and construction.*

7.3.2 *For trees to be retained and their requirements met, procedures must be in place to protect trees at every stage of the development process. This should be considered at the earliest planning stage of any outdoor event or design of a development project where trees are involved.*

7.3.3 *Trees and their root systems may occupy a substantial part of any development site and because of their potential size, can have a major influence on planning the use of the site.*

7.3.4 *Existing trees of appropriate species and sound structure can significantly enhance new development by providing immediate benefits such as shade and stormwater reduction as well as complementing new development.*

7.3.5 *Most trees will take many years and possibly decades to establish but can be injured or killed in a very short time, as their vulnerability is commonly not understood. This is especially so in relation to tree root systems which cannot be seen. Irreparable injury frequently occurs in the early stages of site preparation and remedial measures routinely fail.*

7.3.6 *Early identification and protection of important trees on development sites is essential from the outset and will minimise the problems of retaining inappropriate trees.*

7.3.7 *Successful long-term retention of trees on development sites depends on an acceptance and acknowledgement of the constraints and benefits that existing trees generate. Protecting trees in accordance with the Standard may influence design and construction costs and this should be considered in project budgets and contracts. The gains and benefits of retaining trees would accrue if the measures detailed in the Standard are applied.*

7.4 THE TREE PROTECTION ZONE

7.4.1 The Tree Protection Zone (TPZ) is the principal means of protecting trees on development sites. The TPZ is a combination of the root area and crown area requiring protection. It is an area isolated from construction disturbance, so that the tree remains viable. The TPZ incorporates the structural root zone (SRZ).

7.4.2 It may be possible to encroach into or make variations to the standard or optimal TPZ. Encroachment includes excavation, compacted fill and machine trenching.

7.4.3 If the proposed encroachment is greater than 10% of the TPZ or inside the SRZ, the project arborist must demonstrate that the tree(s) would remain viable.

7.5 SRZ & TPZ CALCULATIONS

7.5.1 **The SRZ (or CRZ): Structural/Critical Root Zone** is the zone around a tree required to protect the tree's stability. **Generally, no development activities are permitted within this zone unless there are no other suitable options.**

- 7.5.2 The TPZ or Optimal Tree Protection Zone is the principal means of protecting the tree and is calculated using the formula $TPZ = DBH$ (diameter @ 1.4 meters above ground level) X 12.

7.6 POTENTIAL IMPACTS

- 7.6.1 I believe there will be minimal impact of the Conifer for the proposed development at 120 Rundle Street, Kent Town as there is no major excavation works within the Tree Protection Zone (TPZ) of the subject tree. If the recommendations within Section 10 are adhered to the subject tree will not be compromised.

8.0 DISCUSSIONS

- 8.1 This report has been prepared to ensure the **Conifer** tree growing in the neighboring property at **31 Fullarton Road, Kent Town** is not adversely affected by the proposed works at **120 Rundle Street, Kent Town**.
- 8.2 I believe the tree will not be compromised by the proposed development as there is no major encroachment or excavation works required to be undertaken within the **Tree Protection Zone (TPZ)** of the subject tree.
- 8.3 The encroachment into the **Tree Protection Zone (TPZ)** is from the paved driveway and car parking area which is the same/similar as its current use.
- 8.4 The proposed garage will be located outside of the **Tree Protection Zone (TPZ)** of the tree.
- 8.5 Refer to the recommendation within section ten to assure the subject tree is not impacted by the proposed development.

9.0 LEGISLATE REQUIREMENTS

- 9.1 The subject tree is classified as a “**Regulated**” as per the *Planning, Development and Act March 2016*.

PO 2.1

Regulated and significant trees, including their root systems, are not unduly compromised by excavation and / or filling of land, or the sealing of surfaces within the vicinity of the tree to support their retention and health: *Yes – the proposed development at 120 Rundle Street, Kent Town will have minimal impact on the subject tree due to limited excavation works require and filling. The area is proposed to be paved and it is recommended the pavers are a permeable paver to allow air and water to the tree root system.*

10.0 RECOMMENDATIONS

- 10.1 The aim of this report is to provide protection measures for the subject tree which is growing in the neighbouring property at **31 Fullarton Road, Kent Town** during and post the demolition and construction phases of the proposed works at **120 Rundle Street, Kent Town SA 5067**.
- 10.2 **Based on the findings within this report, there will be no impact to the subject tree if the following recommendations are adhered to.**
- 10.2.1 **THERE IS TO BE NO SOIL LEVEL CHANGES BELOW TOPSOIL WITHIN THE TREE PROTECTION ZONE (TPZ) OF THE SUBJECT TREE.**

- 10.2.2** There is to be no mechanical excavation works undertaken within the **Tree Protection Zone (TPZ)** of the **subject tree**. **ALL WORKS WITHIN THE TREE PROTECTION ZONE (TPZ) ARE TO BE UNDERTAKEN USING NON-DESTRUCTIVE METHODS.**
- 10.2.3** Any excavation works that are required for the preparation and the construction phase within the **Tree Protection Zone (TPZ)** of the **subject tree** is to be undertaken the supervision of the **Project Arborist** or by a suitably qualified arborist (**minimum level 4 in Arboriculture**).
- 10.2.4** If any larger tree roots with a diameter greater than **40 mm** are discovered during the construction phase of the proposal, the **Project Arborist** needs to be contacted to inspect and assess and obtain council approval.
- 10.2.5** If any tree roots are exposed, temporary protection measure may be required such as hessian sheeting as multiple layers and this should be secured and also maintained moist until tree roots are covered/remediated.
- 10.3** The area of the **Tree Protection Zone (TPZ)** requires to be fenced during the construction phase. Refer to figure two for location of this fencing. This fencing can be removed when paving of this area needs to be undertaken.
- 10.4** Following are a list of activities that are not permitted within the **Tree Protection Zone (TPZ)** of the subject tree:
- Storage of materials
 - Refuelling
 - Parking of Vehicles/plant
 - Dumping of waste
 - Placement/storage of fill
 - Soil level changes
 - Preparation of concrete products/chemicals
 - Mechanical excavation
 - Washing down of tools/equipment
 - Temporary or permanent installation of utilities and signs
- 10.5** **DEVELOPMENT PHASE: If it is proposed to undertake landscaping works within the area of the TPZ such as paving, I do recommend a permeable paving product to be used. All works required within the area of the TPZ must be undertaken by hand or using non-destructive methods.**
- 10.6** The profile for paving within the **Tree Protection Zone (TPZ)** is to be constructed without being detrimental to the trees health and condition:
- 10.7** When paving with a **Tree Protection Zone (TPZ)** of a tree, a **permeable paver** needs to be used along with a suitable base profile.
- 10.8** Do not allow for changes of the soil to below the top soil when undertaking paving within the **Tree Protection Zone (TPZ)**. Carefully remove/scraped away to the original soil surface (top soil).
- 10.8.1** **Paving Option Number 1**
- 10.8.1.1** Paver 50 – 80mm (80mm paver is required to support vehicle loads) Joints filled with 2 - 5mm clean aggregate. Regular paving sand is not suitable.
- 10.8.1.2** Bedding layer of 30 – 50mm deep bedding layer of washed uniformly graded aggregate between 5 – 7 mm.
- 10.8.1.3** A base course layer 100 – 300mm deep and comprising 20 mm washed uniformly graded aggregate. A geotextile fabric may be required over the subbase, preventing different layers from mixing, blocking pore spaces.
- 10.8.2** **Paving Option Number 2**

- 10.8.2.1 Paver 50 – 80mm (80mm paver is required to support vehicle loads) Joints filled with 2 - 5mm clean aggregate. Regular paving sand is not suitable.
- 10.8.2.2 No bedding layers.
- 10.8.2.3 A base course layer 100 – 300mm deep of SPACE structural soil (structurally permeable aerated compactable earth). These materials can be compacted and will still allow air and water exchange.

10.9 **Fencing:**

- 10.9.1 **Any works required for fencing around the property are to be undertaken using non-destructive methods such as hand/hydro-vac when working within the Structural Root Zone (SRZ) and Tree Protection Zone (TPZ) of the subject tree.**
- 10.9.2 **The style of fencing should be post and rail when located within the Tree Protection Zone (TPZ) of the subject tree.**
- 10.10 **Any services that maybe required to enter and exit the development area should avoid the TPZ and SRZ wherever possible however. If they must pass within the TPZ, non-destructive methods such as Hydro vac® systems must be used and tree roots to remain intact.**
- 10.11 **Approval from the relevant authorities needs to be granted prior to commencement of any works.**

Mark Elliott
Consultant Arborist/Diploma Arboriculture

APPENDIX A: REFERENCES

Planning Development and Infrastructure Act March 2016

Australian Standards - Protection of Trees on Development Sites AS4790 – 2009

Australian Standards – Pruning of Amenity Trees AS4373 – 2007

Mattheck, C and Breloar, J – The Body language of Trees (1994)

Roberts, J. Jackson N and Smith D – Tree Roots in Built Environment - 1994

APPENDIX B: TREE PROTECTION ZONE SIGNAGE



APPENDIX C: DISCLAIMER AND LIMITATIONS

This report only covers identifiable defects present at the time of inspection. The author accepts no responsibility or can be held liable for any structural defect or unforeseen event/situation that may occur after the time of inspection, unless clearly specified timescales are detailed within the report.

The author cannot guarantee trees contained within this report will be structurally sound under all circumstances, and cannot guarantee that the recommendations made will categorically result in the tree being made safe.

Unless specifically mentioned this report will only be concerned with above ground inspections, that will be undertaken visually from ground level. Trees are living organisms and as such cannot be classified as safe under any circumstances. The recommendations are made on the basis of what can be reasonably identified at the time of inspection therefore the author accepts no liability for any recommendations made.

Care has been taken to obtain all information from reliable sources. All data has been verified insofar as possible; however, the author can neither guarantee nor be responsible for the accuracy of information provided by others.

HERITAGE IMPACT REPORT

bbarchitects

PROPERTY ADDRESS: **120 Rundle Street Kent Town**
 APPLICATION NUMBER: **23014875**
 DATE: 8 September 2023
 PROPOSAL: Alterations and additions
 HERITAGE STATUS: LOCAL HERITAGE PLACE
 HERITAGE ADVISOR: David Brown, BB Architects
 PLANNER: Mark Thomson



City of
Norwood
Payneham
& St Peters

ADVICE SOUGHT

No pre Planning Consent advice has been sought from Council's Heritage Advisor by the applicant. The applicant has modified the works to the front of the Local Heritage Place following some feedback.

DESCRIPTION

The building is a two storey late Victorian sandstone building that is listed as a Local Heritage Place in the Business Neighbourhood Zone within the Parade/Fullarton Road Historic Area



PROPOSAL

The proposal is for a two level rear addition to the existing building, along with the changes proposed to the front façade and balcony.

The proposed addition is a simple rectilinear two level contemporary form set at the rear of the existing building. The addition is constructed from bagged brick with the colour in the drawings shown to be similar to the sandstone walls of the existing building.

The proposed changes to the front elevation include the demolition of the western upper level windows, and replacing them with a pair of French doors, and the replacement of the cast iron balcony balustrade with a new balustrade reusing the cast iron elements.

The upper level central rear window is proposed to be cut down to form an opening to the new addition.

There are other internal elements proposed to be removed as well including the original stairs, a fire place, walls and doors. The existing landscaping in the side garden is proposed to be removed as well including the mature palm tree.

COMMENTS

The proposed addition is an elegant simple design with good visual and physical separation from the original building. The addition is set at the rear, borrows from the form, and heights of the existing building, and does not have a major impact on the historic fabric of the Local Heritage Place.

The change from slim sash windows on the front to slim French doors built in a style to match the old windows is understandable to allow access to the balcony. The change over of this element will not have a detrimental impact on the heritage value of the building.

The new balcony reuses the important historic cast iron, so is a good outcome.

It is not possible to pass comment on the internal demolition work, and whether the loss of some of these original elements will impact the heritage value of the building, as there are no internal views provided, and I have not seen inside the building. It is likely that the loss of the original stair, if it is in good condition, is not an ideal outcome, but it is debatable as to whether it would impact the heritage value of the place.

Overall the proposed addition is an acceptable outcome in terms of the impact on the Local Heritage Place and the streetscape. The changes to the front façade are relatively minor and will allow for the continued use and upgrading of the building.



City of
Norwood
Payneham
& St Peters

Memorandum

To	Geoff Parsons (Manager, Development Assessment) and Mark Thomson (Consultant Planner)		
From	Rebecca van der Pennen (Traffic Engineer)		
Date	9/10/2023	File Number	A757703
Subject	Traffic Comments – 120 Rundle Street, Kent Town – DA 23014875		

Development application 23014875 was referred to the Council's traffic team for review on the 7th of September 2023. Gayle Buckby (Manager, Traffic and Integrated Transport) provided the following comments which were provided to the applicant on the 12th of September 2023;

Can the following be clarified.

- *Can turn paths be shown with accurate B85 vehicles? The vehicles shown are 4.6m long, but a B85 vehicle is 5.2m long.*
- *The manoeuvring would likely require more than a 3-point turn (5-point or more).*
- *The turn paths shown, may be possible if the garage is reduced in length, however the location of the tree is a concern.*
- *If a vehicle entered the driveway and the car parks were full, it would need to reverse out onto Rundle Street.*
- *Can a turn path be provided that shows how a car parked in Patient space No. 2 would manoeuvre out?*
- *Is there a formal arrangement in place for the public turn around area to occur within the private property space?*

Other considerations:

- *The driveway is 40 metres from the busy junction of Fullarton Road and it would be hazardous if vehicles reversed out.*
- *I note the stobie pole that is within the crossover on Rundle Street that would also make reversing out very difficult.*

The applicant provided updated documentation and I was requested to review whether Gayle's initial comments had been addressed via email on the 22nd of September 2023. Upon my review I provided the following comments over the course of a few emails to Mark;

- *The patient car park dimensions do not meet the requirements set out in AS2890.1.*
- *Pedestrian safety sightlines from site access should be considered as per AS2890.1 Figure 3.3. Vegetation within the sight line triangles should be kept to below eye height (1.15m) to maximise sight lines to pedestrians crossing the driveway.*
- *The 300mm clearance on the left side of the plan (garden bed) will be utilised as access to the vehicles and a landscaped treatment will block this (driver's door on this side of the vehicle).*
- *Vehicle turnaround onsite*
 - *The standards do state a B99 should be used for access driveways where failure of a vehicle to physically fit into the facility would result in a possible hazard. However, there can be some allowance for special circumstances. Based on the provided B85 turn path the vehicle is just able to turnaround with the required clearance. A B99 would not be able to turnaround on the site. Rundle Street is not a Council road so the reversing movement onto Rundle Street is for DIT to provide comment on; and*
 - *The provided vehicle turn path requires use of the pedestrian walkway to facilitate the movement. It is noted that low pedestrian volumes are expected onsite.*

- *No response provided for the following previous comments;*
 - *Is there a formal arrangement in place for the public turn around area to occur within the private property space?*

The applicant has updated their plans by increasing the length of the patient car park spaces to meet the dimension of the Australian Standard.

5.3 DEVELOPMENT NUMBER 23023331 – PAUL GALLASCH – 133-139 MAGILL ROAD, STEPNEY

DEVELOPMENT NO.:	23023331
APPLICANT:	Paul Gallasch
ADDRESS:	133-139 MAGILL RD STEPNEY SA 5069
NATURE OF DEVELOPMENT:	Change of use from restaurant to bistro/bar
ZONING INFORMATION:	<p>Zones:</p> <ul style="list-style-type: none"> • Suburban Main Street <p>Overlays:</p> <ul style="list-style-type: none"> • Local Heritage Place • Local Heritage Place • Prescribed Wells Area • Regulated and Significant Tree • Traffic Generating Development • Urban Transport Routes • Airport Building Heights (Regulated) • Advertising Near Signalised Intersections • Future Road Widening • Heritage Adjacency • Hazards (Flooding - General) • Local Heritage Place <p>Technical Numeric Variations (TNVs):</p> <ul style="list-style-type: none"> • Maximum Building Height (Levels) (Maximum building height is 2 levels)
LODGEMENT DATE:	11 Aug 2023
RELEVANT AUTHORITY:	Assessment panel/Assessment manager at City of Norwood, Payneham and St. Peters
PLANNING & DESIGN CODE VERSION:	P&D Code (in effect) - Version 2023.13 - 31/08/2023
CATEGORY OF DEVELOPMENT:	Code Assessed - Performance Assessed
NOTIFICATION:	Yes
RECOMMENDING OFFICER:	Edmund Feary Senior Urban Planner
REFERRALS STATUTORY:	None
REFERRALS NON-STATUTORY:	None

CONTENTS:

APPENDIX 1:	Relevant P&D Code Policies	ATTACHMENT 5:	Representations
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DETAILED DESCRIPTION OF PROPOSAL:

The proposal involves a change of use from the existing approved restaurant to a bistro/bar. The business intending to move in plans to offer simple meals and coffee, as well as beer and wine. It would be a “literary style” bar, and therefore intends to host regular book launches, talks, poetry readings and art classes.

BACKGROUND:

The site has an existing approval for a restaurant, which was approved for the inside of the shop under DA 155/220/2010, and varied to include the outdoor area under DA 155/394/2011. There was no condition limiting hours of operation applied to the authorisation for this Development Application. This restaurant also had a liquor licence which allowed for the service of alcohol until 1am, seven days a week.

While a bistro is substantially similar to a restaurant, case law suggests that a bar is a distinct kind of land use from cafés, restaurants and the like, and is not a kind of shop. Therefore, while an application for the bistro element would not be necessary, the bar element is considered distinct and requires a change of use. As a distinct land use, it is not exempted from Public Notification under Table 5, and notification has therefore occurred.

While this application initially proposed increasing the capacity but this is no longer being sought. The capacity of the venue would therefore remain 49 persons.

SUBJECT LAND & LOCALITY:

Site Description:

Location reference: 133-139 MAGILL RD STEPNEY SA 5069

Title ref.: CT
5537/763

Plan Parcel:
F134759 AL8

Council: THE CITY OF NORWOOD PAYNEHAM
AND ST PETERS

- | | |
|----------------------|--|
| Shape: | irregular- the site includes one shopfront facing Magill Road (known as 137 Magill Road, Stepney) and a courtyard to the rear. The courtyard includes access to Ann Street on the eastern side. |
| Frontage width: | Approx. 6.6m |
| Depth: Approx. | 28.2m |
| Area: approx. | 300m ² |
| Topography: | relatively flat |
| Existing Structures: | single-storey shopfront facing Magill Road (Local Heritage Place) forming a continuous façade to the street, with verandahs overhanging the footpath. Structures around the rear courtyard are generally industrial in character including workshops and “outhouse” style toilets. |
| Existing Vegetation: | a series of small trees are present through the courtyard area. |

Locality

The locality for the proposed development extends along Magill Road approximately 50m in each direction, and down Ann Street by approximately 60m.

Along Magill Road, the building is one of a series of historic, continuous shopfronts, with verandahs overhanging the footpath. These are generally used for retail purposes of various kinds including cafés and homewares stores.

Along Ann Street, there are other commercial uses including offices and a café, before transitioning to a denser residential area which is currently under construction - that being the "Otto's" development.

CONSENT TYPE REQUIRED:

Planning Consent

CATEGORY OF DEVELOPMENT:

- **PER ELEMENT:**
Other - Commercial/Industrial - Bar: Code Assessed - Performance Assessed
- **OVERALL APPLICATION CATEGORY:**
Code Assessed - Performance Assessed
- **REASON**
P&D Code; Undefined land use

PUBLIC NOTIFICATION

- **REASON**
Undefined land use which is therefore not exempt from notification under Table 5 of the Suburban Main Street Zone.
- **LIST OF REPRESENTATIONS**

Given Name	Family Name	Address	Position	Wishes to be heard?
Nabeel	Najjar	12 Treloar Avenue, Kensington Park	Opposed	No
Stuart	Williams	131 Frederick St, Evandale	Opposed	No

*This representor represents business interests near the site but has provided their home address

- **SUMMARY**

Concerns raised by the two representors are as follows:

1. There is insufficient vehicle parking provided; and,
2. There are already too many similar businesses in the area.

AGENCY REFERRALS

None

INTERNAL REFERRALS

None

PLANNING ASSESSMENT

The application has been assessed against the relevant provisions of the Planning & Design Code, which are contained in Appendix One.

Land Use

The existing approved use is as a restaurant and utilises the same footprint as the proposed bistro/bar. The approved restaurant which formerly occupied the site also operated with both daytime and nighttime trade.

Nonetheless, a bar is a distinct use which is notable for its higher intensity and greater focus on the consumption of alcohol. It is noted that the previously approved use had a liquor licence which allowed consumption until 1am every night of the week.

The Suburban Main Street Zone does envisage these kinds of activities. It is noted POs 1.1, 1.2 and 1.3 below state the following:

Retail, office, entertainment and recreation uses are supplemented by other businesses that provide a range of goods and services to the local community.

Land uses promote movement and activity during daylight and evening hours, including restaurants, educational, community and cultural facilities, and accommodation for visitors and residents.

Ground floor uses contribute to an active and vibrant main street.

The proposed use contributes to a vibrant main street, and it is notable in promoting both daytime and nighttime activity as envisaged in PO 1.2.

It is therefore considered that on a fundamental level, the use is appropriate in the Zone. Further detail on its impacts will be provided under the “Environmental Factors” section below.

Heritage

While the building is a Local Heritage Place, the proposed use is highly unlikely to have any detrimental impact on its heritage value. It is noted that the building is an historic shopfront and has been used for similar purposes for many years.

In addition, the heritage value of the building is unlikely to be negatively affected as:

- No structural alterations are proposed;
- The façade of the building will not be affected;
- The use is similar to that previously occupying the site;
- No changes (apart from different branding, which does not require approval) would be perceptible from outside the site.

Traffic Impact, Access and Parking

The site is in a Designated Area for vehicle parking, being in the Suburban Main Street Zone, and being situated along a high frequency public transport corridor.

The proposal would not alter the existing access arrangements, with no off-street parking available on site, nor would it increase the floor area in use.

In accordance with Table 2 of the Transport, Access and Parking module, all non-residential uses should provide the same amount of car parking for the same amount of floor area. Furthermore, existing shortfalls in car parking carry over to a new land use. As a result, there is considered to be no need for additional car parks to be provided to comply with the P&D Code.

One representor expressed concern that it would set a precedent for utilising a courtyard area as floor area to justify the removal of car parks. The courtyard is already approved for use for dining under DA 155/394/2011 and therefore it does not increase the floor area available. Moreover, it does not reduce the number of parking spaces available as suggested by the representor, and it is unlikely that the courtyard could act as a functional car park.

Furthermore, this ability to move between different uses without needing more car parking, despite the likelihood of this use increasing the real demand for spaces, was the exact intent of the designated area parking provisions. Therefore, while “precedent” is not strictly a concept in planning, to some extent it has already been set.

Environmental Factors

Noise

Performance Outcome 1.2 of the Interface Between Land Uses module of the general development policies states:

“Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.”

Performance Outcome 4.1 of the Interface Between Land Uses module of the general development policies states:

“Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).”

Performance Outcome 4.5 of the Interface Between Land Uses module of the general development policies states:

“Outdoor areas associated with licenses premises (such as beer gardens or dining areas) are designed and/or sited to not cause unreasonable noise impact on existing sensitive receivers (or lawfully approved sensitive receivers).”

Since the previous approval, the “Otto’s” development of a series of townhouses immediately behind this site, has commenced. This has brought sensitive receivers closer to the site. The nearest approved sensitive receiver is (or will be when its construction is finished) some 14m from the edge of the courtyard, but there is a two-storey building, constructed from rendered concrete blocks, between the dwellings and the courtyard.

It is noted that no representations were received which mentioned noise impacts in any way. Given the large building which sits between the courtyard and the sensitive receivers, it is considered that this is sufficient to minimise adverse noise impacts to ensure that they are not unreasonable as per the Performance Outcomes above.

Performance Outcome 4.6 of the Interface Between Land Uses module states:

“Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.”

There is a corresponding Designated Performance Feature, which states:

Development incorporating music includes noise attenuation measures that will achieve the following noise levels:

Assessment location	Music noise level
<i>Externally at the nearest existing or envisaged noise sensitive location</i>	<i>Less than 8dB above the level of background noise ($L_{90,15min}$) in any octave band of the sound spectrum ($LOCT_{10,15} < LOCT_{90,15} + 8dB$)</i>

No acoustic report has been provided or asked for as part of this application which would be able to assess the proposal against this Designated Performance Feature and the test of “suitable acoustic amenity”.

The applicant was informed that the Assessment Manager would likely recommend a condition relating to live music or the like, given the lack of a noise assessment having demonstrated the premises’ suitability for this. No mention of the question of live music was made in the documents which were put to public notification. However the applicant has asked that either no such condition be applied, or a condition which allows some live music and spoken word (e.g. for poetry readings or stand up comedy).

Upon deeper consideration, it is considered unlikely that acoustic (i.e. unamplified) music inside the shop building would cause an unsuitable level of acoustic amenity for the sensitive receivers behind. It is also plausible that this standard can be achieved in the

courtyard, thought this is less certain. Nonetheless, it is considered that a blanket ban on live music is likely to be unnecessarily restrictive.

A generic condition is recommended which can provide flexibility, while ensuring that, if necessary, action can be taken.

“Any live or amplified music or spoken word (e.g. in the form of a poetry reading or comedy show) shall be maintained at a reasonable volume so as to ensure a suitable level of acoustic amenity for nearby sensitive receivers to the reasonable satisfaction of the Assessment Manager or their Delegate.”

A further generic condition relating to other noise is also recommended.

Capacity

The capacity of the premises is to be unchanged- 49 persons. This is considered to be reasonable for the size of the premises and is not considered to contribute to an unreasonable level of nuisance or the like. As the increased capacity that was originally proposed is what is provided for in the business description, which would be part of the stamped plans, a condition to clarify the capacity is recommended.

Hours of Operation

Performance Outcome 2.1 of the Interface Between Land Uses module of the general development policies states:

“Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having to regard to:

- a) the nature of the development*
- b) measures to mitigate off-site impacts*
- c) the extent to which the development is desired in the zone*
- d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.”*

It must be considered that the existing approval, albeit for a less intense use, had no restrictions on hours of operation. Nonetheless, it is considered that placing a restriction on hours of operation would be necessary to ensure that the use does not create a nuisance.

Given the greater potential for noise to emanate from the outdoor area, and thus the greater potential for amenity impacts, it is considered worthwhile to place greater restrictions on operating hours in the courtyard.

The hours which were provided and were listed during public notification were:

- Sun-Thurs: 7am until 11pm (Courtyard until 10pm)
- Fri – Sat: 7am until 1am (Courtyard until 12am)

Representors did not object to the hours proposed because they believe they would create a nuisance- rather, objections were based on market competition and car parking grounds, which are addressed separately.

To consider the points raised in the Performance Outcome above:

- a) The nature of development is more intense and disruptive than the existing use. However, the extent to which it is more disruptive is likely to be reasonably limited considering the existing liquor licence until 1am, and that the capacity would not increase. Therefore, the nature of development is not considered to be so disruptive as to require substantially reduced hours.
- b) Mitigation measures are more present inside the building than in the courtyard. Nonetheless, the large building between the courtyard and any sensitive receivers is likely to provide significant mitigation.
- c) The development is not specifically envisaged in the Zone, but it is not a use that is specifically envisaged in any zones. As noted in the *Land Use* section, the use is consistent with the Performance Outcomes of the Zone which relate to land use, meaning that a logical person could conclude it is reasonably anticipated in the Zone.
- d) No such measures are likely to be necessary.

Therefore, it is considered that the hours of operation provided for the public notification period are appropriate.

Competition

It is not the place of an assessment for Planning Consent to determine an application on the grounds of market competition. There is no policy in the Code in relation to this, and it would violate fundamental principles about Governmental control of the free market. While the number of cafés and eateries, and indeed the existing hotel, in the area has been noted by both representors, this is not considered to be a valid reason to refuse Planning Consent.

CONCLUSION

The application seeks a change of use from a restaurant to a bistro/bar, with the intending business planning to host events such as book launches, poetry readings and the like, as well as being a gathering place for the local community.

The use is of a kind envisaged by the Performance Outcomes of the Suburban Main Street Zone, though not specifically envisaged anywhere by the Code as it is not of a defined kind. This use does not increase the theoretical demand for vehicle parking, due to being in a Designated Area.

While no detailed noise assessment has been conducted, it is considered that the increase in noise from the existing approved use is not sufficiently substantial as to warrant further professional advice.

RECOMMENDATION

It is recommended that the Council Assessment Panel resolve that:

1. Pursuant to Section 107(2)(c) of the Planning, Development and Infrastructure Act 2016, and having undertaken an assessment of the application against the Planning and Design Code, the application is NOT seriously at variance with the provisions of the Planning and Design Code; and

Development Application Number 23023331, by Paul Gallasch is granted Planning Consent subject to the following conditions:

CONDITIONS

Planning Consent

Condition 1

The development granted Planning Consent shall be undertaken and completed in accordance with the stamped plans and documentation, except where varied by conditions below (if any).

Condition 2

Noise emanating from the premises shall be maintained at a level that does not cause an unreasonable nuisance to occupiers of land in the locality, to the reasonable satisfaction of the Assessment Manager or their delegate.

Condition 3

The hours of operation of the premises shall be restricted to following times, noting the different times for outdoor and indoor areas to reduce noise impacts on neighbouring residents:

- Sun-Thurs: indoors 7am-11pm, outdoors 7am-10pm
- Fri - Sat: indoors 7am-1am, outdoors 7am-12am

Condition 4

All deliveries to the site and waste collection from the site shall be restricted to the following times:

- Monday-Saturday 7am-7pm

Condition 5

Any live or amplified music or spoken word (e.g. in the form of a poetry reading or comedy show) shall be maintained at a reasonable volume so as to ensure a suitable level of acoustic amenity for nearby sensitive receivers to the reasonable satisfaction of the Assessment Manager or their Delegate.

Condition 6

The approved capacity of the premises herein approved shall be 49 persons.

ADVISORY NOTES

Planning Consent

Advisory Note 1

No work can commence on this development unless a Development Approval has been obtained. If one or more Consents have been granted on this Decision Notification Form, you must not start any site works or building work or change of use of the land until you have received notification that Development Approval has been granted.

Advisory Note 2

Consents issued for this Development Application will remain valid for the following periods of time:

1. Planning Consent is valid for 24 months following the date of issue, within which time Development Approval must be obtained;
2. Development Approval is valid for 24 months following the date of issue, within which time works must have substantially commenced on site;
3. Works must be substantially completed within 3 years of the date on which Development Approval is issued.

If an extension is required to any of the above-mentioned timeframes a request can be made for an extension of time by emailing the Planning Department at townhall@npsp.sa.gov.au. Whether or not an extension of time will be granted will be at the discretion of the relevant authority.

Advisory Note 3

Appeal Rights - General rights of review and appeal exist in relation to any assessment, request, direction or act of a relevant authority in relation to the determination of this application, including conditions.

Advisory Note 4

The Applicant is reminded of its responsibilities under the *Environment Protection Act 1993*, to not harm the environment. Specifically, paint, plaster, concrete, brick wastes and wash waters should not be discharged into the stormwater system, litter should be appropriately stored on site pending removal, excavation and site disturbance should be limited, entry/exit points to the site should be managed to prevent soil being carried off site by vehicles, sediment barriers should be used (particularly on sloping sites), and material stockpiles should all be placed on site and not on the footpath or public roads or reserves. Further information is available by contacting the EPA.

Advisory Note 5

The granting of this consent does not remove the need for the beneficiary to obtain all other consents which may be required by any other legislation.

The Applicant's attention is particularly drawn to the requirements of the *Fences Act 1975* regarding notification of any neighbours affected by new boundary development or boundary fencing. Further information is available in the 'Fences and the Law' booklet available through the Legal Services Commission.

Advisory Note 6

The Applicant is advised that construction noise is not allowed:

1. on any Sunday or public holiday; or
2. after 7pm or before 7am on any other day

Advisory Note 7

The Applicant is advised that any works undertaken on Council owned land (including but not limited to works relating to crossovers, driveways, footpaths, street trees and stormwater connections) will require the approval of the Council pursuant to the *Local Government Act 1999* prior to any works being undertaken. Further information may be obtained by contacting Council's Public Realm Compliance Officer on 8366 4513.

Advisory Note 8

The Applicant is advised that the condition of the footpath, kerbing, vehicular crossing point, street tree(s) and any other Council infrastructure located adjacent to the subject land will be inspected by the Council prior to the commencement of building work and at the completion of building work. Any damage to Council infrastructure that occurs during construction must be rectified as soon as practicable and in any event, no later than four (4) weeks after substantial completion of the building work. The Council reserves its right to recover all costs associated with remedying any damage that has not been repaired in a timely manner from the appropriate person.

Advisory Note 9

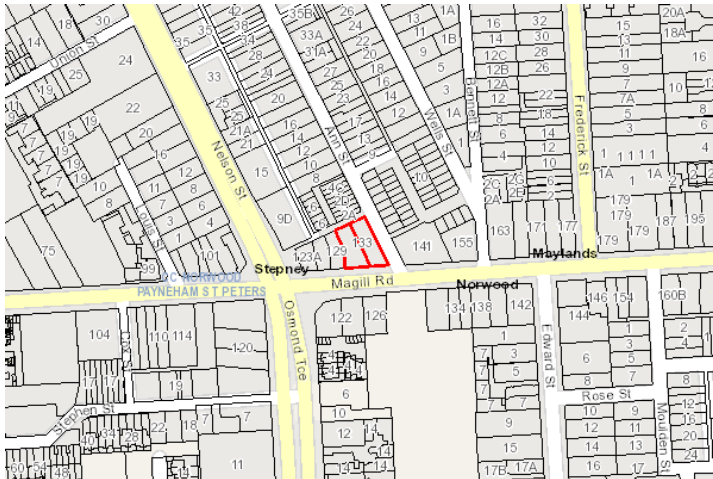
The Council has not surveyed the subject land and has, for the purpose of its assessment, assumed that all dimensions and other details provided by the Applicant are correct and accurate.

133-139 MAGILL RD STEPNEY SA 5069

Address:

Click to view a detailed interactive [SAILIS](#) in SAILIS

To view a detailed interactive property map in SAPPA click on the map below



Property Zoning Details

Zone	Suburban Main Street
Overlay	<ul style="list-style-type: none"> Airport Building Heights (Regulated) (<i>All structures over 45 metres</i>) Advertising Near Signalised Intersections Future Road Widening Heritage Adjacency Hazards (Flooding - General) Local Heritage Place (7837) Local Heritage Place (7838) Local Heritage Place (7839) Prescribed Wells Area Regulated and Significant Tree Traffic Generating Development Urban Transport Routes
Local Variation (TNV)	Maximum Building Height (Levels) (<i>Maximum building height is 2 levels</i>)

Development Pathways

- Suburban Main Street

1. Accepted Development
Means that the development type does not require planning consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.
 - Brush fence
 - Building alterations
 - Building work on railway land
 - Consulting room
 - Office
 - Partial demolition of a building or structure
 - Shop
 - Solar photovoltaic panels (roof mounted)
 - Water tank (above ground)
 - Water tank (underground)

2. Code Assessed - Deemed to Satisfy
Means that the development type requires consent (planning approval). Please ensure compliance with relevant land use and development controls in the Code.
 - Consulting room
 - Office
 - Shop
 - Temporary accommodation in an area affected by bushfire

3. Code Assessed - Performance Assessed
Performance Assessed development types listed below are those for which the Code identifies relevant policies. Additional development types that are not listed as Accepted, Deemed to Satisfy or Restricted default to a Performance assessed Pathway. Please contact your local council for more information.
 - Advertisement
 - Consulting room
 - Demolition
 - Dwelling
 - Fence
 - Land division
 - Office
 - Retaining wall
 - Shop
 - Store
 - Telecommunications facility
 - Tree-damaging activity
 - Verandah

4. Impact Assessed - Restricted
Means that the development type requires approval. Classes of development that are classified as Restricted are listed in Table 4 of the relevant Zones.

Part 2 - Zones and Sub Zones

Suburban Main Street Zone

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	A mix of land uses including retail, office, commercial, community, civic and medium density residential development that supports the local area.
DO 2	A high degree of pedestrian activity and main street activity with well-lit and visually engaging shop fronts and business displays including alfresco seating and dining facilities.
DO 3	An intimate public realm with active streets created by integrated mixed use buildings.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
<p>PO 1.1</p> <p>Retail, office, entertainment and recreation uses are supplemented by other businesses that provide a range of goods and services to the local community.</p>	<p>DTS/DPF 1.1</p> <p>Development comprises one or more of the following:</p> <ul style="list-style-type: none"> (a) Advertisement (b) Child care facility (c) Cinema (d) Community facility (e) Consulting room (f) Dwelling (g) Health facility (h) Hotel (i) Indoor recreation facility (j) Library (k) Office (l) Place of worship (m) Shop (n) Tourist accommodation.
<p>PO 1.2</p> <p>Land uses promote movement and activity during daylight and evening hours, including restaurants, educational, community and cultural facilities, and accommodation for visitors and residents.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Ground floor uses contribute to an active and vibrant main street.</p>	<p>DTS/DPF 1.3</p> <p>Shop, office, or consulting room uses are located on the ground floor level of buildings.</p>
<p>PO 1.4</p> <p>Dwellings developed in conjunction with non-residential uses to support business, entertainment and recreational activities contribute to making the main street precinct and pedestrian thoroughfares pleasant and lively places.</p>	<p>DTS/DPF 1.4</p> <p>Dwellings developed in conjunction with non-residential uses are sited:</p> <ul style="list-style-type: none"> (a) at upper levels of buildings with non-residential uses located at ground level or (b) behind non-residential uses on the same allotment.

<p>PO 1.5</p> <p>Tourist accommodation and visitor attractions support the visiting public and holiday makers.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
<p>PO 1.6</p> <p>Development is sited and designed to achieve or maintain a vibrant and interesting streetscape within retail areas.</p>	<p>DTS/DPF 1.6</p> <p>Any of the following:</p> <ul style="list-style-type: none"> (a) shop, other than a bulky goods outlet with a gross leasable floor area more than 500m² (b) cinema (c) hotel (d) licensed premises.
<p>PO 1.7</p> <p>Changes in the use of land encourage the efficient reuse of commercial premises to maintain and enhance vibrancy within activity centres.</p>	<p>DTS/DPF 1.7</p> <p>A change of use to a shop, office, consulting room or any combination of these uses where all of the following are achieved:</p> <ul style="list-style-type: none"> (a) the area to be occupied by the proposed development is in an existing building and is currently used as a shop, office, consulting room or any combination of these uses (b) if the proposed change of use is for a shop that primarily involves the handling and sale of foodstuffs, areas used for the storage and collection of refuse are sited at least 10m from the site of a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions (c) if the proposed change of use is for a shop that primarily involves heating and cooking of foodstuffs in a commercial kitchen and is within 30m of any neighbourhood-type zone boundary or a dwelling (other than a dwelling directly associated with the proposed shop), an exhaust duct and stack (chimney) exists or is capable of being installed for discharging exhaust emissions (d) if the change in use involves a gross leasable floor area greater than 250m² and has direct frontage to an arterial road, it achieves: the primary vehicle access (being the access where the majority of vehicles access / egress the site of the proposed development) is from a road that is not an arterial road or the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared (e) off-street vehicular parking exists in accordance with the rate(s) specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas to the nearest whole number, except where: <ul style="list-style-type: none"> (i) the building is a local heritage place or (ii) the required contribution will be made into a relevant car parking offset scheme (other than where a relevant contribution has previously been made) or (iii) the development is located on a site that operates as an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.
<p>Built Form and Character</p>	
<p>PO 2.1</p> <p>Buildings sensitively frame the main street and public spaces and provide overall visual relief from building height and mass.</p>	<p>DTS/DPF 2.1</p> <p>Buildings:</p> <ul style="list-style-type: none"> (a) include a clearly defined podium or street wall with a maximum building height of 1 building level or 4m in height (b) have levels above the defined podium or street wall setback a minimum of 2m from that wall.
<p>PO 2.2</p> <p>Buildings preserve the main street appearance by complementing the key shop-front elements such as narrow buildings and tenancy footprints with frequently repeated frontages, and clear-glazed narrow shop front displays above raised display levels (base stall boards) and recessed entries.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Pedestrian shelter and shade is provided over footpaths through the use of structures such as awnings, canopies and verandas.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Buildings are adaptable and flexible to accommodate a range of land uses, including retail, office and residential without the need for significant change to the</p>	<p>DTS/DPF 2.4</p> <p>Ground floor levels of buildings incorporate a minimum ceiling height of 3.5m.</p>

building.	
<p>PO 2.5</p> <p>Buildings create visual interest, promote an active interface with the main street frontage and maximise passive surveillance.</p>	<p>DTS/DPF 2.5</p> <p>Not less than 50% of the ground floor primary frontage of buildings is visually permeable, transparent or clear glazed.</p>
<p>PO 2.6</p> <p>Outbuildings, carports and garages located behind the primary building facing the main street ensure vibrancy and activity along the main street.</p>	<p>DTS/DPF 2.6</p> <p>None are applicable.</p>
<p>PO 2.7</p> <p>Development creates an efficient and convenient pedestrian network establishing linkages within the main street and to adjoining zones.</p>	<p>DTS/DPF 2.7</p> <p>None are applicable.</p>
Building height and setbacks	
<p>PO 3.1</p> <p>Building height is consistent with the form expressed in any relevant <i>Maximum Building Height (Levels) Technical and Numeric Variation layer</i> and the <i>Maximum Building Height (Metres) Technical and Numeric Variation layer</i> or is low-to-medium rise, where the height is commensurate with the development site's frontage and depth as well as the main street width, to complement the main street character.</p>	<p>DTS/DPF 3.1</p> <p>Building height is:</p> <p>(a) no greater than:</p> <p style="margin-left: 20px;">(i) the following:</p> <div style="border: 1px solid black; background-color: #0056b3; color: white; padding: 2px; text-align: center; margin: 5px 0;">Maximum Building Height (Levels)</div> <div style="border: 1px solid black; padding: 2px; margin: 2px 0;">Maximum building height is 2 levels</div> <p style="margin-left: 20px;">(ii) in all other cases (i.e. there are blank fields for both maximum building height (metres) and maximum building height (levels)):</p> <p style="margin-left: 40px;">A. where the site has a frontage of at least 25m and depth of at least 50m - 4 building levels up to a height of 15m or</p> <p style="margin-left: 40px;">B. in all other cases - 3 building levels up to a height of 12m</p> <p>and</p> <p>(b) not less than:</p> <p>In relation to DTS/DPF 3.1, in instances where:</p> <p>(a) more than one value is returned is returned in the same field:</p> <p style="margin-left: 20px;">(i) for the purpose of DTS/DPF 3.1(a)(i), refer to the <i>Maximum Building Height (Metres) Technical and Numeric Variation layer</i> or <i>Maximum Building Height (Levels) layer</i> in the SA planning database to determine the applicable value relevant to the site of the proposed development</p> <p style="margin-left: 20px;">(ii) for the purpose of DTS/DPF 3.1(b) refer to the <i>Minimum Building Height (Levels) Numeric Variation layer</i> in the SA planning database to determine the applicable value relevant to the site of the proposed development</p> <p>(b) no value is returned for DTS/DPF 3.1(b) (ie there is a blank field), then there is no minimum building height and DTS/DPF 3.1(b) is met.</p>
<p>PO 3.2</p> <p>Buildings mitigate visual impacts of building massing on residential development within a neighbourhood-type zone.</p>	<p>DTS/DPF 3.2</p> <p>Buildings constructed within a building envelope provided by a 45 degree plane measured from a height of 3 metres above natural ground level at the boundary of an allotment used for residential purposes in a neighbourhood-type zone as shown in the following diagram (except where this boundary is a southern boundary or where this boundary is the street boundary):</p>



PO 3.3

Buildings mitigate overshadowing of residential development within a neighbourhood-type zone.

DTS/DPF 3.3

Buildings on sites with a southern boundary adjoining an allotment used for residential purposes in a neighbourhood-type zone are constructed within a building envelope provided by a 30 degree plane grading north measured from a height of 3m above natural ground level at the southern boundary, as shown in the following diagram (except where this boundary is a street boundary):

PO 3.4

Buildings with no setbacks from road boundaries achieve a continuity of street façade to the main street, but with sections of building set back to create outdoor dining areas, visually interesting building entrances and intimate, active spaces.

DTS/DPF 3.4

None are applicable.

PO 3.5

Buildings with no setback from side boundaries achieve a continuity of street façade to the main street.

DTS/DPF 3.5

Except where contrary to DTS/DPF 3.2 or 3.3, building walls located on the site's side boundaries, with the front wall set back in line with neighbouring buildings.

PO 3.6

Buildings that are set back from rear boundaries (other than street boundaries) minimise impacts on neighbouring properties, including access to natural light and ventilation.

DTS/DPF 3.6

Buildings are set back a minimum 3 metres from rear boundaries where the subject land directly abuts an allotment of a different zone, except where the development abuts the wall of an existing or simultaneously constructed building on the adjoining land.

PO 3.7

Buildings are set back from rear access ways to provide adequate manoeuvrability for vehicles.

DTS/DPF 3.7

Buildings are set back from the rear access way have:

- (a) no requirement where the access way is 6.5m or more wide or
- (b) where the access way is less than 6.5m wide, the distance equal to the additional width required to make the access way at least 6.5m wide.

<p>PO 3.8</p> <p>Buildings on an allotment fronting a road that is not a State maintained road, and where land on the opposite side of the road is within a neighbourhood-type zone, provides an orderly transition to the built form scale envisaged in the adjacent zone to complement the streetscape character.</p>	<p>DTS/DPF 3.8</p> <p>None are applicable.</p>
Traffic, parking and access	
<p>PO 4.1</p> <p>Development minimises the need for vehicle crossovers on the main street to reduce conflicts with pedestrians and avoid disruption to the continuity of built form.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Vehicle parking is located behind buildings away from the primary main street frontage and is designed to minimise its impacts on residential amenity.</p>	<p>DTS/DPF 4.2</p> <p>Vehicle parking areas are located behind the building line of the associated building.</p>
Advertisements	
<p>PO 5.1</p> <p>Advertisements are sited and designed to achieve an overall consistency of appearance along individual street frontages.</p>	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>
<p>PO 5.2</p> <p>Freestanding advertisements:</p> <p>(a) identify the associated business(es) (b) are of a size that is commensurate with the scale of the centre and the street frontage (c) avoid visual clutter (d) positively respond to the context without dominating the locality (e) are sited and designed to not detract from the main street character.</p>	<p>DTS/DPF 5.2</p> <p>Freestanding advertisements:</p> <p>(a) do not exceed 8m in height, the adjacent building wall height, or the zone's height allowance (whichever is the lesser) (b) do not have a sign face that exceeds 6m² per side.</p>
Land Division	
<p>PO 6.1</p> <p>Land division and site amalgamation creates allotments that are suitable for a variety of residential and commercial activities and improve the level of development integration.</p>	<p>DTS/DPF 6.1</p> <p>None are applicable.</p>
Concept Plans	
<p>PO 7.1</p> <p>Development is compatible with the outcomes sought by any relevant Concept Plan contained within Part 12 - Concept Plans of the Planning and Design Code to support the orderly development of land through staging of development and provision of infrastructure.</p>	<p>DTS/DPF 7.1</p> <p>The site of the development is wholly located outside any relevant Concept Plan boundary. The following Concept Plans are relevant:</p> <p>In relation to DTS/DPF 7.1, in instances where:</p> <p>(a) one or more Concept Plan is returned, refer to Part 12 - Concept Plans in the Planning and Design Code to determine if a Concept Plan is relevant to the site of the proposed development. Note: multiple concept plans may be relevant. (b) in instances where 'no value' is returned, there is no relevant concept plan and DTS/DPF 7.1 is met.</p>

Table 5 - Procedural Matters (PM) - Notification

The following table identifies, pursuant to section 107(6) of the *Planning, Development and Infrastructure Act 2016*, classes of performance assessed development that are excluded from notification. The table also identifies any exemptions to the placement of notices when notification is required.

Interpretation

Notification tables exclude the classes of development listed in Column A from notification provided that they do not fall within a corresponding exclusion prescribed in Column B.

Where a development or an element of a development falls within more than one class of development listed in Column A, it will be excluded from notification if it is excluded (in its entirety) under any of those classes of development. It need not be excluded under all applicable classes of development.

Where a development involves multiple performance assessed elements, all performance assessed elements will require notification (regardless of whether one or more elements are excluded in the applicable notification table) unless every performance assessed element of the application is excluded in the applicable notification table, in which case the application will not require notification.

A relevant authority may determine that a variation to 1 or more corresponding exclusions prescribed in Column B is minor in nature and does not require notification.

Class of Development (Column A)	Exceptions (Column B)
1. Development which, in the opinion of the relevant authority, is of a minor nature only and will not unreasonably impact on the owners or occupiers of land in the locality of the site of the development.	None specified.
2. Any kind of development where the site of the development is not adjacent land to a site (or land) used for residential purposes in a neighbourhood-type zone.	<p>Except any of the following:</p> <ol style="list-style-type: none"> 1. the demolition (or partial demolition) of a State or Local Heritage Place (other than an excluded building) 2. the demolition (or partial demolition) of a building in a Historic Area Overlay (other than an excluded building).
<p>3. Any development involving any of the following (or of any combination of any of the following):</p> <ol style="list-style-type: none"> (a) advertisement (b) child care facility (c) cinema (d) community facility (e) consulting room (f) dwelling located above a non-residential building level (g) indoor recreation facility (h) library (i) office (j) place of worship (k) shop (l) temporary public service depot (m) tourist accommodation. 	<p>Except development that exceeds the maximum building height specified in Suburban Main Street Zone DTS/DPF 3.1 or does not satisfy any of the following:</p> <ol style="list-style-type: none"> 1. Suburban Main Street Zone DTS/DPF 3.2 2. Suburban Main Street Zone DTS/DPF 3.3.
<p>4. Any development involving any of the following (or of any combination of any of the following):</p> <ol style="list-style-type: none"> (a) air handling unit, air conditioning system or exhaust fan (b) carport (c) deck (d) fence (e) internal building works (f) land division (g) outbuilding (h) pergola (i) private bushfire shelter (j) recreation area (k) replacement building (l) retaining wall (m) shade sail (n) solar photovoltaic panels (roof mounted) (o) swimming pool or spa pool and associated swimming pool safety features (p) temporary accommodation in an area affected by bushfire (q) tree damaging activity (r) verandah (s) water tank. 	None specified.
5. Demolition.	<p>Except any of the following:</p> <ol style="list-style-type: none"> 1. the demolition (or partial demolition) of a State or Local Heritage Place (other than an excluded building) 2. the demolition (or partial demolition) of a building in a Historic Area Overlay (other than an excluded building).
6. Railway line.	Except where located outside of a rail corridor or rail reserve.

Placement of Notices - Exemptions for Performance Assessed Development

None specified.

Placement of Notices - Exemptions for Restricted Development

None specified.

Part 3 - Overlays

Advertising Near Signalised Intersections Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Provision of a safe road environment by reducing driver distraction at key points of conflict on the road.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Advertisements Near Signalised Intersections	
<p>PO 1.1</p> <p>Advertising near signalised intersections does not cause unreasonable distraction to road users through illumination, flashing lights, or moving or changing displays or messages.</p>	<p>DTS/DPF 1.1</p> <p>Advertising:</p> <ul style="list-style-type: none"> (a) is not illuminated (b) does not incorporate a moving or changing display or message (c) does not incorporate a flashing light(s).

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Advertisement or advertising hoarding that:</p> <ul style="list-style-type: none"> (a) is within 100m of a: <ul style="list-style-type: none"> (i) signalised intersection or (ii) signalised pedestrian crossing and (b) will: <ul style="list-style-type: none"> (i) be internally illuminated or (ii) incorporate a moving or changing display or message or (iii) incorporate a flashing light. 	Commissioner of Highways.	To provide expert technical assessment on potential risks relating to pedestrian and road safety which may arise from advertisements near intersections.	Development of a class to which Schedule 9 clause 3 item 21 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Airport Building Heights (Regulated) Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Management of potential impacts of buildings and generated emissions to maintain operational and safety requirements of registered and certified commercial and military airfields, airports, airstrips and helicopter landing sites.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 Building height does not pose a hazard to the operation of a certified or registered aerodrome.	DTS/DPF 1.1 Buildings are located outside the area identified as 'All structures' (no height limit is prescribed) and do not exceed the height specified in the Airport Building Heights (Regulated) Overlay which applies to the subject site as shown on the SA Property and Planning Atlas. In instances where more than one value applies to the site, the lowest value relevant to the site of the proposed development is applicable.
PO 1.2 Exhaust stacks are designed and sited to minimise plume impacts on aircraft movements associated with a certified or registered aerodrome.	DTS/DPF 1.2 Development does not include exhaust stacks.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Any of the following classes of development: (a) building located in an area identified as 'All structures' (no height limit is prescribed) or will exceed the height specified in the <i>Airport Building Heights (Regulated) Overlay</i> (b) building comprising exhaust stacks that generates plumes, or may cause plumes to be generated, above a height specified in the <i>Airport Building Heights (Regulated) Overlay</i> .	The airport-operator company for the relevant airport within the meaning of the <i>Airports Act 1996</i> of the Commonwealth or, if there is no airport-operator company, the Secretary of the Minister responsible for the administration of the <i>Airports Act 1996</i> of the Commonwealth.	To provide expert assessment and direction to the relevant authority on potential impacts on the safety and operation of aviation activities.	Development of a class to which Schedule 9 clause 3 item 1 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Future Road Widening Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development which is consistent with and will not compromise efficient delivery of future road widening requirements.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Future Road Widening	
PO 1.1 Development does not compromise or is located and designed to minimise its impact on future road widening requirements.	DTS/DPF 1.1 Development does not involve building work, or building work is located wholly outside the land subject to the 6m Consent Area, the C Type Requirement or the Strip Requirement of the Metropolitan Adelaide Road Widening Plan.

Procedural Matters (PM)

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Other than where all deemed-to-satisfy criteria for all policies relevant to this referral are met, development (including the division of land) that is within or may encroach within a Future Road Widening Area.	Commissioner of Highways.	To provide expert technical assessment and direction to the relevant authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 4 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Hazards (Flooding – General) Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Impacts on people, property, infrastructure and the environment from general flood risk are minimised through the appropriate siting and design of development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use	
PO 1.1 Buildings housing vulnerable people, community services facilities, key infrastructure and emergency services are sited away from flood areas enable uninterrupted operation of services and reduce likelihood of entrapment.	DTS/DPF 1.1 Child care facilities, educational facilities, retirement and supported accommodation, emergency services facilities, hospitals and prisons located outside the 1% AEP flood event.
Flood Resilience	
PO 2.1 Development is sited, designed and constructed to prevent the entry of floodwaters	DTS/DPF 2.1 Habitable buildings, commercial and industrial buildings, and buildings used for

Where the entry of flood waters is likely to result in undue damage to or compromise ongoing activities within buildings.	animal keeping incorporate a finished ground and floor level not less than: In instances where no finished floor level value is specified, a building incorporates a finished floor level at least 300mm above the height of a 1% AEP flood event.
Environmental Protection	
PO 3.1 Buildings and structures used either partly or wholly to contain or store hazardous materials are designed to prevent spills or leaks leaving the confines of the building during a 1% AEP flood event to avoid potential environmental harm.	DTS/DPF 3.1 Development involving the storage or disposal of hazardous materials is wholly located outside of the 1% AEP flood plain or flow path.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Heritage Adjacency Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development adjacent to State and Local Heritage Places maintains the heritage and cultural values of those Places.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<small>Built Form</small>	
PO 1.1 Development adjacent to a State or Local Heritage Place does not dominate, encroach on or unduly impact on the setting of the Place.	DTS/DPF 1.1 None are applicable.
<small>Land Division</small>	
PO 2.1 Land division adjacent to a State or Local Heritage Place creates allotments that are of a size and dimension that enables the siting and setbacks of new buildings from allotment boundaries so that they do not dominate, encroach or unduly impact on the setting of the Place.	DTS/DPF 2.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
Development which in the opinion of the relevant authority materially affects the context within which the State Heritage Place is situated.	Minister responsible for the administration of the <i>Heritage Places Act 1993</i> .	To provide expert assessment and direction to the relevant authority on the potential impacts of development adjacent State Heritage Places.	Development of a class to which Schedule 9 clause 3 item 17 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Local Heritage Place Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development maintains the heritage and cultural values of Local Heritage Places through conservation, ongoing use and adaptive reuse.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Built Form	
PO 1.1 The form of new buildings and structures maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.1 None are applicable.
PO 1.2 Massing, scale and siting of development maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.2 None are applicable.
PO 1.3 Design and architectural detailing (including but not limited to roof pitch and form, openings, chimneys and verandahs) maintains the heritage values of the Local Heritage Place.	DTS/DPF 1.3 None are applicable.
PO 1.4 Development is consistent with boundary setbacks and setting.	DTS/DPF 1.4 None are applicable.

<p>PO 1.5</p> <p>Materials and colours are either consistent with or complement the heritage values of the Local Heritage Place.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
<p>PO 1.6</p> <p>New buildings and structures are not placed or erected between the primary or secondary street boundaries and the façade of a Local Heritage Place.</p>	<p>DTS/DPF 1.6</p> <p>None are applicable.</p>
<p>PO 1.7</p> <p>Development of a Local Heritage Place retains features contributing to its heritage value.</p>	<p>DTS/DPF 1.7</p> <p>None are applicable.</p>
<p>Alterations and Additions</p>	
<p>PO 2.1</p> <p>Alterations and additions complement the subject building and are sited to be unobtrusive, not conceal or obstruct heritage elements and detailing, or dominate the Local Heritage Place or its setting.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Adaptive reuse and revitalisation of Local Heritage Places to support their retention in a manner that respects and references the original use of the Local Heritage Place.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>Ancillary Development</p>	
<p>PO 3.1</p> <p>Ancillary development, including carports, outbuildings and garages, complements the heritage values of the Local Heritage Place.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Ancillary development, including carports, outbuildings and garages, is located behind the building line and does not dominate the Local Heritage Place or its setting.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
<p>PO 3.3</p> <p>Advertising and advertising hoardings are designed to complement the Local Heritage Place, be unobtrusive, be below the parapet line, not conceal or obstruct heritage elements and detailing, or dominate the building or its setting.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Fencing and gates closer to a street boundary (other than a laneway) than the street elevation of the associated building are consistent with the traditional period, style and form of the Local Heritage Place.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
<p>Land Division</p>	
<p>PO 4.1</p> <p>Land division creates allotments that:</p> <p>(a) maintain the heritage values of the Local Heritage Place, including setting</p> <p>(b) are of a dimension to accommodate new development that reinforces and is compatible with the heritage values of the Local Heritage Place.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>Landscape Context and Streetscape Amenity</p>	
<p>PO 5.1</p>	<p>DTS/DPF 5.1</p>

Individually heritage listed trees, parks, historic gardens and memorial avenues are retained unless: (a) trees / plantings are, or have the potential to be, a danger to life or property or (b) trees / plantings are significantly diseased and their life expectancy is short.	None are applicable.
Demolition	
PO 6.1 Local Heritage Places are not demolished, destroyed or removed in total or in part unless: (a) the portion of the Local Heritage Place to be demolished, destroyed or removed is excluded from the extent of listing that is of heritage value or (b) the structural integrity or condition of the Local Heritage Place represents an unacceptable risk to public or private safety and is irredeemably beyond repair.	DTS/DPF 6.1 None are applicable.
PO 6.2 The demolition, destruction or removal of a building, portion of a building or other feature or attribute is appropriate where it does not contribute to the heritage values of the Local Heritage Place.	DTS/DPF 6.2 None are applicable.
Conservation Works	
PO 7.1 Conservation works to the exterior of a Local Heritage Place (and other features identified in the extent of listing) match original materials to be repaired and utilise traditional work methods.	DTS/DPF 7.1 None are applicable.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Prescribed Wells Area Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Sustainable water use in prescribed wells areas.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
PO 1.1 All development, but in particular involving any of the following:	DTS/DPF 1.1 Development satisfies either of the following:

<p>(a) horticulture (b) activities requiring irrigation (c) aquaculture (d) industry (e) intensive animal husbandry (f) commercial forestry</p> <p>has a lawful, sustainable and reliable water supply that does not place undue strain on water resources in prescribed wells areas.</p>	<p>(a) the applicant has a current water licence in which sufficient spare capacity exists to accommodate the water needs of the proposed use or or (b) the proposal does not involve the taking of water for which a licence would be required under the <i>Landscape South Australia Act 2019</i>.</p>
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Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Any of the following classes of development that require or may require water to be taken in addition to any allocation that has already been granted under the <i>Landscape South Australia Act 2019</i>:</p> <p>(a) horticulture (b) activities requiring irrigation (c) aquaculture (d) industry (e) intensive animal husbandry (f) commercial forestry.</p>	<p>The Chief Executive of the Department of the Minister responsible for the administration of the <i>Landscape South Australia Act 2019</i>.</p>	<p>To provide expert technical assessment and direction to the relevant authority on the taking of water to ensure development is undertaken sustainably.</p>	<p>Development of a class to which Schedule 9 clause 3 item 13 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>
<p>Commercial forestry that requires a forest water licence under Part 8 Division 6 of the <i>Landscape South Australia Act 2019</i>.</p>			

Regulated and Significant Tree Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Conservation of regulated and significant trees to provide aesthetic and environmental benefits and mitigate tree loss.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Tree Retention and Health	
<p>PO 1.1</p> <p>Regulated trees are retained where they:</p> <p>(a) make an important visual contribution to local character and amenity (b) are indigenous to the local area and listed under the <i>National Parks and Wildlife Act 1972</i> as a rare or endangered native species and / or (c) provide an important habitat for native fauna.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Significant trees are retained where they:</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>

<p>(a) make an important contribution to the character or amenity of the local area</p> <p>(b) are indigenous to the local area and are listed under the <i>National Parks and Wildlife Act 1972</i> as a rare or endangered native species</p> <p>(c) represent an important habitat for native fauna</p> <p>(d) are part of a wildlife corridor of a remnant area of native vegetation</p> <p>(e) are important to the maintenance of biodiversity in the local environment and / or</p> <p>(f) form a notable visual element to the landscape of the local area.</p>	
<p>PO 1.3</p> <p>A tree damaging activity not in connection with other development satisfies (a) and (b):</p> <p>(a) tree damaging activity is only undertaken to:</p> <ul style="list-style-type: none"> (i) remove a diseased tree where its life expectancy is short (ii) mitigate an unacceptable risk to public or private safety due to limb drop or the like (iii) rectify or prevent extensive damage to a building of value as comprising any of the following: <ul style="list-style-type: none"> A. a Local Heritage Place B. a State Heritage Place C. a substantial building of value <p>and there is no reasonable alternative to rectify or prevent such damage other than to undertake a tree damaging activity</p> <ul style="list-style-type: none"> (iv) reduce an unacceptable hazard associated with a tree within 20m of an existing residential, tourist accommodation or other habitable building from bushfire (v) treat disease or otherwise in the general interests of the health of the tree and / or (vi) maintain the aesthetic appearance and structural integrity of the tree <p>(b) in relation to a significant tree, tree-damaging activity is avoided unless all reasonable remedial treatments and measures have been determined to be ineffective.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>A tree-damaging activity in connection with other development satisfies all the following:</p> <p>(a) it accommodates the reasonable development of land in accordance with the relevant zone or subzone where such development might not otherwise be possible</p> <p>(b) in the case of a significant tree, all reasonable development options and design solutions have been considered to prevent substantial tree-damaging activity occurring.</p>	<p>DTS/DPF 1.4</p> <p>None are applicable.</p>
<p>Ground work affecting trees</p>	
<p>PO 2.1</p> <p>Regulated and significant trees, including their root systems, are not unduly compromised by excavation and / or filling of land, or the sealing of surfaces within the vicinity of the tree to support their retention and health.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>Land Division</p>	
<p>PO 3.1</p> <p>Land division results in an allotment configuration that enables its subsequent development and the retention of regulated and significant trees as far as is reasonably practicable.</p>	<p>DTS/DPF 3.1</p> <p>Land division where:</p> <ul style="list-style-type: none"> (a) there are no regulated or significant trees located within or adjacent to the plan of division or (b) the application demonstrates that an area exists to accommodate subsequent development of proposed allotments after an allowance has been made for a tree protection zone around any regulated tree within and adjacent to the plan of division.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
None	None	None	None

Traffic Generating Development Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes and Major Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from urban transport routes and major urban transport routes.

Performance Outcomes (PO) and Deemed to Satisfy (DTS) / Designated Performance Feature (DPF) Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Traffic Generating Development	
<p>PO 1.1</p> <p>Development designed to minimise its potential impact on the safety, efficiency and functional performance of the State Maintained Road network.</p>	<p>DTS/DPF 1.1</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) building, or buildings, containing in excess of 50 dwellings (b) land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more.
<p>PO 1.2</p> <p>Access points sited and designed to accommodate the type and volume of traffic likely to be generated by development.</p>	<p>DTS/DPF 1.2</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) building, or buildings, containing in excess of 50 dwellings (b) land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more.
<p>PO 1.3</p> <p>Sufficient accessible on-site queuing provided to meet the needs of the development so that queues do not impact on the State Maintained Road network.</p>	<p>DTS/DPF 1.3</p> <p>Access is obtained directly from a State Maintained Road where it involves any of the following types of development:</p> <ul style="list-style-type: none"> (a) building, or buildings, containing in excess of 50 dwellings (b) land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more.

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, any of the following classes of development that are proposed within 250m of a State Maintained Road:</p> <ul style="list-style-type: none"> (a) except where a proposed development has previously been referred under clause (b) - a building, or buildings, containing in excess of 50 dwellings (b) except where a proposed development has previously been referred under clause (a) - land division creating 50 or more additional allotments (c) commercial development with a gross floor area of 10,000m² or more (d) retail development with a gross floor area of 2,000m² or more (e) a warehouse or transport depot with a gross leasable floor area of 8,000m² or more (f) industry with a gross floor area of 20,000m² or more (g) educational facilities with a capacity of 250 students or more. 	Commissioner of Highways.	To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.	Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.

Urban Transport Routes Overlay

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Safe and efficient operation of Urban Transport Routes for all road users.
DO 2	Provision of safe and efficient access to and from Urban Transport Routes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Access - Safe Entry and Exit (Traffic Flow)	
<p>PO 1.1</p> <p>Access is designed to allow safe entry and exit to and from a site to meet the needs of development and minimise traffic flow interference associated with access movements along adjacent State maintained roads.</p>	<p>DTS/DPF 1.1</p> <p>An access point satisfies (a), (b) or (c):</p> <ul style="list-style-type: none"> (a) where servicing a single (1) dwelling / residential allotment: <ul style="list-style-type: none"> (i) it will not result in more than one access point (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 3m and 4m (measured at the site boundary) (b) where the development will result in 2 and up to 6 dwellings: <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site in a forward direction (iii) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees (iv) passenger vehicles (with a length up to 5.2m) can enter and exit the site wholly within the kerbside lane of the road (v) it will have a width of between 5.8m to 6m (measured at the site boundary) and an access depth of 6m (measured from the site boundary into the site) (c) where the development will result in 7 or more dwellings, or is a non-residential land use: <ul style="list-style-type: none"> (i) it will not result in more than one access point servicing the development site (ii) vehicles can enter and exit the site using left turn only movements (iii) vehicles can enter and exit the site in a forward direction (iv) vehicles can cross the property boundary at an angle between 70 degrees and 90 degrees

	<ul style="list-style-type: none"> (v) it will have a width of between 6m and 7m (measured at the site boundary), where the development is expected to accommodate vehicles with a length of 6.4m or less (vi) it will have a width of between 6m and 9m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 6.4m to 8.8m (vii) it will have a width of between 9m and 12m (measured at the site boundary), where the development is expected to accommodate vehicles with a length from 8.8m to 12.5m (viii) provides for simultaneous two-way vehicle movements at the access: <ul style="list-style-type: none"> A. with entry and exit movements for vehicles with a length up to 5.2m vehicles being fully within the kerbside lane of the road <li style="text-align: center;">and B. with entry movements of 8.8m vehicles (where relevant) being fully within the kerbside lane of the road and the exit movements of 8.8m vehicles do not cross the centreline of the road.
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Access - On-Site Queuing

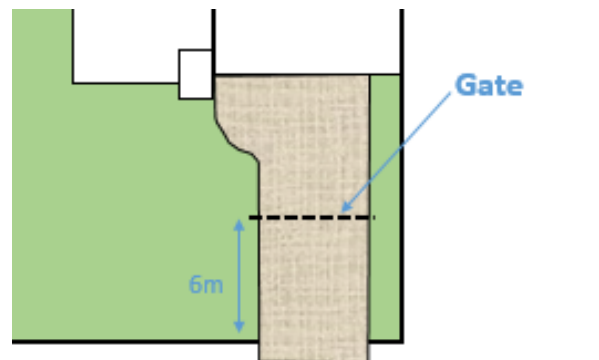
PO 2.1

Sufficient accessible on-site queuing adjacent to access points is provided to meet the needs of development so that all vehicle queues can be contained fully within the boundaries of the development site, to minimise interruption on the functional performance of the road and maintain safe vehicle movements.

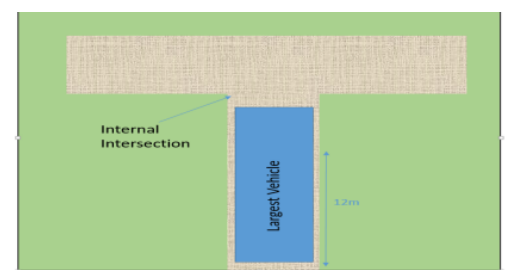
DTS/DPF 2.1

An access point in accordance with one of the following:

- (a) will not service, or is not intended to service, more than 6 dwellings and there are no internal driveways, intersections, car parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site) as shown in the following diagram:



- (b) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
 - (i) is expected to be serviced by vehicles with a length no greater than 6.4m
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
- (c) will service, or is intended to service, development that will generate less than 60 vehicle movements per day, and:
 - (i) is expected to be serviced by vehicles with a length greater than a 6.4m small rigid vehicle
 - (ii) there are no internal driveways, intersections, parking spaces or gates within 6.0m of the access point (measured from the site boundary into the site)
 - (iii) any termination of or change in priority of movement within the main car park aisle is located far enough into the site so that the largest vehicle expected on-site can store fully within the site before being required to stop
 - (iv) all parking or manoeuvring areas for commercial vehicles are located a minimum of 12m or the length of the longest vehicle expected on site from the access (measured from the site boundary into the site) as shown in the following diagram:



Access - (Location Spacing) - Existing Access Point

PO 3.1

Existing access points are designed to accommodate the type and volume of traffic likely to be generated by the development.

DTS/DPF 3.1

An existing access point satisfies (a), (b) or (c):

- (a) it will not service, or is not intended to service, more than 6 dwellings
- (b) it is not located on a Controlled Access Road and will not service development that will result in a larger class of vehicle expected to access the site using the existing access
- (c) is not located on a Controlled Access Road and development constitutes:
 - (i) a change of use between an office <500m² gross leasable floor area and a consulting room <500m² gross leasable floor area or vice versa
 - (ii) a change in use from a shop to an office, consulting room or personal or domestic services establishment
 - (iii) a change of use from a consulting room or office <250m² gross leasable floor area to shop <250m² gross leasable floor area
 - (iv) a change of use from a shop <500m² gross leasable floor area to a warehouse <500m² gross leasable floor area
 - (v) an office or consulting room with a <500m² gross leasable floor area
 - (vi) a change of use from a residential dwelling to a shop, office, consulting room or personal or domestic services establishment with <250m² gross leasable floor area.

Access - Location (Spacing) - New Access Points

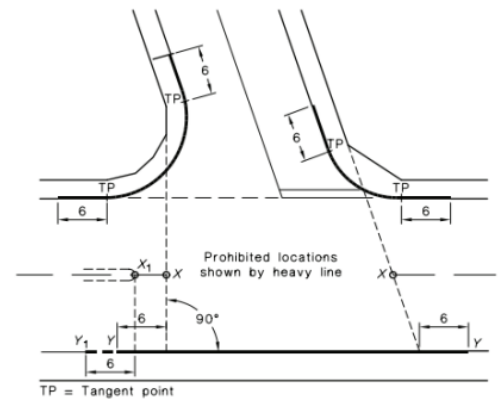
PO 4.1

New access points are spaced apart from any existing access point or public road junction to manage impediments to traffic flow and maintain safe and efficient operating conditions on the road.

DTS/DPF 4.1

A new access point satisfies (a), (b) or (c):

- (a) where a development site is intended to serve between 1 and 6 dwellings, access to the site is from the local road network (not being a Controlled Access Road) and is located outside of the bold lines shown in the following diagram:



NOTE:

The points marked X_1 and X are respectively at the median end on a divided road and at the intersection of the main road centre-line and the extensions of the side road property lines shown as dotted lines, on an undivided road. On a divided road, dimension Y-Y extends to Point Y_1 .

- (b) where the development site is intended to serve between 1 and 6 dwellings, the new access:
 - (i) is not located on a Controlled Access Road
 - (ii) is not located on a section of road affected by double barrier lines
 - (iii) will be on a road with a speed environment of 70km/h or less
 - (iv) is located outside of the bold lines on the diagram shown in the diagram following part (a)
 - (v) is located a minimum of 6m from a median opening or pedestrian crossing
- (c) where DTS/DPF 4.1 part (a) and (b) do not apply and access from an alternative local road at least 25m from the State Maintained Road is not available, and the access is not located on a Controlled Access Road, the new access is separated in accordance with the following:

Speed Limit	Separation between access points	Separation from public road junctions and merging/terminating lanes
50 km/h or less	No spacing requirement	20m
60 km/h	5m (for development intended to serve between 1 and 6 dwellings) and 10m for all other cases	73m
70 km/h	40m	92m
80 km/h	50m	114m
90 km/h	65m	139m
100 km/h	80m	165m
110 km/h	100m	193m

Access - Location (Sight Lines)

PO 5.1

Access points are located and designed to accommodate sight lines that enable drivers and pedestrians to navigate potential conflict points with roads in a controlled and safe manner.

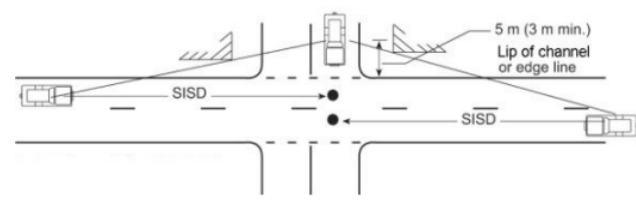
DTS/DPF 5.1

An access point satisfies (a) and (c) or (b) and (c):

(a) the development site does or is intended to serve between 1 and 6 dwellings and utilises an existing access point
or

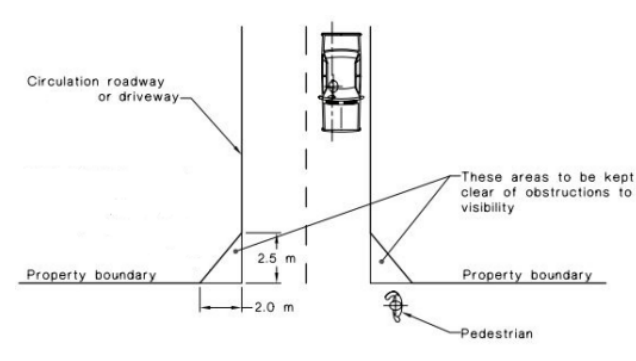
(b) drivers approaching or exiting an access point have an unobstructed line of sight in accordance with the following (measured at a height of 1.1m above the surface of the road):

Speed Limit	Access point serving 1-6 dwellings	Access point serving all other development
40 km/h or less	47m	73m
50 km/h	63m	97m
60 km/h	81m	123m
70 km/h	100m	151m
80 km/h	121m	181m
90 km/h	144m	226m
100 km/h	169m	282m
110km/h	195m	300m



and

(c) pedestrian sightlines in accordance with the following diagram:



Access – Mud and Debris

PO 6.1

Access points constructed to minimise mud or other debris being carried or transferred onto the road to ensure safe road operating conditions.

DTS/DPF 6.1

Where the road has an unsealed shoulder and the road is not kerbed, the access way is sealed from the edge of seal on the road for a minimum of 10m or to the property boundary (whichever is closer).

Access - Stormwater

PO 7.1

Access points are designed to minimise negative impact on roadside drainage of water.

DTS/DPF 7.1

Development does not:

(a) decrease the capacity of an existing drainage point
 (b) restrict or prevent the flow of stormwater through an existing drainage point and system
 (c) result in access points becoming stormwater flow paths directly onto the road.

Building on Road Reserve

PO 8.1

Buildings or structures that encroach onto, above or below road reserves are designed and sited to minimise impact on safe movements by all road users.

DTS/DPF 8.1

Buildings or structures are not located on, above or below the road reserve.

Public Road Junctions

<p>PO 9.1</p> <p>New junctions with a public road (including the opening of unmade public road junctions) or modifications to existing road junctions are located and designed to ensure safe operating conditions are maintained on the State Maintained Road.</p>	<p>DTS/DPF 9.1</p> <p>Development does not comprise any of the following:</p> <ul style="list-style-type: none"> (a) creating a new junction with a public road (b) opening an unmade public road junction (c) modifying an existing public road junction.
<p>Corner Cut-Offs</p>	
<p>PO 10.1</p> <p>Development is located and designed to maintain sightlines for drivers turning into and out of public road junctions to contribute to driver safety.</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram:</p> <div style="text-align: center; margin-top: 20px;"> </div>

Procedural Matters (PM) - Referrals

The following table identifies classes of development / activities that require referral in this Overlay and the applicable referral body. It sets out the purpose of the referral as well as the relevant statutory reference from Schedule 9 of the Planning, Development and Infrastructure (General) Regulations 2017.

Class of Development / Activity	Referral Body	Purpose of Referral	Statutory Reference
<p>Except where all of the relevant deemed-to-satisfy criteria are met, development (including the division of land) that involves any of the following to/on a State Maintained Road or within 25 metres of an intersection with any such road:</p> <ul style="list-style-type: none"> (a) creation of a new access or junction (b) alterations to an existing access or public road junction (except where deemed to be minor in the opinion of the relevant authority) (c) development that changes the nature of vehicular movements or increase the number or frequency of movements through an existing access (except where deemed to be minor in the opinion of the relevant authority). 	<p>Commissioner of Highways.</p>	<p>To provide expert technical assessment and direction to the Relevant Authority on the safe and efficient operation and management of all roads relevant to the Commissioner of Highways as described in the Planning and Design Code.</p>	<p>Development of a class to which Schedule 9 clause 3 item 7 of the Planning, Development and Infrastructure (General) Regulations 2017 applies.</p>

Part 4 - General Development Policies

Advertisements

Assessment Provisions (AP)


Desired Outcome (DO)

Desired Outcome

DO 1	Advertisements and advertising hoardings are appropriate to context, efficient and effective in communicating with the public, limited in number to avoid clutter, and do not create hazard.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<i>Appearance</i>	
<p>PO 1.1</p> <p>Advertisements are compatible and integrated with the design of the building and/or land they are located on.</p>	<p>DTS/DPF 1.1</p> <p>Advertisements attached to a building satisfy all of the following:</p> <ul style="list-style-type: none"> (a) are not located in a Neighbourhood-type zone (b) where they are flush with a wall: <ul style="list-style-type: none"> (i) if located at canopy level, are in the form of a fascia sign (ii) if located above canopy level: <ul style="list-style-type: none"> A. do not have any part rising above parapet height B. are not attached to the roof of the building (c) where they are not flush with a wall: <ul style="list-style-type: none"> (i) if attached to a verandah, no part of the advertisement protrudes beyond the outer limits of the verandah structure (ii) if attached to a two-storey building: <ul style="list-style-type: none"> A. has no part located above the finished floor level of the second storey of the building B. does not protrude beyond the outer limits of any verandah structure below C. does not have a sign face that exceeds 1m2 per side. (d) if located below canopy level, are flush with a wall (e) if located at canopy level, are in the form of a fascia sign (f) if located above a canopy: <ul style="list-style-type: none"> (i) are flush with a wall (ii) do not have any part rising above parapet height (iii) are not attached to the roof of the building. (g) if attached to a verandah, no part of the advertisement protrudes beyond the outer limits of the verandah structure (h) if attached to a two-storey building, have no part located above the finished floor level of the second storey of the building (i) where they are flush with a wall, do not, in combination with any other existing sign, cover more than 15% of the building facade to which they are attached.
<p>PO 1.2</p> <p>Advertising hoardings do not disfigure the appearance of the land upon which they are situated or the character of the locality.</p>	<p>DTS/DPF 1.2</p> <p>Where development comprises an advertising hoarding, the supporting structure is:</p> <ul style="list-style-type: none"> (a) concealed by the associated advertisement and decorative detailing or (b) not visible from an adjacent public street or thoroughfare, other than a support structure in the form of a single or dual post design.
<p>PO 1.3</p> <p>Advertising does not encroach on public land or the land of an adjacent allotment.</p>	<p>DTS/DPF 1.3</p> <p>Advertisements and/or advertising hoardings are contained within the boundaries of the site.</p>
<p>PO 1.4</p> <p>Where possible, advertisements on public land are integrated with existing structures and infrastructure.</p>	<p>DTS/DPF 1.4</p> <p>Advertisements on public land that meet at least one of the following:</p> <ul style="list-style-type: none"> (a) achieves Advertisements DTS/DPF 1.1 (b) are integrated with a bus shelter.
<p>PO 1.5</p> <p>Advertisements and/or advertising hoardings are of a scale and size appropriate to the character of the locality.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>

Proliferation of Advertisements	
<p>PO 2.1</p> <p>Proliferation of advertisements is minimised to avoid visual clutter and untidiness.</p>	<p>DTS/DPF 2.1</p> <p>No more than one freestanding advertisement is displayed per occupancy.</p>
<p>PO 2.2</p> <p>Multiple business or activity advertisements are co-located and coordinated to avoid visual clutter and untidiness.</p>	<p>DTS/DPF 2.2</p> <p>Advertising of a multiple business or activity complex is located on a single advertisement fixture or structure.</p>
<p>PO 2.3</p> <p>Proliferation of advertisements attached to buildings is minimised to avoid visual clutter and untidiness.</p>	<p>DTS/DPF 2.3</p> <p>Advertisements satisfy all of the following:</p> <ul style="list-style-type: none"> (a) are attached to a building (b) other than in a Neighbourhood-type zone, where they are flush with a wall, cover no more than 15% of the building facade to which they are attached (c) do not result in more than one sign per occupancy that is not flush with a wall.
Advertising Content	
<p>PO 3.1</p> <p>Advertisements are limited to information relating to the lawful use of land they are located on to assist in the ready identification of the activity or activities on the land and avoid unrelated content that contributes to visual clutter and untidiness.</p>	<p>DTS/DPF 3.1</p> <p>Advertisements contain information limited to a lawful existing or proposed activity or activities on the same site as the advertisement.</p>
Amenity Impacts	
<p>PO 4.1</p> <p>Light spill from advertisement illumination does not unreasonably compromise the amenity of sensitive receivers.</p>	<p>DTS/DPF 4.1</p> <p>Advertisements do not incorporate any illumination.</p>
Safety	
<p>PO 5.1</p> <p>Advertisements and/or advertising hoardings erected on a verandah or projecting from a building wall are designed and located to allow for safe and convenient pedestrian access.</p>	<p>DTS/DPF 5.1</p> <p>Advertisements have a minimum clearance of 2.5m between the top of the footpath and base of the underside of the sign.</p>
<p>PO 5.2</p> <p>Advertisements and/or advertising hoardings do not distract or create a hazard to drivers through excessive illumination.</p>	<p>DTS/DPF 5.2</p> <p>No advertisement illumination is proposed.</p>
<p>PO 5.3</p> <p>Advertisements and/or advertising hoardings do not create a hazard to drivers by:</p> <ul style="list-style-type: none"> (a) being liable to interpretation by drivers as an official traffic sign or signal (b) obscuring or impairing drivers' view of official traffic signs or signals (c) obscuring or impairing drivers' view of features of a road that are potentially hazardous (such as junctions, bends, changes in width and traffic control devices) or other road or rail vehicles at/or approaching level crossings. 	<p>DTS/DPF 5.3</p> <p>Advertisements satisfy all of the following:</p> <ul style="list-style-type: none"> (a) are not located in a public road or rail reserve (b) are located wholly outside the land shown as 'Corner Cut-Off Area' in the following diagram <div style="text-align: center; margin-top: 10px;">  <p>The diagram illustrates a 'Corner Cut-Off Area' at the intersection of a road and an allotment. A dashed line represents the 'Road Reserve'. A solid line represents the 'Allotment Boundary'. A shaded triangular area at the corner is labeled 'Corner Cut-Off Area'. Two dimensions of 4.5M are shown: one from the road centerline to the start of the cut-off area, and another from the start of the cut-off area to the allotment boundary.</p> </div>
<p>PO 5.4</p> <p>Advertisements and/or advertising hoardings do not create a hazard by distracting</p>	<p>DTS/DPF 5.4</p> <p>Advertisements and/or advertising hoardings are not located along or adjacent to a</p>

drivers from the primary driving task at a location where the demands on driver concentration are high.	road having a speed limit of 80km/h or more.
<p>PO 5.5</p> Advertisements and/or advertising hoardings provide sufficient clearance from the road carriageway to allow for safe and convenient movement by all road users.	<p>DTS/DPF 5.5</p> Where the advertisement or advertising hoarding is: <ul style="list-style-type: none"> (a) on a kerbed road with a speed zone of 60km/h or less, the advertisement or advertising hoarding is located at least 0.6m from the roadside edge of the kerb (b) on an unkerbed road with a speed zone of 60km/h or less, the advertisement or advertising hoarding is located at least 5.5m from the edge of the seal (c) on any other kerbed or unkerbed road, the advertisement or advertising hoarding is located a minimum of the following distance from the roadside edge of the kerb or the seal: <ul style="list-style-type: none"> (a) 110 km/h road - 14m (b) 100 km/h road - 13m (c) 90 km/h road - 10m (d) 70 or 80 km/h road - 8.5m.
<p>PO 5.6</p> Advertising near signalised intersections does not cause unreasonable distraction to road users through illumination, flashing lights, or moving or changing displays or messages.	<p>DTS/DPF 5.6</p> Advertising: <ul style="list-style-type: none"> (a) is not illuminated (b) does not incorporate a moving or changing display or message (c) does not incorporate a flashing light(s).

Animal Keeping and Horse Keeping

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Animals are kept at a density that is not beyond the carrying capacity of the land and in a manner that minimises their adverse effects on the environment, local amenity and surrounding development.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<i>Siting and Design</i>	
<p>PO 1.1</p> Animal keeping, horse keeping and associated activities do not create adverse impacts on the environment or the amenity of the locality.	<p>DTS/DPF 1.1</p> None are applicable.
<p>PO 1.2</p> Animal keeping and horse keeping is located and managed to minimise the potential transmission of disease to other operations where animals are kept.	<p>DTS/DPF 1.2</p> None are applicable.
<i>Horse Keeping</i>	
<p>PO 2.1</p> Water from stable wash-down areas is directed to appropriate absorption areas and/or drainage pits to minimise pollution of land and water.	<p>DTS/DPF 2.1</p> None are applicable.

<p>PO 2.2</p> <p>Stables, horse shelters or associated yards are sited appropriate distances away from sensitive receivers and/or allotments in other ownership to avoid adverse impacts from dust, erosion and odour.</p>	<p>DTS/DPF 2.2</p> <p>Stables, horse shelters and associated yards are sited in accordance with all of the following:</p> <p>(a) 30m or more from any sensitive receivers (existing or approved) on land in other ownership (b) where an adjacent allotment is vacant and in other ownership, 30m or more from the boundary of that allotment.</p>
<p>PO 2.3</p> <p>All areas accessible to horses are separated from septic tank effluent disposal areas to protect the integrity of that system. Stable flooring is constructed with an impervious material to facilitate regular cleaning.</p>	<p>DTS/DPF 2.3</p> <p>Septic tank effluent disposal areas are enclosed with a horse-proof barrier such as a fence to exclude horses from this area.</p>
<p>PO 2.4</p> <p>To minimise environmental harm and adverse impacts on water resources, stables, horse shelters and associated yards are appropriately set back from a watercourse.</p>	<p>DTS/DPF 2.4</p> <p>Stables, horse shelters and associated yards are set back 50m or more from a watercourse.</p>
<p>PO 2.5</p> <p>Stables, horse shelters and associated yards are located on slopes that are stable to minimise the risk of soil erosion and water runoff.</p>	<p>DTS/DPF 2.5</p> <p>Stables, horse shelters and associated yards are not located on land with a slope greater than 10% (1-in-10).</p>
<p>Kennels</p>	
<p>PO 3.1</p> <p>Kennel flooring is constructed with an impervious material to facilitate regular cleaning.</p>	<p>DTS/DPF 3.1</p> <p>The floors of kennels satisfy all of the following:</p> <p>(a) are constructed of impervious concrete (b) are designed to be self-draining when washed down.</p>
<p>PO 3.2</p> <p>Kennels and exercise yards are designed and sited to minimise noise nuisance to neighbours through measures such as:</p> <p>(a) adopting appropriate separation distances (b) orientating openings away from sensitive receivers.</p>	<p>DTS/DPF 3.2</p> <p>Kennels are sited 500m or more from the nearest sensitive receiver on land in other ownership.</p>
<p>PO 3.3</p> <p>Dogs are regularly observed and managed to minimise nuisance impact on adjoining sensitive receivers from animal behaviour.</p>	<p>DTS/DPF 3.3</p> <p>Kennels are sited in association with a permanent dwelling on the land.</p>
<p>Wastes</p>	
<p>PO 4.1</p> <p>Storage of manure, used litter and other wastes (other than wastewater lagoons) is designed, constructed and managed to minimise attracting and harbouring vermin.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Facilities for the storage of manure, used litter and other wastes (other than wastewater lagoons) are located to minimise the potential for polluting water resources.</p>	<p>DTS/DPF 4.2</p> <p>Waste storage facilities (other than wastewater lagoons) are located outside the 1% AEP flood event areas.</p>

Aquaculture

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Aquaculture facilities are developed in an ecologically, economically and socially sustainable manner to support an equitable sharing of marine, coastal and inland resources and mitigate conflict with other water-based and land-based uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land-based Aquaculture	
<p>PO 1.1</p> <p>Land-based aquaculture and associated components are sited and designed to mitigate adverse impacts on nearby sensitive receivers.</p>	<p>DTS/DPF 1.1</p> <p>Land-based aquaculture and associated components are located to satisfy all of the following:</p> <p>(a) 200m or more from a sensitive receiver in other ownership (b) 500m or more from the boundary of a zone primarily intended to accommodate sensitive receivers</p> <p>or</p> <p>The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i>.</p>
<p>PO 1.2</p> <p>Land-based aquaculture and associated components are sited and designed to prevent surface flows from entering ponds in a 1% AEP sea flood level event.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Land-based aquaculture and associated components are sited and designed to prevent pond leakage that would pollute groundwater.</p>	<p>DTS/DPF 1.3</p> <p>The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i>.</p>
<p>PO 1.4</p> <p>Land-based aquaculture and associated components are sited and designed to prevent farmed species escaping and entering into any waters.</p>	<p>DTS/DPF 1.4</p> <p>The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i>.</p>
<p>PO 1.5</p> <p>Land-based aquaculture and associated components, including intake and discharge pipes, are designed to minimise the need to traverse sensitive areas to minimise impact on the natural environment.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
<p>PO 1.6</p> <p>Pipe inlets and outlets associated with land-based aquaculture are sited and designed to minimise the risk of disease transmission.</p>	<p>DTS/DPF 1.6</p> <p>The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i>.</p>
<p>PO 1.7</p> <p>Storage areas associated with aquaculture activity are integrated with the use of the land and sited and designed to minimise their visual impact on the surrounding environment.</p>	<p>DTS/DPF 1.7</p> <p>None are applicable.</p>
Marine Based Aquaculture	
<p>PO 2.1</p>	<p>DTS/DPF 2.1</p>

<p>Marine aquaculture is sited and designed to minimise its adverse impacts on sensitive ecological areas including:</p> <p>(a) creeks and estuaries (b) wetlands (c) significant seagrass and mangrove communities (d) marine habitats and ecosystems.</p>	None are applicable.
<p>PO 2.2</p> <p>Marine aquaculture is sited in areas with adequate water current to disperse sediments and dissolve particulate wastes to prevent the build-up of waste that may cause environmental harm.</p>	<p>DTS/DPF 2.2</p> <p>The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i>.</p>
<p>PO 2.3</p> <p>Marine aquaculture is designed to not involve discharge of human waste on the site, on any adjacent land or into nearby waters.</p>	<p>DTS/DPF 2.3</p> <p>The development does not include toilet facilities located over water.</p>
<p>PO 2.4</p> <p>Marine aquaculture (other than inter-tidal aquaculture) is located an appropriate distance seaward of the high water mark.</p>	<p>DTS/DPF 2.4</p> <p>Marine aquaculture development is located 100m or more seaward of the high water mark</p> <p>or</p> <p>The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i>.</p>
<p>PO 2.5</p> <p>Marine aquaculture is sited and designed to not obstruct or interfere with:</p> <p>(a) areas of high public use (b) areas, including beaches, used for recreational activities such as swimming, fishing, skiing, sailing and other water sports (c) areas of outstanding visual or environmental value (d) areas of high tourism value (e) areas of important regional or state economic activity, including commercial ports, wharfs and jetties (f) the operation of infrastructure facilities including inlet and outlet pipes associated with the desalination of sea water.</p>	<p>DTS/DPF 2.5</p> <p>None are applicable.</p>
<p>PO 2.6</p> <p>Marine aquaculture is sited and designed to minimise interference and obstruction to the natural processes of the coastal and marine environment.</p>	<p>DTS/DPF 2.6</p> <p>None are applicable.</p>
<p>PO 2.7</p> <p>Marine aquaculture is designed to be as unobtrusive as practicable by incorporating measures such as:</p> <p>(a) using feed hoppers painted in subdued colours and suspending them as close as possible to the surface of the water (b) positioning structures to protrude the minimum distance practicable above the surface of the water (c) avoiding the use of shelters and structures above cages and platforms unless necessary to exclude predators and protected species from interacting with the farming structures and/or stock inside the cages, or for safety reasons (d) positioning racks, floats and other farm structures in unobtrusive locations landward from the shoreline.</p>	<p>DTS/DPF 2.7</p> <p>None are applicable.</p>
<p>PO 2.8</p> <p>Access, launching and maintenance facilities utilise existing established roads, tracks, ramps and paths to or from the sea where possible to minimise environmental and amenity impacts.</p>	<p>DTS/DPF 2.8</p> <p>The development utilises existing established roads, tracks, ramps and/or paths (as applicable) to access the sea.</p>
<p>PO 2.9</p> <p>Access, launching and maintenance facilities are developed as common user</p>	<p>DTS/DPF 2.9</p> <p>The development utilises existing established roads, tracks, ramps and/or paths (as</p>

facilities and are co-located where practicable to mitigate adverse impacts on coastal areas.	applicable) to access the sea.
PO 2.10 Marine aquaculture is sited to minimise potential impacts on, and to protect the integrity of, reserves under the <i>National Parks and Wildlife Act 1972</i> .	DTS/DPF 2.10 Marine aquaculture is located 1000m or more seaward of the boundary of any reserve under the <i>National Parks and Wildlife Act 1972</i> .
PO 2.11 Onshore storage, cooling and processing facilities do not impair the coastline and its visual amenity by: (a) being sited, designed, landscaped and of a scale to reduce the overall bulk and appearance of buildings and complement the coastal landscape (b) making provision for appropriately sited and designed vehicular access arrangements, including using existing vehicular access arrangements as far as practicable (c) incorporating appropriate waste treatment and disposal.	DTS/DPF 2.11 The development does not include any onshore facilities in conjunction with a proposal for marine aquaculture.
Navigation and Safety	
PO 3.1 Marine aquaculture sites are suitably marked to maintain navigational safety.	DTS/DPF 3.1 The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i> .
PO 3.2 Marine aquaculture is sited to provide adequate separation between farms for safe navigation.	DTS/DPF 3.2 The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i> .
Environmental Management	
PO 4.1 Marine aquaculture is maintained to prevent hazards to people and wildlife, including breeding grounds and habitats of native marine mammals and terrestrial fauna, especially migratory species.	DTS/DPF 4.1 None are applicable.
PO 4.2 Marine aquaculture is designed to facilitate the relocation or removal of structures in the case of emergency such as oil spills, algal blooms and altered water flows.	DTS/DPF 4.2 None are applicable.
PO 4.3 Marine aquaculture provides for progressive or future reclamation of disturbed areas ahead of, or upon, decommissioning.	DTS/DPF 4.3 None are applicable.
PO 4.4 Aquaculture operations incorporate measures for the removal and disposal of litter, disused material, shells, debris, detritus, dead animals and animal waste to prevent pollution of waters, wetlands, or the nearby coastline.	DTS/DPF 4.4 The development is the subject of an aquaculture lease and/or licence (as applicable) granted under the <i>Aquaculture Act 2001</i> .

Beverage Production in Rural Areas

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Mitigation of potential amenity and environmental impacts of value-adding beverage production facilities such as wineries, distilleries, cideries and breweries.

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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Odour and Noise	
PO 1.1 Beverage production activities are designed and sited to minimise odour impacts on rural amenity.	DTS/DPF 1.1 None are applicable.
PO 1.2 Beverage production activities are designed and sited to minimise noise impacts on sensitive receivers.	DTS/DPF 1.2 None are applicable.
PO 1.3 Fermentation, distillation, manufacturing, storage, packaging and bottling activities occur within enclosed buildings to improve the visual appearance within a locality and manage noise associated with these activities.	DTS/DPF 1.3 None are applicable.
PO 1.4 Breweries are designed to minimise odours emitted during boiling and fermentation stages of production.	DTS/DPF 1.4 Brew kettles are fitted with a vapour condenser.
PO 1.5 Beverage production solid wastes are stored in a manner that minimises odour impacts on sensitive receivers in other ownership.	DTS/DPF 1.5 Solid waste from beverage production is collected and stored in sealed containers and removed from the site within 48 hours.
Water Quality	
PO 2.1 Beverage production wastewater management systems (including wastewater irrigation) are set back from watercourses to minimise adverse impacts on water resources.	DTS/DPF 2.1 Wastewater management systems are set back 50m or more from the banks of watercourses and bores.
PO 2.2 The storage or disposal of chemicals or hazardous substances is undertaken in a manner to prevent pollution of water resources.	DTS/DPF 2.2 None are applicable.
PO 2.3 Stormwater runoff from areas that may cause contamination due to beverage production activities (including vehicle movements and machinery operations) is drained to an onsite stormwater treatment system to manage potential environmental impacts.	DTS/DPF 2.3 None are applicable.
PO 2.4 Stormwater runoff from areas unlikely to cause contamination by beverage production and associated activities (such as roof catchments and clean hard-paved	DTS/DPF 2.4 None are applicable.

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surfaces) is diverted away from beverage production areas and wastewater management systems.		
Wastewater Irrigation		
PO 3.1 Beverage production wastewater irrigation systems are designed and located to not contaminate soil and surface and ground water resources or damage crops.	DTS/DPF 3.1 None are applicable.	
PO 3.2 Beverage production wastewater irrigation systems are designed and located to minimise impact on amenity and avoid spray drift onto adjoining land.	DTS/DPF 3.2 Beverage production wastewater is not irrigated within 50m of any dwelling in other ownership.	
PO 3.3 Beverage production wastewater is not irrigated onto areas that pose an undue risk to the environment or amenity such as: (a) waterlogged areas (b) land within 50m of a creek, swamp or domestic or stock water bore (c) land subject to flooding (d) steeply sloping land (e) rocky or highly permeable soil overlaying an unconfined aquifer.	DTS/DPF 3.3 None are applicable.	

Bulk Handling and Storage Facilities

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Facilities for the bulk handling and storage of agricultural, mineral, petroleum, rock, ore or other similar commodities are designed to minimise adverse impacts on transport networks, the landscape and surrounding land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
PO 1.1 Bulk handling and storage facilities are sited and designed to minimise risks of adverse air quality and noise impacts on sensitive receivers.	DTS/DPF 1.1 Facilities for the handling, storage and dispatch of commodities in bulk (excluding processing) meet the following minimum separation distances from sensitive receivers: (a) bulk handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals), where the handling of these materials into or from vessels does not exceed 100 tonnes per day: 300m or more from residential premises not associated with the facility (b) bulk handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility: 300m or more from residential premises not associated with the facility (c) bulk petroleum storage involving individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1,000 cubic metres: 500m or more (d) coal handling with: a. capacity up to 1 tonne per day or a storage capacity up to 50 tonnes: 500m or more b. capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes: 1000m or more.

Buffers and Landscaping	
<p>PO 2.1</p> <p>Bulk handling and storage facilities incorporate a buffer area for the establishment of dense landscaping adjacent road frontages to enhance the appearance of land and buildings from public thoroughfares.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Bulk handling and storage facilities incorporate landscaping to assist with screening and dust filtration.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
Access and Parking	
<p>PO 3.1</p> <p>Roadways and vehicle parking areas associated with bulk handling and storage facilities are designed and surfaced to control dust emissions and prevent drag out of material from the site.</p>	<p>DTS/DPF 3.1</p> <p>Roadways and vehicle parking areas are sealed with an all-weather surface.</p>
Slipways, Wharves and Pontoons	
<p>PO 4.1</p> <p>Slipways, wharves and pontoons used for the handling of bulk materials (such as fuel, oil, catch, bait and the like) incorporate catchment devices to avoid the release of materials into adjacent waters.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>

Clearance from Overhead Powerlines

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Protection of human health and safety when undertaking development in the vicinity of overhead transmission powerlines.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Buildings are adequately separated from aboveground powerlines to minimise potential hazard to people and property.</p>	<p>DTS/DPF 1.1</p> <p>One of the following is satisfied:</p> <p>(a) a declaration is provided by or on behalf of the applicant to the effect that the proposal would not be contrary to the regulations prescribed for the purposes of section 86 of the <i>Electricity Act 1996</i></p> <p>(b) there are no aboveground powerlines adjoining the site that are the subject of the proposed development.</p>

Design

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	<p>Development is:</p> <ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributes to the character of the immediate area (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access, and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All development	
External Appearance	
<p>PO 1.1</p> <p>Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Where zero or minor setbacks are desirable, development provides shelter over footpaths (in the form of verandahs, awnings, canopies and the like, with adequate lighting) to positively contribute to the walkability, comfort and safety of the public realm.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>Plant, exhaust and intake vents and other technical equipment is integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by:</p> <ul style="list-style-type: none"> (a) positioning plant and equipment in unobtrusive locations viewed from public roads and spaces (b) screening rooftop plant and equipment from view (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses. 	<p>DTS/DPF 1.4</p> <p>Development does not incorporate any structures that protrude beyond the roofline.</p>
<p>PO 1.5</p> <p>The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form) taking into account the form of development contemplated in the relevant zone.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
Safety	
<p>PO 2.1</p> <p>Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>

<p>PO 2.2</p> <p>Development is designed to differentiate public, communal and private areas.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.</p>	<p>DTS/DPF 2.4</p> <p>None are applicable.</p>
<p>PO 2.5</p> <p>Common areas and entry points of buildings (such as the foyer areas of residential buildings), and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.</p>	<p>DTS/DPF 2.5</p> <p>None are applicable.</p>
<p>Landscaping</p>	
<p>PO 3.1</p> <p>Soft landscaping and tree planting is incorporated to:</p> <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) maximise shade and shelter (c) maximise stormwater infiltration (d) enhance the appearance of land and streetscapes (e) contribute to biodiversity. 	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Soft landscaping and tree planting maximises the use of locally indigenous plant species, incorporates plant species best suited to current and future climate conditions and avoids pest plant and weed species.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
<p>Environmental Performance</p>	
<p>PO 4.1</p> <p>Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Buildings incorporate climate-responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.</p>	<p>DTS/DPF 4.3</p> <p>None are applicable.</p>
<p>Water Sensitive Design</p>	
<p>PO 5.1</p> <p>Development is sited and designed to maintain natural hydrological systems without negatively impacting:</p> <ul style="list-style-type: none"> (a) the quantity and quality of surface water and groundwater (b) the depth and directional flow of surface water and groundwater 	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>

<p>(c) the quality and function of natural springs.</p>	
On-site Waste Treatment Systems	
<p>PO 6.1</p> <p>Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.</p>	<p>DTS/DPF 6.1</p> <p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> (a) encroach within an area used as private open space or result in less private open space than that specified in Design Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.
Carparking Appearance	
<p>PO 7.1</p> <p>Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on the streetscapes through techniques such as:</p> <ul style="list-style-type: none"> (a) limiting protrusion above finished ground level (b) screening through appropriate planting, fencing and mounding (c) limiting the width of openings and integrating them into the building structure. 	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
<p>PO 7.2</p> <p>Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.</p>	<p>DTS/DPF 7.2</p> <p>None are applicable.</p>
<p>PO 7.3</p> <p>Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.</p>	<p>DTS/DPF 7.3</p> <p>None are applicable.</p>
<p>PO 7.4</p> <p>Street level vehicle parking areas incorporate tree planting to provide shade and reduce solar heat absorption and reflection.</p>	<p>DTS/DPF 7.4</p> <p>None are applicable.</p>
<p>PO 7.5</p> <p>Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.</p>	<p>DTS/DPF 7.5</p> <p>None are applicable.</p>
<p>PO 7.6</p> <p>Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.</p>	<p>DTS/DPF 7.6</p> <p>None are applicable.</p>
<p>PO 7.7</p> <p>Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.</p>	<p>DTS/DPF 7.7</p> <p>None are applicable.</p>
Earthworks and sloping land	
<p>PO 8.1</p> <p>Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.</p>	<p>DTS/DPF 8.1</p> <p>Development does not involve any of the following:</p> <ul style="list-style-type: none"> (a) excavation exceeding a vertical height of 1m (b) filling exceeding a vertical height of 1m

	(c) a total combined excavation and filling vertical height of 2m or more.
<p>PO 8.2</p> <p>Driveways and access tracks are designed and constructed to allow safe and convenient access on sloping land (with a gradient exceeding 1 in 8).</p>	<p>DTS/DPF 8.2</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):</p> <p>(a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway</p> <p>(b) are constructed with an all-weather trafficable surface.</p>
<p>PO 8.3</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):</p> <p>(a) do not contribute to the instability of embankments and cuttings</p> <p>(b) provide level transition areas for the safe movement of people and goods to and from the development</p> <p>(c) are designed to integrate with the natural topography of the land.</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>
<p>PO 8.4</p> <p>Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on-site drainage systems to minimise erosion.</p>	<p>DTS/DPF 8.4</p> <p>None are applicable.</p>
<p>PO 8.5</p> <p>Development does not occur on land at risk of landslip nor increases the potential for landslip or land surface instability.</p>	<p>DTS/DPF 8.5</p> <p>None are applicable.</p>
Fences and Walls	
<p>PO 9.1</p> <p>Fences, walls and retaining walls are of sufficient height to maintain privacy and security without unreasonably impacting the visual amenity and adjoining land's access to sunlight or the amenity of public places.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>
<p>PO 9.2</p> <p>Landscaping incorporated on the low side of retaining walls is visible from public roads and public open space to minimise visual impacts.</p>	<p>DTS/DPF 9.2</p> <p>A vegetated landscaped strip 1m wide or more is provided against the low side of a retaining wall.</p>
Overlooking / Visual Privacy (in building 3 storeys or less)	
<p>PO 10.1</p> <p>Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses.</p>	<p>DTS/DPF 10.1</p> <p>Upper level windows facing side or rear boundaries shared with a residential allotment/site satisfy one of the following:</p> <p>(a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 200mm</p> <p>(b) have sill heights greater than or equal to 1.5m above finished floor level</p> <p>(c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.</p>
<p>PO 10.2</p> <p>Development mitigates direct overlooking from balconies, terraces and decks to habitable rooms and private open space of adjoining residential uses.</p>	<p>DTS/DPF 10.2</p> <p>One of the following is satisfied:</p> <p>(a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace</p> <p>or</p> <p>(b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of:</p> <p>(i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</p> <p>or</p> <p>(ii) 1.7m above finished floor level in all other cases</p>

All Residential development			
Front elevations and passive surveillance			
<p>PO 11.1</p> <p>Dwellings incorporate windows along primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 11.1</p> <p>Each dwelling with a frontage to a public street:</p> <p>(a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m</p> <p>(b) has an aggregate window area of at least 2m² facing the primary street.</p>		
<p>PO 11.2</p> <p>Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.</p>	<p>DTS/DPF 11.2</p> <p>Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.</p>		
Outlook and amenity			
<p>PO 12.1</p> <p>Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 12.1</p> <p>A living room of a dwelling incorporates a window with an outlook towards the street frontage or private open space, public open space, or waterfront areas.</p>		
<p>PO 12.2</p> <p>Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.</p>	<p>DTS/DPF 12.2</p> <p>None are applicable.</p>		
Ancillary Development			
<p>PO 13.1</p> <p>Residential ancillary buildings and structures are sited and designed to not detract from the streetscape or appearance of buildings on the site or neighbouring properties.</p>	<p>DTS/DPF 13.1</p> <p>Ancillary buildings:</p> <p>(a) are ancillary to a dwelling erected on the same site</p> <p>(b) have a floor area not exceeding 60m²</p> <p>(c) are not constructed, added to or altered so that any part is situated:</p> <p style="margin-left: 20px;">(i) in front of any part of the building line of the dwelling to which it is ancillary or</p> <p style="margin-left: 20px;">(ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads)</p> <p>(d) in the case of a garage or carport, the garage or carport:</p> <p style="margin-left: 20px;">(i) is set back at least 5.5m from the boundary of the primary street</p> <p style="margin-left: 20px;">(ii) when facing a primary street or secondary street, has a total door / opening not exceeding:</p> <p style="margin-left: 40px;">A. for dwellings of single building level - 7m in width or 50% of the site frontage, whichever is the lesser</p> <p style="margin-left: 40px;">B. for dwellings comprising two or more building levels at the building line fronting the same public street - 7m in width</p> <p>(e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless:</p> <p style="margin-left: 20px;">(i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and</p> <p style="margin-left: 20px;">(ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent</p> <p>(f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary</p> <p>(g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure</p> <p>(h) have a wall height or post height not exceeding 3m above natural ground level (and not including a gable end)</p> <p>(i) have a roof height where no part of the roof is more than 5m above the natural ground level</p> <p>(j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour</p> <p>(k) retains a total area of soft landscaping in accordance with (i) or (ii), whichever is less:</p> <p style="margin-left: 20px;">(i) a total area as determined by the following table:</p>		
<table border="1" style="background-color: #003366; color: white; width: 200px; height: 40px; margin-left: auto;"> <tr> <td style="padding: 5px;">Dwelling site</td> <td style="padding: 5px;">Minimum</td> </tr> </table>		Dwelling site	Minimum
Dwelling site	Minimum		

	<p>area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <tr> <td style="width: 70%; text-align: center;"><150</td> <td style="width: 30%; text-align: center;">10%</td> </tr> <tr> <td style="text-align: center;">150-200</td> <td style="text-align: center;">15%</td> </tr> <tr> <td style="text-align: center;">201-450</td> <td style="text-align: center;">20%</td> </tr> <tr> <td style="text-align: center;">>450</td> <td style="text-align: center;">25%</td> </tr> </table>	<150	10%	150-200	15%	201-450	20%	>450	25%
<150	10%								
150-200	15%								
201-450	20%								
>450	25%								
<p>PO 13.2</p> <p>Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision or car parking requirements and do not result in over-development of the site.</p>	<p>DTS/DPF 13.2</p> <p>Ancillary buildings and structures do not result in:</p> <ul style="list-style-type: none"> (a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space (b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas. <p>(ii) the amount of existing soft landscaping prior to the development occurring.</p>								
<p>PO 13.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa is positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers.</p>	<p>DTS/DPF 13.3</p> <p>The pump and/or filtration system is ancillary to a dwelling erected on the same site and is:</p> <ul style="list-style-type: none"> (a) enclosed in a solid acoustic structure that is located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment. 								
<p>PO 13.4</p> <p>Buildings and structures that are ancillary to an existing non-residential use do not detract from the streetscape character, appearance of buildings on the site of the development, or the amenity of neighbouring properties.</p>	<p>DTS/DPF 13.4</p> <p>Non-residential ancillary buildings and structures:</p> <ul style="list-style-type: none"> (a) are ancillary and subordinate to an existing non-residential use on the same site (b) have a floor area not exceeding the following: <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 5px;"> <thead> <tr> <th style="width: 50%;">Allotment size</th> <th style="width: 50%;">Floor area</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">≤500m²</td> <td style="text-align: center;">60m²</td> </tr> <tr> <td style="text-align: center;">>500m²</td> <td style="text-align: center;">80m²</td> </tr> </tbody> </table> (c) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> (i) in front of any part of the building line of the main building to which it is ancillary or (ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (d) in the case of a garage or carport, the garage or carport: <ul style="list-style-type: none"> (i) is set back at least 5.5m from the boundary of the primary street (e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless: <ul style="list-style-type: none"> (i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary (ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent (f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary (g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure 	Allotment size	Floor area	≤500m ²	60m ²	>500m ²	80m ²		
Allotment size	Floor area								
≤500m ²	60m ²								
>500m ²	80m ²								

	<ul style="list-style-type: none"> (h) have a wall height (or post height) not exceeding 3m (and not including a gable end) (i) have a roof height where no part of the roof is more than 5m above the natural ground level (j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour.
<small>Garage appearance</small>	
<p>PO 14.1</p> <p>Garaging is designed to not detract from the streetscape or appearance of a dwelling.</p>	<p>DTS/DPF 14.1</p> <p>Garages and carports facing a street:</p> <ul style="list-style-type: none"> (a) are situated so that no part of the garage or carport is in front of any part of the building line of the dwelling (b) are set back at least 5.5m from the boundary of the primary street (c) have a garage door / opening not exceeding 7m in width (d) have a garage door / opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.
<small>Massing</small>	
<p>PO 15.1</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 15.1</p> <p>None are applicable</p>
<small>Dwelling additions</small>	
<p>PO 16.1</p> <p>Dwelling additions are sited and designed to not detract from the streetscape or amenity of adjoining properties and do not impede on-site functional requirements.</p>	<p>DTS / DPF 16.1</p> <p>Dwelling additions:</p> <ul style="list-style-type: none"> (a) are not constructed, added to or altered so that any part is situated closer to a public street (b) do not result in: <ul style="list-style-type: none"> (i) excavation exceeding a vertical height of 1m (ii) filling exceeding a vertical height of 1m (iii) a total combined excavation and filling vertical height of 2m or more (iv) less Private Open Space than specified in Design Table 1 - Private Open Space (v) less on-site parking than specified in Transport Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas (vi) upper level windows facing side or rear boundaries unless: <ul style="list-style-type: none"> A. they are permanently obscured to a height of 1.5m above finished floor level that is fixed or not capable of being opened more than 200mm or B. have sill heights greater than or equal to 1.5m above finished floor level or C. incorporate screening to a height of 1.5m above finished floor level (vii) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: <ul style="list-style-type: none"> A. 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land B. 1.7m above finished floor level in all other cases.
<small>Private Open Space</small>	
<p>PO 17.1</p> <p>Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.</p>	<p>DTS/DPF 17.1</p> <p>Private open space is provided in accordance with Design Table 1 - Private Open Space.</p>
<small>Water Sensitive Design</small>	
<p>PO 18.1</p> <p>Residential development creating a common driveway / access includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.</p>	<p>DTS/DPF 18.1</p> <p>Residential development creating a common driveway / access that services 5 or more dwellings achieves the following stormwater runoff outcomes:</p> <ul style="list-style-type: none"> (a) 80 per cent reduction in average annual total suspended solids (b) 60 per cent reduction in average annual total phosphorus (c) 45 per cent reduction in average annual total nitrogen.
<p>PO 18.2</p>	<p>DTS/DPF 18.2</p>

Residential development creating a common driveway / access includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	Development creating a common driveway / access that services 5 or more dwellings: <ul style="list-style-type: none"> (a) maintains the pre-development peak flow rate from the site based upon a 0.35 runoff coefficient for the 18.1% AEP 30-minute storm and the stormwater runoff time to peak is not increased or captures and retains the difference in pre-development runoff volume (based upon a 0.35 runoff coefficient) vs post development runoff volume from the site for an 18.1% AEP 30-minute storm; and (b) manages site generated stormwater runoff up to and including the 1% AEP flood event to avoid flooding of buildings.
Car parking, access and manoeuvrability	
<p>PO 19.1</p> <p>Enclosed parking spaces are of a size and dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 19.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single width car parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m per space (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double width car parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.4m (iii) minimum garage door width of 2.4m per space.
<p>PO 19.2</p> <p>Uncovered parking spaces are of a size and dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 19.2</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m
<p>PO 19.3</p> <p>Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, pedestrian movement, domestic waste collection, landscaped street frontages and on-street parking.</p>	<p>DTS/DPF 19.3</p> <p>Driveways and access points on sites with a frontage to a public road of 10m or less have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site.</p>
<p>PO 19.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 19.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed: <ul style="list-style-type: none"> (i) is set back 6m or more from the tangent point of an intersection of 2 or more roads (ii) is set back outside of the marked lines or infrastructure dedicating a pedestrian crossing (iii) does not involve the removal, relocation or damage to of mature street trees, street furniture or utility infrastructure services.
<p>PO 19.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 19.5</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient of the driveway does not exceed a grade of 1 in 4 and includes transitions to ensure a maximum grade change of 12.5% (1 in 8) for summit changes, and 15% (1 in 6.7) for sag changes, in accordance with AS 2890.1:2004 to prevent vehicles bottoming or scraping (b) the centreline of the driveway has an angle of no less than 70 degrees and no more than 110 degrees from the street boundary to which it takes its access as shown in the following diagram:

	Number of bedrooms	Minimum internal floor area
	Studio	35m ²
	1 bedroom	50m ²
	2 bedroom	65m ²
	3+ bedrooms	80m ² and any dwelling over 3 bedrooms provides an additional 15m ² for every additional bedroom
<p>PO 22.2</p> <p>The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.</p>	<p>DTS/DPF 22.2</p> <p>None are applicable.</p>	
<p>PO 22.3</p> <p>Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.</p>	<p>DTS/DPF 22.3</p> <p>None are applicable.</p>	
<p>PO 22.4</p> <p>Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.</p>	<p>DTS/DPF 22.4</p> <p>Dwelling sites/allotments are not in the form of a battle-axe arrangement.</p>	
Communal Open Space		
<p>PO 23.1</p> <p>Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.</p>	<p>DTS/DPF 23.1</p> <p>None are applicable.</p>	
<p>PO 23.2</p> <p>Communal open space is of sufficient size and dimensions to cater for group recreation.</p>	<p>DTS/DPF 23.2</p> <p>Communal open space incorporates a minimum dimension of 5 metres.</p>	
<p>PO 23.3</p> <p>Communal open space is designed and sited to:</p> <p>(a) be conveniently accessed by the dwellings which it services</p> <p>(b) have regard to acoustic, safety, security and wind effects.</p>	<p>DTS/DPF 23.3</p> <p>None are applicable.</p>	
<p>PO 23.4</p> <p>Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.</p>	<p>DTS/DPF 23.4</p> <p>None are applicable.</p>	
<p>PO 23.5</p> <p>Communal open space is designed and sited to:</p> <p>(a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings</p> <p>(b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.</p>	<p>DTS/DPF 23.5</p> <p>None are applicable.</p>	

Carparking, access and manoeuvrability	
<p>PO 24.1</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.</p>	<p>DTS/DPF 24.1</p> <p>Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements:</p> <ul style="list-style-type: none"> (a) minimum 0.33 on-street car parks per proposed dwellings (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
<p>PO 24.2</p> <p>The number of vehicular access points onto public roads is minimised to reduce interruption of the footpath and positively contribute to public safety and walkability.</p>	<p>DTS/DPF 24.2</p> <p>Access to group dwellings or dwellings within a residential flat building is provided via a single common driveway.</p>
<p>PO 24.3</p> <p>Residential driveways that service more than one dwelling are designed to allow safe and convenient movement.</p>	<p>DTS/DPF 24.3</p> <p>Driveways that service more than 1 dwelling or a dwelling on a battle-axe site:</p> <ul style="list-style-type: none"> (a) have a minimum width of 3m (b) for driveways servicing more than 3 dwellings: <ul style="list-style-type: none"> (i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street (ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.
<p>PO 24.4</p> <p>Residential driveways in a battle-axe configuration are designed to allow safe and convenient movement.</p>	<p>DTS/DPF 24.4</p> <p>Where in a battle-axe configuration, a driveway servicing one dwelling has a minimum width of 3m.</p>
<p>PO 24.5</p> <p>Residential driveways that service more than one dwelling are designed to allow passenger vehicles to enter and exit the site and manoeuvre within the site in a safe and convenient manner.</p>	<p>DTS/DPF 24.5</p> <p>Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.</p>
<p>PO 24.6</p> <p>Dwellings are adequately separated from common driveways and manoeuvring areas.</p>	<p>DTS/DPF 24.6</p> <p>Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.</p>
Soft Landscaping	
<p>PO 25.1</p> <p>Soft landscaping is provided between dwellings and common driveways to improve the outlook for occupants and appearance of common areas.</p>	<p>DTS/DPF 25.1</p> <p>Other than where located directly in front of a garage or a building entry, soft landscaping with a minimum dimension of 1m is provided between a dwelling and common driveway.</p>
<p>PO 25.2</p> <p>Soft landscaping is provided that improves the appearance of common driveways.</p>	<p>DTS/DPF 25.2</p> <p>Where a common driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).</p>
Site Facilities / Waste Storage	
<p>PO 26.1</p> <p>Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.</p>	<p>DTS/DPF 26.1</p> <p>None are applicable.</p>

<p>PO 26.2</p> <p>Provision is made for suitable external clothes drying facilities.</p>	<p>DTS/DPF 26.2</p> <p>None are applicable.</p>
<p>PO 26.3</p> <p>Provision is made for suitable household waste and recyclable material storage facilities which are:</p> <p>(a) located away, or screened, from public view, and</p> <p>(b) conveniently located in proximity to dwellings and the waste collection point.</p>	<p>DTS/DPF 26.3</p> <p>None are applicable.</p>
<p>PO 26.4</p> <p>Waste and recyclable material storage areas are located away from dwellings.</p>	<p>DTS/DPF 26.4</p> <p>Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.</p>
<p>PO 26.5</p> <p>Where waste bins cannot be conveniently collected from the street, provision is made for on-site waste collection, designed to accommodate the safe and convenient access, egress and movement of waste collection vehicles.</p>	<p>DTS/DPF 26.5</p> <p>None are applicable.</p>
<p>PO 26.6</p> <p>Services including gas and water meters are conveniently located and screened from public view.</p>	<p>DTS/DPF 26.6</p> <p>None are applicable.</p>
Supported accommodation and retirement facilities	
Siting and Configuration	
<p>PO 27.1</p> <p>Supported accommodation and housing for aged persons and people with disabilities is located where on-site movement of residents is not unduly restricted by the slope of the land.</p>	<p>DTS/DPF 27.1</p> <p>None are applicable.</p>
Movement and Access	
<p>PO 28.1</p> <p>Development is designed to support safe and convenient access and movement for residents by providing:</p> <p>(a) ground-level access or lifted access to all units</p> <p>(b) level entry porches, ramps, paths, driveways, passenger loading areas and areas adjacent to footpaths that allow for the passing of wheelchairs and resting places</p> <p>(c) car parks with gradients no steeper than 1-in-40 and of sufficient area to provide for wheelchair manoeuvrability</p> <p>(d) kerb ramps at pedestrian crossing points.</p>	<p>DTS/DPF 28.1</p> <p>None are applicable.</p>
Communal Open Space	
<p>PO 29.1</p> <p>Development is designed to provide attractive, convenient and comfortable indoor and outdoor communal areas to be used by residents and visitors.</p>	<p>DTS/DPF 29.1</p> <p>None are applicable.</p>
<p>PO 29.2</p> <p>Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.</p>	<p>DTS/DPF 29.2</p> <p>None are applicable.</p>
<p>PO 29.3</p> <p>Communal open space is of sufficient size and dimensions to cater for group</p>	<p>DTS/DPF 29.3</p> <p>Communal open space incorporates a minimum dimension of 5 metres.</p>

recreation.	
<p>PO 29.4</p> <p>Communal open space is designed and sited to:</p> <p>(a) be conveniently accessed by the dwellings which it services</p> <p>(b) have regard to acoustic, safety, security and wind effects.</p>	<p>DTS/DPF 29.4</p> <p>None are applicable.</p>
<p>PO 29.5</p> <p>Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.</p>	<p>DTS/DPF 29.5</p> <p>None are applicable.</p>
<p>PO 29.6</p> <p>Communal open space is designed and sited to:</p> <p>(a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings</p> <p>(b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.</p>	<p>DTS/DPF 29.6</p> <p>None are applicable.</p>
Site Facilities / Waste Storage	
<p>PO 30.1</p> <p>Development is designed to provide storage areas for personal items and specialised equipment such as small electric powered vehicles, including facilities for the recharging of small electric powered vehicles.</p>	<p>DTS/DPF 30.1</p> <p>None are applicable.</p>
<p>PO 30.2</p> <p>Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.</p>	<p>DTS/DPF 30.2</p> <p>None are applicable.</p>
<p>PO 30.3</p> <p>Provision is made for suitable external clothes drying facilities.</p>	<p>DTS/DPF 30.3</p> <p>None are applicable.</p>
<p>PO 30.4</p> <p>Provision is made for suitable household waste and recyclable material storage facilities conveniently located and screened from public view.</p>	<p>DTS/DPF 30.4</p> <p>None are applicable.</p>
<p>PO 30.5</p> <p>Waste and recyclable material storage areas are located away from dwellings.</p>	<p>DTS/DPF 30.5</p> <p>Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.</p>
<p>PO 30.6</p> <p>Provision is made for on-site waste collection where 10 or more bins are to be collected at any one time.</p>	<p>DTS/DPF 30.6</p> <p>None are applicable.</p>
<p>PO 30.7</p> <p>Services including gas and water meters are conveniently located and screened from public view.</p>	<p>DTS/DPF 30.7</p> <p>None are applicable.</p>
All non-residential development	
Water Sensitive Design	
<p>PO 31.1</p>	<p>DTS/DPF 31.1</p>

Development likely to result in significant risk of export of litter, oil or grease includes stormwater management systems designed to minimise pollutants entering stormwater.	None are applicable.						
PO 31.2 Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.	DTS/DPF 31.2 None are applicable.						
Wash-down and Waste Loading and Unloading							
PO 32.1 Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, vessels, plant or equipment are: <ul style="list-style-type: none"> (a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off (b) paved with an impervious material to facilitate wastewater collection (c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area designed to drain wastewater to either: (d) <ul style="list-style-type: none"> (i) a treatment device such as a sediment trap and coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme or (ii) a holding tank and its subsequent removal off-site on a regular basis. 	DTS/DPF 32.1 None are applicable.						
Decks							
Design and Siting							
PO 33.1 Decks are designed and sited to: <ul style="list-style-type: none"> (a) complement the associated building form (b) minimise impacts on the streetscape through siting behind the building line of the principal building (unless on a significant allotment or open space) (c) minimise cut and fill and overall massing when viewed from adjacent land. 	DTS/DPF 33.1 Decks: <ul style="list-style-type: none"> (a) where ancillary to a dwelling: <ul style="list-style-type: none"> (i) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> A. in front of any part of the building line of the dwelling to which it is ancillary or B. within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (ii) are set back at least 900mm from side or rear allotment boundaries (iii) when attached to the dwelling, has a finished floor level consistent with the finished ground floor level of the dwelling (iv) where associated with a residential use, retains a total area of soft landscaping for the entire development site, including any common property, with a minimum dimension of 700mm in accordance with (A) or (B), whichever is less: <ul style="list-style-type: none"> A. a total area is determined by the following table: <table border="1" style="width: 100%; margin-top: 10px; border-collapse: collapse;"> <thead> <tr style="background-color: #1a3d54; color: white;"> <th style="padding: 5px;">Site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="padding: 5px;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px; text-align: center;"><150</td> <td style="padding: 5px; text-align: center;">10%</td> </tr> <tr> <td style="padding: 5px; text-align: center;">150-200</td> <td style="padding: 5px; text-align: center;">15%</td> </tr> </tbody> </table>	Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%
Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site						
<150	10%						
150-200	15%						

	<table border="1" style="margin-left: auto; margin-right: auto;"> <tr> <td style="width: 50%; text-align: center;">>200-450</td> <td style="width: 50%; text-align: center;">20%</td> </tr> <tr> <td style="text-align: center;">>450</td> <td style="text-align: center;">25%</td> </tr> </table>	>200-450	20%	>450	25%
>200-450	20%				
>450	25%				
	<p>B. the amount of existing soft landscaping prior to the development occurring.</p> <p>(b) where in association with a non-residential use:</p> <ul style="list-style-type: none"> (i) are set back at least 2 metres from the boundary of an allotment used for residential purposes. (ii) are set back at least 2 metres from a public road. (iii) have a floor area not exceeding 25m² <p>(c) in all cases, has a finished floor level not exceeding 1 metre above natural ground level at any point.</p>				
<p>PO 33.2</p> <p>Decks are designed and sited to minimise direct overlooking of habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones through suitable floor levels, screening and siting taking into account the slope of the subject land, existing vegetation on the subject land, and fencing.</p>	<p>DTS/DPF 33.2</p> <p>Decks with a finished floor level/s 500mm or more above natural ground level facing side or rear boundaries shared with a residential use in a neighbourhood-type zone incorporate screening with a maximum of 25% transparency/openings, permanently fixed to the outer edge of the deck not less than 1.5 m above the finished floor level/s.</p>				
<p>PO 33.3</p> <p>Decks used for outdoor dining, entertainment or other commercial uses provide carparking in accordance with the primary use of the deck.</p>	<p>DTS/DPF 33.3</p> <p>Decks used for commercial purposes do not result in less on-site car parking for the primary use of the subject land than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.</p>				

Table 1 - Private Open Space

Dwelling Type	Minimum Rate
Dwelling (at ground level)	<p>Total private open space area:</p> <ul style="list-style-type: none"> (a) Site area <301m²: 24m² located behind the building line. (b) Site area ≥ 301m²: 60m² located behind the building line. <p style="text-align: center;">Minimum directly accessible from a living room: 16m² / with a minimum dimension 3m.</p>
Dwelling (above ground level)	<p>Studio (no separate bedroom): 4m² with a minimum dimension 1.8m</p> <p>One bedroom: 8m² with a minimum dimension 2.1m</p> <p>Two bedroom dwelling: 11m² with a minimum dimension 2.4m</p> <p>Three + bedroom dwelling: 15m² with a minimum dimension 2.6m</p>
Cabin or caravan (permanently fixed to the ground) in a residential park or a caravan and tourist park	<p>Total area: 16m², which may be used as second car parking space, provided on each site intended for residential occupation.</p>

Design in Urban Areas

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome

DO 1	<p>Development is:</p> <ul style="list-style-type: none"> (a) contextual - by considering, recognising and carefully responding to its natural surroundings or built environment and positively contributing to the character of the locality (b) durable - fit for purpose, adaptable and long lasting (c) inclusive - by integrating landscape design to optimise pedestrian and cyclist usability, privacy and equitable access and promoting the provision of quality spaces integrated with the public realm that can be used for access and recreation and help optimise security and safety both internally and within the public realm, for occupants and visitors (d) sustainable - by integrating sustainable techniques into the design and siting of development and landscaping to improve community health, urban heat, water management, environmental performance, biodiversity and local amenity and to minimise energy consumption.
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Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
All Development	
External Appearance	
<p>PO 1.1</p> <p>Buildings reinforce corners through changes in setback, articulation, materials, colour and massing (including height, width, bulk, roof form and slope).</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Where zero or minor setbacks are desirable, development provides shelter over footpaths (in the form of verandahs, awnings, canopies and the like, with adequate lighting) to positively contribute to the walkability, comfort and safety of the public realm.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Building elevations facing the primary street (other than ancillary buildings) are designed and detailed to convey purpose, identify main access points and complement the streetscape.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>Plant, exhaust and intake vents and other technical equipment are integrated into the building design to minimise visibility from the public realm and negative impacts on residential amenity by:</p> <ul style="list-style-type: none"> (a) positioning plant and equipment discretely, in unobtrusive locations as viewed from public roads and spaces (b) screening rooftop plant and equipment from view (c) when located on the roof of non-residential development, locating the plant and equipment as far as practicable from adjacent sensitive land uses. 	<p>DTS/DPF 1.4</p> <p>Development does not incorporate any structures that protrude beyond the roofline.</p>
<p>PO 1.5</p> <p>The negative visual impact of outdoor storage, waste management, loading and service areas is minimised by integrating them into the building design and screening them from public view (such as fencing, landscaping and built form), taking into account the form of development contemplated in the relevant zone.</p>	<p>DTS/DPF 1.5</p> <p>None are applicable.</p>
Safety	
<p>PO 2.1</p> <p>Development maximises opportunities for passive surveillance of the public realm by providing clear lines of sight, appropriate lighting and the use of visually permeable screening wherever practicable.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Development is designed to differentiate public, communal and private areas.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>

<p>PO 2.3</p> <p>Buildings are designed with safe, perceptible and direct access from public street frontages and vehicle parking areas.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Development at street level is designed to maximise opportunities for passive surveillance of the adjacent public realm.</p>	<p>DTS/DPF 2.4</p> <p>None are applicable.</p>
<p>PO 2.5</p> <p>Common areas and entry points of buildings (such as the foyer areas of residential buildings) and non-residential land uses at street level, maximise passive surveillance from the public realm to the inside of the building at night.</p>	<p>DTS/DPF 2.5</p> <p>None are applicable.</p>
<p><small>Landscaping</small></p>	
<p>PO 3.1</p> <p>Soft landscaping and tree planting are incorporated to:</p> <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) maximise shade and shelter (c) maximise stormwater infiltration (d) enhance the appearance of land and streetscapes. 	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p><small>Environmental Performance</small></p>	
<p>PO 4.1</p> <p>Buildings are sited, oriented and designed to maximise natural sunlight access and ventilation to main activity areas, habitable rooms, common areas and open spaces.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Buildings are sited and designed to maximise passive environmental performance and minimise energy consumption and reliance on mechanical systems, such as heating and cooling.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
<p>PO 4.3</p> <p>Buildings incorporate climate responsive techniques and features such as building and window orientation, use of eaves, verandahs and shading structures, water harvesting, at ground landscaping, green walls, green roofs and photovoltaic cells.</p>	<p>DTS/DPF 4.3</p> <p>None are applicable.</p>
<p><small>Water Sensitive Design</small></p>	
<p>PO 5.1</p> <p>Development is sited and designed to maintain natural hydrological systems without negatively impacting:</p> <ul style="list-style-type: none"> (a) the quantity and quality of surface water and groundwater (b) the depth and directional flow of surface water and groundwater (c) the quality and function of natural springs. 	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>
<p><small>On-site Waste Treatment Systems</small></p>	
<p>PO 6.1</p> <p>Dedicated on-site effluent disposal areas do not include any areas to be used for, or could be reasonably foreseen to be used for, private open space, driveways or car parking.</p>	<p>DTS/DPF 6.1</p> <p>Effluent disposal drainage areas do not:</p> <ul style="list-style-type: none"> (a) encroach within an area used as private open space or result in less private open space than that specified in Design in Urban Areas Table 1 - Private Open Space (b) use an area also used as a driveway (c) encroach within an area used for on-site car parking or result in less on-site car parking than that specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements

or Table 2 - Off-Street Car Parking Requirements in Designated Areas.

Car parking appearance

PO 7.1

Development facing the street is designed to minimise the negative impacts of any semi-basement and undercroft car parking on streetscapes through techniques such as:

- (a) limiting protrusion above finished ground level
- (b) screening through appropriate planting, fencing and mounding
- (c) limiting the width of openings and integrating them into the building structure.

DTS/DPF 7.1

None are applicable.

PO 7.2

Vehicle parking areas appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced and the like.

DTS/DPF 7.2

None are applicable.

PO 7.3

Safe, legible, direct and accessible pedestrian connections are provided between parking areas and the development.

DTS/DPF 7.3

None are applicable.

PO 7.4

Street-level vehicle parking areas incorporate tree planting to provide shade, reduce solar heat absorption and reflection.

DTS/DPF 7.4

Vehicle parking areas that are open to the sky and comprise 10 or more car parking spaces include a shade tree with a mature canopy of 4m diameter spaced for each 10 car parking spaces provided and a landscaped strip on any road frontage of a minimum dimension of 1m.

PO 7.5

Street level parking areas incorporate soft landscaping to improve visual appearance when viewed from within the site and from public places.

DTS/DPF 7.5

Vehicle parking areas comprising 10 or more car parking spaces include soft landscaping with a minimum dimension of:

- (a) 1m along all public road frontages and allotment boundaries
- (b) 1m between double rows of car parking spaces.

PO 7.6

Vehicle parking areas and associated driveways are landscaped to provide shade and positively contribute to amenity.

DTS/DPF 7.6

None are applicable.

PO 7.7

Vehicle parking areas and access ways incorporate integrated stormwater management techniques such as permeable or porous surfaces, infiltration systems, drainage swales or rain gardens that integrate with soft landscaping.

DTS/DPF 7.7

None are applicable.

Earthworks and sloping land

PO 8.1

Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.

DTS/DPF 8.1

Development does not involve any of the following:

- (a) excavation exceeding a vertical height of 1m
- (b) filling exceeding a vertical height of 1m
- (c) a total combined excavation and filling vertical height of 2m or more.

PO 8.2

Driveways and access tracks designed and constructed to allow safe and convenient access on sloping land.

DTS/DPF 8.2

Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8) satisfy (a) and (b):

- (a) do not have a gradient exceeding 25% (1-in-4) at any point along the driveway
- (b) are constructed with an all-weather trafficable surface.

<p>PO 8.3</p> <p>Driveways and access tracks on sloping land (with a gradient exceeding 1 in 8):</p> <p>(a) do not contribute to the instability of embankments and cuttings (b) provide level transition areas for the safe movement of people and goods to and from the development (c) are designed to integrate with the natural topography of the land.</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>
<p>PO 8.4</p> <p>Development on sloping land (with a gradient exceeding 1 in 8) avoids the alteration of natural drainage lines and includes on site drainage systems to minimise erosion.</p>	<p>DTS/DPF 8.4</p> <p>None are applicable.</p>
<p>PO 8.5</p> <p>Development does not occur on land at risk of landslip or increase the potential for landslip or land surface instability.</p>	<p>DTS/DPF 8.5</p> <p>None are applicable.</p>
<p>Fences and walls</p>	
<p>PO 9.1</p> <p>Fences, walls and retaining walls of sufficient height maintain privacy and security without unreasonably impacting visual amenity and adjoining land's access to sunlight or the amenity of public places.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>
<p>PO 9.2</p> <p>Landscaping is incorporated on the low side of retaining walls that are visible from public roads and public open space to minimise visual impacts.</p>	<p>DTS/DPF 9.2</p> <p>A vegetated landscaped strip 1m wide or more is provided against the low side of a retaining wall.</p>
<p>Overlooking / Visual Privacy (low rise buildings)</p>	
<p>PO 10.1</p> <p>Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones.</p>	<p>DTS/DPF 10.1</p> <p>Upper level windows facing side or rear boundaries shared with a residential use in a neighbourhood-type zone:</p> <p>(a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 125mm (b) have sill heights greater than or equal to 1.5m above finished floor level (c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5 m above the finished floor level.</p>
<p>PO 10.2</p> <p>Development mitigates direct overlooking from balconies to habitable rooms and private open space of adjoining residential uses in neighbourhood type zones.</p>	<p>DTS/DPF 10.2</p> <p>One of the following is satisfied:</p> <p>(a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace or (b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of: (i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land or (ii) 1.7m above finished floor level in all other cases</p>
<p>Site Facilities / Waste Storage (excluding low rise residential development)</p>	
<p>PO 11.1</p> <p>Development provides a dedicated area for on-site collection and sorting of recyclable materials and refuse, green organic waste and wash bay facilities for the ongoing maintenance of bins that is adequate in size considering the number and nature of the activities they will serve and the frequency of collection.</p>	<p>DTS/DPF 11.1</p> <p>None are applicable.</p>
<p>PO 11.2</p> <p>Communal waste storage and collection areas are located, enclosed and designed to be screened from view from the public domain, open space and dwellings.</p>	<p>DTS/DPF 11.2</p> <p>None are applicable.</p>
<p>PO 11.3</p>	<p>DTS/DPF 11.3</p>

Communal waste storage and collection areas are designed to be well ventilated and located away from habitable rooms.	None are applicable.
PO 11.4 Communal waste storage and collection areas are designed to allow waste and recycling collection vehicles to enter and leave the site without reversing.	DTS/DPF 11.4 None are applicable.
PO 11.5 For mixed use developments, non-residential waste and recycling storage areas and access provide opportunities for on-site management of food waste through composting or other waste recovery as appropriate.	DTS/DPF 11.5 None are applicable.
All Development - Medium and High Rise	
External Appearance	
PO 12.1 Buildings positively contribute to the character of the local area by responding to local context.	DTS/DPF 12.1 None are applicable.
PO 12.2 Architectural detail at street level and a mixture of materials at lower building levels near the public interface are provided to reinforce a human scale.	DTS/DPF 12.2 None are applicable.
PO 12.3 Buildings are designed to reduce visual mass by breaking up building elevations into distinct elements.	DTS/DPF 12.3 None are applicable.
PO 12.4 Boundary walls visible from public land include visually interesting treatments to break up large blank elevations.	DTS/DPF 12.4 None are applicable.
PO 12.5 External materials and finishes are durable and age well to minimise ongoing maintenance requirements.	DTS/DPF 12.5 Buildings utilise a combination of the following external materials and finishes: (a) masonry (b) natural stone (c) pre-finished materials that minimise staining, discolouring or deterioration.
PO 12.6 Street-facing building elevations are designed to provide attractive, high quality and pedestrian-friendly street frontages.	DTS/DPF 12.6 Building street frontages incorporate: (a) active uses such as shops or offices (b) prominent entry areas for multi-storey buildings (where it is a common entry) (c) habitable rooms of dwellings (d) areas of communal public realm with public art or the like, where consistent with the zone and/or subzone provisions.
PO 12.7 Entrances to multi-storey buildings are safe, attractive, welcoming, functional and contribute to streetscape character.	DTS/DPF 12.7 Entrances to multi-storey buildings are: (a) oriented towards the street (b) clearly visible and easily identifiable from the street and vehicle parking areas (c) designed to be prominent, accentuated and a welcoming feature if there are no active or occupied ground floor uses (d) designed to provide shelter, a sense of personal address and transitional space around the entry (e) located as close as practicable to the lift and / or lobby access to minimise the need for long access corridors (f) designed to avoid the creation of potential areas of entrapment.
PO 12.8 Building services, plant and mechanical equipment are screened from the public realm.	DTS/DPF 12.8 None are applicable.

Landscaping

PO 13.1

Development facing a street provides a well landscaped area that contains a deep soil space to accommodate a tree of a species and size adequate to provide shade, contribute to tree canopy targets and soften the appearance of buildings.

DTS/DPF 13.1

Buildings provide a 4m by 4m deep soil space in front of the building that accommodates a medium to large tree, except where no building setback from front property boundaries is desired.

PO 13.2

Deep soil zones are provided to retain existing vegetation or provide areas that can accommodate new deep root vegetation, including tall trees with large canopies to provide shade and soften the appearance of multi-storey buildings.

DTS/DPF 13.2

Multi-storey development provides deep soil zones and incorporates trees at not less than the following rates, except in a location or zone where full site coverage is desired.

Site area	Minimum deep soil area	Minimum dimension	Tree / deep soil zones
<300 m ²	10 m ²	1.5m	1 small tree / 10 m ²
300-1500 m ²	7% site area	3m	1 medium tree / 30 m ²
>1500 m ²	7% site area	6m	1 large or medium tree / 60 m ²
<small>Tree size and site area definitions</small>			
Small tree	4-6m mature height and 2-4m canopy spread		
Medium tree	6-12m mature height and 4-8m canopy spread		
Large tree	12m mature height and >8m canopy spread		
Site area	The total area for development site, not average area per dwelling		

PO 13.3

Deep soil zones with access to natural light are provided to assist in maintaining vegetation health.

DTS/DPF 13.3

None are applicable.

PO 13.4

Unless separated by a public road or reserve, development sites adjacent to any zone that has a primary purpose of accommodating low-rise residential development incorporate a deep soil zone along the common boundary to enable medium to large trees to be retained or established to assist in screening new buildings of 3 or more building levels in height.

DTS/DPF 13.4

Building elements of 3 or more building levels in height are set back at least 6m from a zone boundary in which a deep soil zone area is incorporated.

Environmental

PO 14.1

Development minimises detrimental micro-climatic impacts on adjacent land and buildings.

DTS/DPF 14.1

None are applicable.

<p>PO 14.2</p> <p>Development incorporates sustainable design techniques and features such as window orientation, eaves and shading structures, water harvesting and use, green walls and roof designs that enable the provision of rain water tanks (where they are not provided elsewhere on site), green roofs and photovoltaic cells.</p>	<p>DTS/DPF 14.2</p> <p>None are applicable.</p>
<p>PO 14.3</p> <p>Development of 5 or more building levels, or 21m or more in height (as measured from natural ground level and excluding roof-mounted mechanical plant and equipment) is designed to minimise the impacts of wind through measures such as:</p> <ul style="list-style-type: none"> (a) a podium at the base of a tall tower and aligned with the street to deflect wind away from the street (b) substantial verandahs around a building to deflect downward travelling wind flows over pedestrian areas (c) the placement of buildings and use of setbacks to deflect the wind at ground level (d) avoiding tall shear elevations that create windy conditions at street level. 	<p>DTS/DPF 14.3</p> <p>None are applicable.</p>
<p>Car Parking</p>	
<p>PO 15.1</p> <p>Multi-level vehicle parking structures are designed to contribute to active street frontages and complement neighbouring buildings.</p>	<p>DTS/DPF 15.1</p> <p>Multi-level vehicle parking structures within buildings:</p> <ul style="list-style-type: none"> (a) provide land uses such as commercial, retail or other non-car parking uses along ground floor street frontages (b) incorporate facade treatments in building elevations facing along major street frontages that are sufficiently enclosed and detailed to complement adjacent buildings.
<p>PO 15.2</p> <p>Multi-level vehicle parking structures within buildings complement the surrounding built form in terms of height, massing and scale.</p>	<p>DTS/DPF 15.2</p> <p>None are applicable.</p>
<p>Overlooking/Visual Privacy</p>	
<p>PO 16.1</p> <p>Development mitigates direct overlooking of habitable rooms and private open spaces of adjacent residential uses in neighbourhood-type zones through measures such as:</p> <ul style="list-style-type: none"> (a) appropriate site layout and building orientation (b) off-setting the location of balconies and windows of habitable rooms or areas with those of other buildings so that views are oblique rather than direct to avoid direct line of sight (c) building setbacks from boundaries (including building boundary to boundary where appropriate) that interrupt views or that provide a spatial separation between balconies or windows of habitable rooms (d) screening devices that are integrated into the building design and have minimal negative effect on residents' or neighbours' amenity. 	<p>DTS/DPF 16.1</p> <p>None are applicable.</p>
<p>All residential development</p>	
<p>Front elevations and passive surveillance</p>	
<p>PO 17.1</p> <p>Dwellings incorporate windows facing primary street frontages to encourage passive surveillance and make a positive contribution to the streetscape.</p>	<p>DTS/DPF 17.1</p> <p>Each dwelling with a frontage to a public street:</p> <ul style="list-style-type: none"> (a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m (b) has an aggregate window area of at least 2m² facing the primary street.
<p>PO 17.2</p> <p>Dwellings incorporate entry doors within street frontages to address the street and provide a legible entry point for visitors.</p>	<p>DTS/DPF 17.2</p> <p>Dwellings with a frontage to a public street have an entry door visible from the primary street boundary.</p>
<p>Outlook and Amenity</p>	

<p>PO 18.1</p> <p>Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 18.1</p> <p>A living room of a dwelling incorporates a window with an external outlook of the street frontage, private open space, public open space, or waterfront areas.</p>								
<p>PO 18.2</p> <p>Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.</p>	<p>DTS/DPF 18.2</p> <p>None are applicable.</p>								
<p>Ancillary Development</p>									
<p>PO 19.1</p> <p>Residential ancillary buildings are sited and designed to not detract from the streetscape or appearance of primary residential buildings on the site or neighbouring properties.</p>	<p>DTS/DPF 19.1</p> <p>Ancillary buildings:</p> <ul style="list-style-type: none"> (a) are ancillary to a dwelling erected on the same site (b) have a floor area not exceeding 60m² (c) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> (i) in front of any part of the building line of the dwelling to which it is ancillary or (ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (d) in the case of a garage or carport, the garage or carport: <ul style="list-style-type: none"> (i) is set back at least 5.5m from the boundary of the primary street (ii) when facing a primary street or secondary street, has a total door / opening not exceeding: <ul style="list-style-type: none"> A. for dwellings of single building level - 7m in width or 50% of the site frontage, whichever is the lesser B. for dwellings comprising two or more building levels at the building line fronting the same public street - 7m in width (e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless: <ul style="list-style-type: none"> (i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary and (ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent (f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary (g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure (h) have a wall height or post height not exceeding 3m above natural ground level (and not including a gable end) (i) have a roof height where no part of the roof is more than 5m above the natural ground level (j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour (k) retains a total area of soft landscaping for the entire development site, including any common property, with a minimum dimension of 700mm in accordance with (i) or (ii), whichever is less: <ul style="list-style-type: none"> (i) a total area as determined by the following table: <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr style="background-color: #1a3d54; color: white;"> <th style="padding: 5px;">Site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="padding: 5px;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;"><150</td> <td style="padding: 5px;">10%</td> </tr> <tr> <td style="padding: 5px;">150-200</td> <td style="padding: 5px;">15%</td> </tr> <tr> <td style="padding: 5px;">>200-450</td> <td style="padding: 5px;">20%</td> </tr> </tbody> </table>	Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%
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<150	10%								
150-200	15%								
>200-450	20%								

	>450	25%
(ii) the amount of existing soft landscaping prior to the development occurring.		

<p>PO 19.2</p> <p>Ancillary buildings and structures do not impede on-site functional requirements such as private open space provision, car parking requirements or result in over-development of the site.</p>	<p>DTS/DPF 19.2</p> <p>Ancillary buildings and structures do not result in:</p> <p>(a) less private open space than specified in Design in Urban Areas Table 1 - Private Open Space</p> <p>(b) less on-site car parking than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.</p>
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<p>PO 19.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers.</p>	<p>DTS/DPF 19.3</p> <p>The pump and/or filtration system is ancillary to a dwelling erected on the same site and is:</p> <p>(a) enclosed in a solid acoustic structure that is located at least 5m from the nearest habitable room located on an adjoining allotment</p> <p style="padding-left: 20px;">or</p> <p>(b) located at least 12m from the nearest habitable room located on an adjoining allotment.</p>
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<p>PO 19.4</p> <p>Buildings and structures that are ancillary to an existing non-residential use do not detract from the streetscape character, appearance of buildings on the site of the development, or the amenity of neighbouring properties.</p>	<p>DTS/DPF 19.4</p> <p>Non-residential ancillary buildings and structures:</p> <p>(a) are ancillary and subordinate to an existing non-residential use on the same site</p> <p>(b) have a floor area not exceeding the following:</p> <table border="1" style="margin-left: 40px; border-collapse: collapse;"> <thead> <tr> <th style="padding: 2px;">Allotment size</th> <th style="padding: 2px;">Floor area</th> </tr> </thead> <tbody> <tr> <td style="padding: 2px;">≤500m²</td> <td style="padding: 2px;">60m²</td> </tr> <tr> <td style="padding: 2px;">>500m²</td> <td style="padding: 2px;">80m²</td> </tr> </tbody> </table> <p>(c) are not constructed, added to or altered so that any part is situated:</p> <p style="padding-left: 20px;">(i) in front of any part of the building line of the main building to which it is ancillary</p> <p style="padding-left: 40px;">or</p> <p style="padding-left: 20px;">(ii) within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads)</p> <p>(d) in the case of a garage or carport, the garage or carport:</p> <p style="padding-left: 20px;">(i) is set back at least 5.5m from the boundary of the primary street</p> <p>(e) if situated on a boundary (not being a boundary with a primary street or secondary street), do not exceed a length of 11.5m unless:</p> <p style="padding-left: 20px;">(i) a longer wall or structure exists on the adjacent site and is situated on the same allotment boundary</p> <p style="padding-left: 20px;">(ii) the proposed wall or structure will be built along the same length of boundary as the existing adjacent wall or structure to the same or lesser extent</p> <p>(f) if situated on a boundary of the allotment (not being a boundary with a primary street or secondary street), all walls or structures on the boundary will not exceed 45% of the length of that boundary</p> <p>(g) will not be located within 3m of any other wall along the same boundary unless on an adjacent site on that boundary there is an existing wall of a building that would be adjacent to or about the proposed wall or structure</p> <p>(h) have a wall height (or post height) not exceeding 3m (and not including a gable end)</p> <p>(i) have a roof height where no part of the roof is more than 5m above the natural ground level</p> <p>(j) if clad in sheet metal, is pre-colour treated or painted in a non-reflective colour.</p>	Allotment size	Floor area	≤500m ²	60m ²	>500m ²	80m ²
Allotment size	Floor area						
≤500m ²	60m ²						
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Residential Development - Low Rise

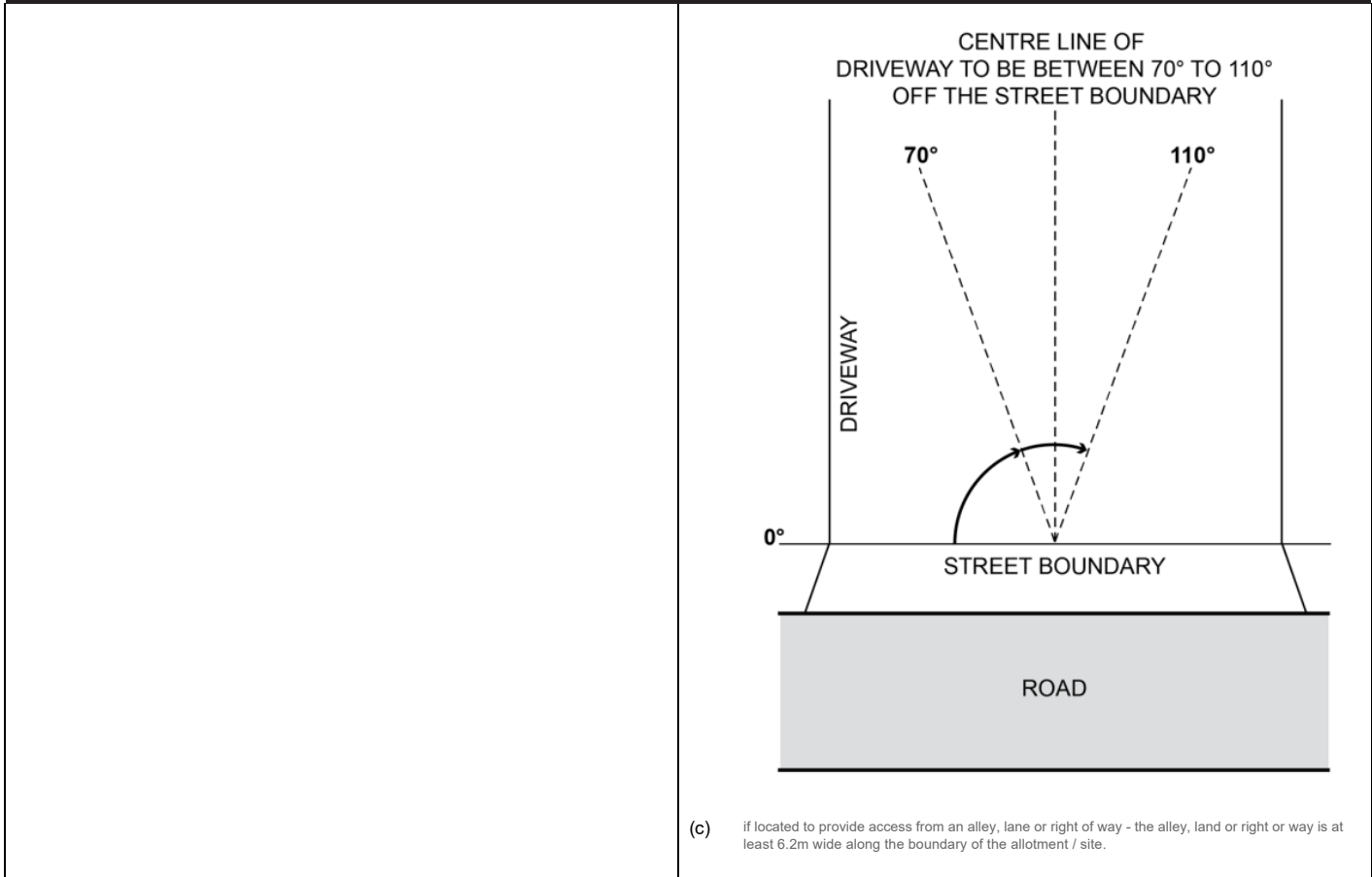
External appearance

<p>PO 20.1</p> <p>Garaging is designed to not detract from the streetscape or appearance of a dwelling.</p>	<p>DTS/DPF 20.1</p> <p>Garages and carports facing a street:</p> <p>(a) are situated so that no part of the garage or carport will be in front of any part of the building line of the dwelling</p> <p>(b) are set back at least 5.5m from the boundary of the primary street</p> <p>(c) have a garage door / opening width not exceeding 7m</p> <p>(d) have a garage door / opening width not exceeding 50% of the site frontage unless the dwelling has two or more building levels at the building line fronting the same public street.</p>
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<p>PO 20.2</p> <p>Dwelling elevations facing public streets and common driveways make a positive</p>	<p>DTS/DPF 20.2</p> <p>Each dwelling includes at least 3 of the following design features within the building</p>
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contribution to the streetscape and the appearance of common driveway areas.	elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway: <ul style="list-style-type: none"> (a) a minimum of 30% of the building wall is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building wall (c) a balcony projects from the building wall (d) a verandah projects at least 1m from the building wall (e) eaves of a minimum 400mm width extend along the width of the front elevation (f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm (g) a minimum of two different materials or finishes are incorporated on the walls of the front building elevation, with a maximum of 80% of the building elevation in a single material or finish. 										
PO 20.3 The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.	DTS/DPF 20.3 None are applicable										
Private Open Space											
PO 21.1 Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.	DTS/DPF 21.1 Private open space is provided in accordance with Design in Urban Areas Table 1 - Private Open Space.										
PO 21.2 Private open space is positioned to provide convenient access from internal living areas.	DTS/DPF 21.2 Private open space is directly accessible from a habitable room.										
Landscaping											
PO 22.1 Soft landscaping is incorporated into development to: <ul style="list-style-type: none"> (a) minimise heat absorption and reflection (b) contribute shade and shelter (c) provide for stormwater infiltration and biodiversity (d) enhance the appearance of land and streetscapes. 	DTS/DPF 22.1 Residential development incorporates soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b): <ul style="list-style-type: none"> (a) a total area for the entire development site, including any common property, as determined by the following table: <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr style="background-color: #1a3d54; color: white;"> <th style="padding: 10px;">Site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="padding: 10px;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;"><150</td> <td style="padding: 5px;">10%</td> </tr> <tr> <td style="padding: 5px;">150-200</td> <td style="padding: 5px;">15%</td> </tr> <tr> <td style="padding: 5px;">>200-450</td> <td style="padding: 5px;">20%</td> </tr> <tr> <td style="padding: 5px;">>450</td> <td style="padding: 5px;">25%</td> </tr> </tbody> </table>	Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
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	(b) at least 30% of any land between the primary street boundary and the primary building line.
Car parking, access and manoeuvrability	
<p>PO 23.1</p> <p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.1</p> <p>Residential car parking spaces enclosed by fencing, walls or other structures have the following internal dimensions (separate from any waste storage area):</p> <p>(a) single width car parking spaces:</p> <p>(i) a minimum length of 5.4m per space</p> <p>(ii) a minimum width of 3.0m</p> <p>(iii) a minimum garage door width of 2.4m</p> <p>(b) double width car parking spaces (side by side):</p> <p>(i) a minimum length of 5.4m</p> <p>(ii) a minimum width of 5.4m</p> <p>(iii) minimum garage door width of 2.4m per space.</p>
<p>PO 23.2</p> <p>Uncovered car parking space are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 23.2</p> <p>Uncovered car parking spaces have:</p> <p>(a) a minimum length of 5.4m</p> <p>(b) a minimum width of 2.4m</p> <p>(c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.</p>
<p>PO 23.3</p> <p>Driveways and access points are located and designed to facilitate safe access and egress while maximising land available for street tree planting, pedestrian movement, domestic waste collection, landscaped street frontages and on-street parking.</p>	<p>DTS/DPF 23.3</p> <p>Driveways and access points satisfy (a) or (b):</p> <p>(a) sites with a frontage to a public road of 10m or less, have a width between 3.0 and 3.2 metres measured at the property boundary and are the only access point provided on the site</p> <p>(b) sites with a frontage to a public road greater than 10m:</p> <p>(i) have a maximum width of 5m measured at the property boundary and are the only access point provided on the site;</p> <p>(ii) have a width between 3.0 metres and 3.2 metres measured at the property boundary and no more than two access points are provided on site, separated by no less than 1m.</p>
<p>PO 23.4</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 23.4</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <p>(a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land</p> <p>(b) where newly proposed, is set back:</p> <p>(i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner</p> <p>(ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance</p> <p>(iii) 6m or more from the tangent point of an intersection of 2 or more roads</p> <p>(iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.</p>
<p>PO 23.5</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 23.5</p> <p>Driveways are designed and sited so that:</p> <p>(a) the gradient of the driveway does not exceed a grade of 1 in 4 and includes transitions to ensure a maximum grade change of 12.5% (1 in 8) for summit changes, and 15% (1 in 6.7) for sag changes, in accordance with AS 2890.1:2004 to prevent vehicles bottoming or scraping</p> <p>(b) the centreline of the driveway has an angle of no less than 70 degrees and no more than 110 degrees from the street boundary to which it takes its access as shown in the following diagram:</p>



(c) if located to provide access from an alley, lane or right of way - the alley, land or right or way is at least 6.2m wide along the boundary of the allotment / site.

PO 23.6
 Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.

DTS/DPF 23.6
 Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:

- (a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number)
- (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly
- (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.

Waste storage

PO 24.1
 Provision is made for the convenient storage of waste bins in a location screened from public view.

DTS/DPF 24.1
 Where dwellings abut both side boundaries a waste bin storage area is provided behind the building line of each dwelling that:

- (a) has a minimum area of 2m² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space); and
- (b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.

Design of Transportable Buildings

PO 25.1
 The sub-floor space beneath transportable buildings is enclosed to give the appearance of a permanent structure.

DTS/DPF 25.1
 Buildings satisfy (a) or (b):

- (a) are not transportable
- (b) the sub-floor space between the building and ground level is clad in a material and finish consistent with the building.

Residential Development - Medium and High Rise (including serviced apartments)

Outlook and Visual Privacy

PO 26.1
 Ground level dwellings have a satisfactory short range visual outlook to public,

DTS/DPF 26.1
 Buildings:

communal or private open space.	<p>(a) provide a habitable room at ground or first level with a window facing toward the street</p> <p>(b) limit the height / extent of solid walls or fences facing the street to 1.2m high above the footpath level or, where higher, to 50% of the site frontage.</p>
<p>PO 26.2</p> <p>The visual privacy of ground level dwellings within multi-level buildings is protected.</p>	<p>DTS/DPF 26.2</p> <p>The finished floor level of ground level dwellings in multi-storey developments is raised by up to 1.2m.</p>
Private Open Space	
<p>PO 27.1</p> <p>Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.</p>	<p>DTS/DPF 27.1</p> <p>Private open space provided in accordance with Design in Urban Areas Table 1 - Private Open Space.</p>
Residential amenity in multi-level buildings	
<p>PO 28.1</p> <p>Residential accommodation within multi-level buildings have habitable rooms, windows and balconies designed and positioned to be separated from those of other dwellings and accommodation to provide visual and acoustic privacy and allow for natural ventilation and the infiltration of daylight into interior and outdoor spaces.</p>	<p>DTS/DPF 28.1</p> <p>Habitable rooms and balconies of independent dwellings and accommodation are separated by at least 6m from one another where there is a direct line of sight between them and 3m or more from a side or rear property boundary.</p>
<p>PO 28.2</p> <p>Balconies are designed, positioned and integrated into the overall architectural form and detail of the development to:</p> <p>(a) respond to daylight, wind, and acoustic conditions to maximise comfort and provide visual privacy</p> <p>(b) allow views and casual surveillance of the street while providing for safety and visual privacy of nearby living spaces and private outdoor areas.</p>	<p>DTS/DPF 28.2</p> <p>Balconies utilise one or a combination of the following design elements:</p> <p>(a) sun screens</p> <p>(b) pergolas</p> <p>(c) louvres</p> <p>(d) green facades</p> <p>(e) openable walls.</p>
<p>PO 28.3</p> <p>Balconies are of sufficient size and depth to accommodate outdoor seating and promote indoor / outdoor living.</p>	<p>DTS/DPF 28.3</p> <p>Balconies open directly from a habitable room and incorporate a minimum dimension of 2m.</p>
<p>PO 28.4</p> <p>Dwellings are provided with sufficient space for storage to meet likely occupant needs.</p>	<p>DTS/DPF 28.4</p> <p>Dwellings (not including student accommodation or serviced apartments) are provided with storage at the following rates with at least 50% or more of the storage volume to be provided within the dwelling:</p> <p>(a) studio: not less than 6m³</p> <p>(b) 1 bedroom dwelling / apartment: not less than 8m³</p> <p>(c) 2 bedroom dwelling / apartment: not less than 10m³</p> <p>(d) 3+ bedroom dwelling / apartment: not less than 12m³.</p>
<p>PO 28.5</p> <p>Dwellings that use light wells for access to daylight, outlook and ventilation for habitable rooms, are designed to ensure a reasonable living amenity is provided.</p>	<p>DTS/DPF 28.5</p> <p>Light wells:</p> <p>(a) are not used as the primary source of outlook for living rooms</p> <p>(b) up to 18m in height have a minimum horizontal dimension of 3m, or 6m if overlooked by bedrooms</p> <p>(c) above 18m in height have a minimum horizontal dimension of 6m, or 9m if overlooked by bedrooms.</p>
<p>PO 28.6</p> <p>Attached or abutting dwellings are designed to minimise the transmission of sound between dwellings and, in particular, to protect bedrooms from possible noise intrusions.</p>	<p>DTS/DPF 28.6</p> <p>None are applicable.</p>
<p>PO 28.7</p>	<p>DTS/DPF 28.7</p>

Dwellings are designed so that internal structural columns correspond with the position of internal walls to ensure that the space within the dwelling/apartment is useable.	None are applicable.										
<small>Dwelling Configuration</small>											
<p>PO 29.1</p> <p>Buildings containing in excess of 10 dwellings provide a variety of dwelling sizes and a range in the number of bedrooms per dwelling to contribute to housing diversity.</p>	<p>DTS/DPF 29.1</p> <p>Buildings containing in excess of 10 dwellings provide at least one of each of the following:</p> <ul style="list-style-type: none"> (a) studio (where there is no separate bedroom) (b) 1 bedroom dwelling / apartment with a floor area of at least 50m² (c) 2 bedroom dwelling / apartment with a floor area of at least 65m² (d) 3+ bedroom dwelling / apartment with a floor area of at least 80m², and any dwelling over 3 bedrooms provides an additional 15m² for every additional bedroom. 										
<p>PO 29.2</p> <p>Dwellings located on the ground floor of multi-level buildings with 3 or more bedrooms have the windows of their habitable rooms overlooking internal courtyard space or other public space, where possible.</p>	<p>DTS/DPF 29.2</p> <p>None are applicable.</p>										
<small>Common Areas</small>											
<p>PO 30.1</p> <p>The size of lifts, lobbies and corridors is sufficient to accommodate movement of bicycles, strollers, mobility aids and visitor waiting areas.</p>	<p>DTS/DPF 30.1</p> <p>Common corridor or circulation areas:</p> <ul style="list-style-type: none"> (a) have a minimum ceiling height of 2.7m (b) provide access to no more than 8 dwellings (c) incorporate a wider section at apartment entries where the corridors exceed 12m in length from a core. 										
<small>Group Dwellings, Residential Flat Buildings and Battle axe Development</small>											
<small>Amenity</small>											
<p>PO 31.1</p> <p>Dwellings are of a suitable size to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 31.1</p> <p>Dwellings have a minimum internal floor area in accordance with the following table:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="text-align: left; padding: 5px;">Number of bedrooms</th> <th style="text-align: left; padding: 5px;">Minimum internal floor area</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;">Studio</td> <td style="padding: 5px;">35m²</td> </tr> <tr> <td style="padding: 5px;">1 bedroom</td> <td style="padding: 5px;">50m²</td> </tr> <tr> <td style="padding: 5px;">2 bedroom</td> <td style="padding: 5px;">65m²</td> </tr> <tr> <td style="padding: 5px;">3+ bedrooms</td> <td style="padding: 5px;">80m² and any dwelling over 3 bedrooms provides an additional 15m² for every additional bedroom</td> </tr> </tbody> </table>	Number of bedrooms	Minimum internal floor area	Studio	35m ²	1 bedroom	50m ²	2 bedroom	65m ²	3+ bedrooms	80m ² and any dwelling over 3 bedrooms provides an additional 15m ² for every additional bedroom
Number of bedrooms	Minimum internal floor area										
Studio	35m ²										
1 bedroom	50m ²										
2 bedroom	65m ²										
3+ bedrooms	80m ² and any dwelling over 3 bedrooms provides an additional 15m ² for every additional bedroom										
<p>PO 31.2</p> <p>The orientation and siting of buildings minimises impacts on the amenity, outlook and privacy of occupants and neighbours.</p>	<p>DTS/DPF 31.2</p> <p>None are applicable.</p>										
<p>PO 31.3</p> <p>Development maximises the number of dwellings that face public open space and public streets and limits dwellings oriented towards adjoining properties.</p>	<p>DTS/DPF 31.3</p> <p>None are applicable.</p>										
<p>PO 31.4</p>	<p>DTS/DPF 31.4</p>										

Battle-axe development is appropriately sited and designed to respond to the existing neighbourhood context.	Dwelling sites/allotments are not in the form of a battle-axe arrangement.
Communal Open Space	
PO 32.1 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 32.1 None are applicable.
PO 32.2 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 32.2 Communal open space incorporates a minimum dimension of 5 metres.
PO 32.3 Communal open space is designed and sited to: (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 32.3 None are applicable.
PO 32.4 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 32.4 None are applicable.
PO 32.5 Communal open space is designed and sited to: (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings (b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.	DTS/DPF 32.5 None are applicable.
Car parking, access and manoeuvrability	
PO 33.1 Driveways and access points are designed and distributed to optimise the provision of on-street visitor parking.	DTS/DPF 33.1 Where on-street parking is available directly adjacent the site, on-street parking is retained adjacent the subject site in accordance with the following requirements: (a) minimum 0.33 on-street car parks per proposed dwelling (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
PO 33.2 The number of vehicular access points onto public roads is minimised to reduce interruption of the footpath and positively contribute to public safety and walkability.	DTS/DPF 33.2 Access to group dwellings or dwellings within a residential flat building is provided via a single common driveway.
PO 33.3 Residential driveways that service more than one dwelling are designed to allow safe and convenient movement.	DTS/DPF 33.3 Driveways that service more than 1 dwelling or a dwelling on a battle-axe site: (a) have a minimum width of 3m (b) for driveways servicing more than 3 dwellings: (i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street (ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.
PO 33.4 Residential driveways that service more than one dwelling or a dwelling on a battle-axe site are designed to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 33.4 Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre.
PO 33.5 Dwellings are adequately separated from common driveways and manoeuvring	DTS/DPF 33.5 Dwelling walls with entry doors or ground level habitable room windows are set back

areas.	at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.
Soft landscaping	
<p>PO 34.1</p> <p>Soft landscaping is provided between dwellings and common driveways to improve the outlook for occupants and appearance of common areas.</p>	<p>DTS/DPF 34.1</p> <p>Other than where located directly in front of a garage or building entry, soft landscaping with a minimum dimension of 1m is provided between a dwelling and common driveway.</p>
<p>PO 34.2</p> <p>Battle-axe or common driveways incorporate landscaping and permeability to improve appearance and assist in stormwater management.</p>	<p>DTS/DPF 34.2</p> <p>Battle-axe or common driveways satisfy (a) and (b):</p> <p>(a) are constructed of a minimum of 50% permeable or porous material</p> <p>(b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).</p>
Site Facilities / Waste Storage	
<p>PO 35.1</p> <p>Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.</p>	<p>DTS/DPF 35.1</p> <p>None are applicable.</p>
<p>PO 35.2</p> <p>Provision is made for suitable external clothes drying facilities.</p>	<p>DTS/DPF 35.2</p> <p>None are applicable.</p>
<p>PO 35.3</p> <p>Provision is made for suitable household waste and recyclable material storage facilities which are:</p> <p>(a) located away, or screened, from public view, and</p> <p>(b) conveniently located in proximity to dwellings and the waste collection point.</p>	<p>DTS/DPF 35.3</p> <p>None are applicable.</p>
<p>PO 35.4</p> <p>Waste and recyclable material storage areas are located away from dwellings.</p>	<p>DTS/DPF 35.4</p> <p>Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.</p>
<p>PO 35.5</p> <p>Where waste bins cannot be conveniently collected from the street, provision is made for on-site waste collection, designed to accommodate the safe and convenient access, egress and movement of waste collection vehicles.</p>	<p>DTS/DPF 35.5</p> <p>None are applicable.</p>
<p>PO 35.6</p> <p>Services including gas and water meters are conveniently located and screened from public view.</p>	<p>DTS/DPF 35.6</p> <p>None are applicable.</p>
Water sensitive urban design	
<p>PO 36.1</p> <p>Residential development creating a common driveway / access includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.</p>	<p>DTS/DPF 36.1</p> <p>None are applicable.</p>
<p>PO 36.2</p>	<p>DTS/DPF 36.2</p>

Residential development creating a common driveway / access includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	None are applicable.
Supported Accommodation and retirement facilities	
Siting, Configuration and Design	
PO 37.1 Supported accommodation and housing for aged persons and people with disabilities is located where on-site movement of residents is not unduly restricted by the slope of the land.	DTS/DPF 37.1 None are applicable.
PO 37.2 Universal design features are incorporated to provide options for people living with disabilities or limited mobility and / or to facilitate ageing in place.	DTS/DPF 37.2 None are applicable.
Movement and Access	
PO 38.1 Development is designed to support safe and convenient access and movement for residents by providing: (a) ground-level access or lifted access to all units (b) level entry porches, ramps, paths, driveways, passenger loading areas and areas adjacent to footpaths that allow for the passing of wheelchairs and resting places (c) car parks with gradients no steeper than 1-in-40, and of sufficient area to provide for wheelchair manoeuvrability (d) kerb ramps at pedestrian crossing points.	DTS/DPF 38.1 None are applicable.
Communal Open Space	
PO 39.1 Development is designed to provide attractive, convenient and comfortable indoor and outdoor communal areas to be used by residents and visitors.	DTS/DPF 39.1 None are applicable.
PO 39.2 Private open space provision may be substituted for communal open space which is designed and sited to meet the recreation and amenity needs of residents.	DTS/DPF 39.2 None are applicable.
PO 39.3 Communal open space is of sufficient size and dimensions to cater for group recreation.	DTS/DPF 39.3 Communal open space incorporates a minimum dimension of 5 metres.
PO 39.4 Communal open space is designed and sited to: (a) be conveniently accessed by the dwellings which it services (b) have regard to acoustic, safety, security and wind effects.	DTS/DPF 39.4 None are applicable.
PO 39.5 Communal open space contains landscaping and facilities that are functional, attractive and encourage recreational use.	DTS/DPF 39.5 None are applicable.
PO 39.6 Communal open space is designed and sited to: (a) in relation to rooftop or elevated gardens, minimise overlooking into habitable room windows or onto the useable private open space of other dwellings	DTS/DPF 39.6 None are applicable.

(b) in relation to ground floor communal space, be overlooked by habitable rooms to facilitate passive surveillance.	
Site Facilities / Waste Storage	
<p>PO 40.1</p> <p>Development is designed to provide storage areas for personal items and specialised equipment such as small electric powered vehicles, including facilities for the recharging of small electric-powered vehicles.</p>	<p>DTS/DPF 40.1</p> <p>None are applicable.</p>
<p>PO 40.2</p> <p>Provision is made for suitable mailbox facilities close to the major pedestrian entry to the site or conveniently located considering the nature of accommodation and mobility of occupants.</p>	<p>DTS/DPF 40.2</p> <p>None are applicable.</p>
<p>PO 40.3</p> <p>Provision is made for suitable external clothes drying facilities.</p>	<p>DTS/DPF 40.3</p> <p>None are applicable.</p>
<p>PO 40.4</p> <p>Provision is made for suitable household waste and recyclable material storage facilities conveniently located away, or screened, from view.</p>	<p>DTS/DPF 40.4</p> <p>None are applicable.</p>
<p>PO 40.5</p> <p>Waste and recyclable material storage areas are located away from dwellings.</p>	<p>DTS/DPF 40.5</p> <p>Dedicated waste and recyclable material storage areas are located at least 3m from any habitable room window.</p>
<p>PO 40.6</p> <p>Provision is made for on-site waste collection where 10 or more bins are to be collected at any one time.</p>	<p>DTS/DPF 40.6</p> <p>None are applicable.</p>
<p>PO 40.7</p> <p>Services, including gas and water meters, are conveniently located and screened from public view.</p>	<p>DTS/DPF 40.7</p> <p>None are applicable.</p>
Student Accommodation	
<p>PO 41.1</p> <p>Student accommodation is designed to provide safe, secure, attractive, convenient and comfortable living conditions for residents, including an internal layout and facilities that are designed to provide sufficient space and amenity for the requirements of student life and promote social interaction.</p>	<p>DTS/DPF 41.1</p> <p>Student accommodation provides:</p> <ul style="list-style-type: none"> (a) a range of living options to meet a variety of accommodation needs, such as one-bedroom, two-bedroom and disability access units (b) common or shared facilities to enable a more efficient use of space, including: <ul style="list-style-type: none"> (i) shared cooking, laundry and external drying facilities (ii) internal and external communal and private open space provided in accordance with Design in Urban Areas Table 1 - Private Open Space (iii) common storage facilities at the rate of 8m³ for every 2 dwellings or students (iv) common on-site parking in accordance with Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas (v) bicycle parking at the rate of one space for every 2 students.
<p>PO 41.2</p> <p>Student accommodation is designed to provide easy adaptation of the building to accommodate an alternative use of the building in the event it is no longer required for student housing.</p>	<p>DTS/DPF 41.2</p> <p>None are applicable.</p>
All non-residential development	
Water Sensitive Design	

<p>PO 42.1</p> <p>Development likely to result in risk of export of sediment, suspended solids, organic matter, nutrients, oil and grease include stormwater management systems designed to minimise pollutants entering stormwater.</p>	<p>DTS/DPF 42.1</p> <p>None are applicable.</p>
<p>PO 42.2</p> <p>Water discharged from a development site is of a physical, chemical and biological condition equivalent to or better than its pre-developed state.</p>	<p>DTS/DPF 42.2</p> <p>None are applicable.</p>
<p>PO 42.3</p> <p>Development includes stormwater management systems to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that development does not increase peak flows in downstream systems.</p>	<p>DTS/DPF 42.3</p> <p>None are applicable.</p>
<p><small>Wash-down and Waste Loading and Unloading</small></p>	
<p>PO 43.1</p> <p>Areas for activities including loading and unloading, storage of waste refuse bins in commercial and industrial development or wash-down areas used for the cleaning of vehicles, plant or equipment are:</p> <ul style="list-style-type: none"> (a) designed to contain all wastewater likely to pollute stormwater within a bunded and roofed area to exclude the entry of external surface stormwater run-off (b) paved with an impervious material to facilitate wastewater collection (c) of sufficient size to prevent 'splash-out' or 'over-spray' of wastewater from the wash-down area (d) are designed to drain wastewater to either: <ul style="list-style-type: none"> (i) a treatment device such as a sediment trap and coalescing plate oil separator with subsequent disposal to a sewer, private or Community Wastewater Management Scheme or (ii) a holding tank and its subsequent removal off-site on a regular basis. 	<p>DTS/DPF 43.1</p> <p>None are applicable.</p>
<p><small>Laneway Development</small></p>	
<p><small>Infrastructure and Access</small></p>	
<p>PO 44.1</p> <p>Development with a primary street comprising a laneway, alley, lane, right of way or similar minor thoroughfare only occurs where:</p> <ul style="list-style-type: none"> (a) existing utility infrastructure and services are capable of accommodating the development (b) the primary street can support access by emergency and regular service vehicles (such as waste collection) (c) it does not require the provision or upgrading of infrastructure on public land (such as footpaths and stormwater management systems) (d) safety of pedestrians or vehicle movement is maintained (e) any necessary grade transition is accommodated within the site of the development to support an appropriate development intensity and orderly development of land fronting minor thoroughfares. 	<p>DTS/DPF 44.1</p> <p>Development with a primary street frontage that is not an alley, lane, right of way or similar public thoroughfare.</p>
<p><small>Decks</small></p>	
<p><small>Design and Siting</small></p>	
<p>PO 45.1</p> <p>Decks are designed and sited to:</p> <ul style="list-style-type: none"> (a) complement the associated building form (b) minimise impacts on the streetscape through siting behind the building line of the principal building (unless on a significant allotment or open space) (c) minimise cut and fill and overall massing when viewed from adjacent land. 	<p>DTS/DPF 45.1</p> <p>Decks:</p> <ul style="list-style-type: none"> (a) where ancillary to a dwelling: <ul style="list-style-type: none"> (i) are not constructed, added to or altered so that any part is situated: <ul style="list-style-type: none"> A. in front of any part of the building line of the dwelling to which it is ancillary or B. within 900mm of a boundary of the allotment with a secondary street (if the land has boundaries on two or more roads) (ii) are set back at least 900mm from side or rear allotment boundaries

	<p>(iii) when attached to the dwelling, has a finished floor level consistent with the finished ground floor level of the dwelling</p> <p>(iv) where associated with a residential use, retains a total area of soft landscaping for the entire development site, including any common property, with a minimum dimension of 700mm in accordance with (A) or (B), whichever is less:</p> <p style="margin-left: 20px;">A. a total area is determined by the following table:</p> <table border="1" style="margin-left: 40px; border-collapse: collapse; width: 80%;"> <thead> <tr style="background-color: #1a3d54; color: white;"> <th style="padding: 5px;">Site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="padding: 5px;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;"><150</td> <td style="padding: 5px;">10%</td> </tr> <tr> <td style="padding: 5px;">150-200</td> <td style="padding: 5px;">15%</td> </tr> <tr> <td style="padding: 5px;">>200-450</td> <td style="padding: 5px;">20%</td> </tr> <tr> <td style="padding: 5px;">>450</td> <td style="padding: 5px;">25%</td> </tr> </tbody> </table> <p style="margin-left: 20px;">B. the amount of existing soft landscaping prior to the development occurring.</p> <p>(b) where in association with a non-residential use:</p> <ul style="list-style-type: none"> (i) are set back at least 2 metres from the boundary of an allotment used for residential purposes. (ii) are set back at least 2 metres from a public road. (iii) have a floor area not exceeding 25m² <p>(c) in all cases, has a finished floor level not exceeding 1 metre above natural ground level at any point.</p>	Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	150-200	15%	>200-450	20%	>450	25%
Site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site										
<150	10%										
150-200	15%										
>200-450	20%										
>450	25%										
<p>PO 45.2</p> <p>Decks are designed and sited to minimise direct overlooking of habitable rooms and private open spaces of adjoining residential uses in neighbourhood-type zones through suitable floor levels, screening and siting taking into account the slope of the subject land, existing vegetation on the subject land, and fencing.</p>	<p>DTS/DPF 45.2</p> <p>Decks with a finished floor level/s 500mm or more above natural ground level facing side or rear boundaries shared with a residential use in a neighbourhood-type zone incorporate screening with a maximum of 25% transparency/openings, permanently fixed to the outer edge of the deck not less than 1.5 m above the finished floor level/s.</p>										
<p>PO 45.3</p> <p>Decks used for outdoor dining, entertainment or other commercial uses provide carparking in accordance with the primary use of the deck.</p>	<p>DTS/DPF 45.3</p> <p>Decks used for commercial purposes do not result in less on-site car parking for the primary use of the subject land than specified in Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements or Table 2 - Off-Street Car Parking Requirements in Designated Areas.</p>										

Table 1 - Private Open Space

Dwelling Type	Dwelling / Site Configuration	Minimum Rate
Dwelling (at ground level, other than a residential flat building that includes above ground dwellings)		Total private open space area: (a) Site area <301m ² : 24m ² located behind the building line. (b) Site area ≥ 301m ² : 60m ² located behind the building line. Minimum directly accessible from a living room: 16m ² /

		with a minimum dimension 3m.
Cabin or caravan (permanently fixed to the ground) in a residential park or caravan and tourist park		Total area: 16m ² , which may be used as second car parking space, provided on each site intended for residential occupation.
Dwelling in a residential flat building or mixed use building which incorporate above ground level dwellings	Dwellings at ground level:	15m ² / minimum dimension 3m
	Dwellings above ground level:	
	Studio (no separate bedroom)	4m ² / minimum dimension 1.8m
	One bedroom dwelling	8m ² / minimum dimension 2.1m
	Two bedroom dwelling	11m ² / minimum dimension 2.4m
	Three + bedroom dwelling	15 m ² / minimum dimension 2.6m

Forestry

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Commercial forestry is designed and sited to maximise economic benefits whilst managing potential negative impacts on the environment, transport networks, surrounding land uses and landscapes.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1 Commercial forestry plantations are established where there is no detrimental effect on the physical environment or scenic quality of the rural landscape.	DTS/DPF 1.1 None are applicable.
PO 1.2 Commercial forestry plantations are established on slopes that are stable to minimise the risk of soil erosion.	DTS/DPF 1.2 Commercial forestry plantations are not located on land with a slope exceeding 20% (1-in-5).
PO 1.3 Commercial forestry plantations and operations associated with their establishment, management and harvesting are appropriately set back from any sensitive receiver to minimise fire risk and noise disturbance.	DTS/DPF 1.3 Commercial forestry plantations and operations associated with their establishment, management and harvesting are set back 50m or more from any sensitive receiver.
Water Protection	
PO 2.1	DTS/DPF 2.1

<p>Commercial forestry plantations incorporate artificial drainage lines (i.e. culverts, runoffs and constructed drains) integrated with natural drainage lines to minimise concentrated water flows onto or from plantation areas.</p>	<p>None are applicable.</p>																					
<p>PO 2.2</p> <p>Appropriate siting, layout and design measures are adopted to minimise the impact of commercial forestry plantations on surface water resources.</p>	<p>DTS/DPF 2.2</p> <p>Commercial forestry plantations:</p> <ul style="list-style-type: none"> (a) do not involve cultivation (excluding spot cultivation) in drainage lines (b) are set back 20m or more from the banks of any major watercourse (a third order or higher watercourse), lake, reservoir, wetland or sinkhole (with direct connection to an aquifer) (c) are set back 10m or more from the banks of any first or second order watercourse or sinkhole (with no direct connection to an aquifer). 																					
<p>Fire Management</p>																						
<p>PO 3.1</p> <p>Commercial forestry plantations incorporate appropriate firebreaks and fire management design elements.</p>	<p>DTS/DPF 3.1</p> <p>Commercial forestry plantations provide:</p> <ul style="list-style-type: none"> (a) 7m or more wide external boundary firebreaks for plantations of 40ha or less (b) 10m or more wide external boundary firebreaks for plantations of between 40ha and 100ha (c) 20m or more wide external boundary firebreaks, or 10m with an additional 10m or more of fuel-reduced plantation, for plantations of 100ha or greater. <p>Note: Firebreaks prescribed above (as well as access tracks) may be included within the setback buffer distances prescribed by other policies of the Code.</p>																					
<p>PO 3.2</p> <p>Commercial forestry plantations incorporate appropriate fire management access tracks.</p>	<p>DTS/DPF 3.2</p> <p>Commercial forestry plantation fire management access tracks:</p> <ul style="list-style-type: none"> (a) are incorporated within all firebreaks (b) are 7m or more wide with a vertical clearance of 4m or more (c) are aligned to provide straight through access at junctions, or if they are a no through access track are appropriately signposted and provide suitable turnaround areas for fire-fighting vehicles (d) partition the plantation into units of 40ha or less in area. 																					
<p>Power-line Clearances</p>																						
<p>PO 4.1</p> <p>Commercial forestry plantations achieve and maintain appropriate clearances from aboveground powerlines.</p>	<p>DTS/DPF 4.1</p> <p>Commercial forestry plantations incorporating trees with an expected mature height of greater than 6m meet the clearance requirements listed in the following table:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="width: 33%;">Voltage of transmission line</th> <th style="width: 15%;">Tower or Pole</th> <th style="width: 52%;">Minimum horizontal clearance distance between plantings and transmission lines</th> </tr> </thead> <tbody> <tr> <td>500 kV</td> <td>Tower</td> <td>38m</td> </tr> <tr> <td>275 kV</td> <td>Tower</td> <td>25m</td> </tr> <tr> <td>132 kV</td> <td>Tower</td> <td>30m</td> </tr> <tr> <td>132 kV</td> <td>Pole</td> <td>20m</td> </tr> <tr> <td>66 kV</td> <td>Pole</td> <td>20m</td> </tr> <tr> <td>Less than 66 kV</td> <td>Pole</td> <td>20m</td> </tr> </tbody> </table>	Voltage of transmission line	Tower or Pole	Minimum horizontal clearance distance between plantings and transmission lines	500 kV	Tower	38m	275 kV	Tower	25m	132 kV	Tower	30m	132 kV	Pole	20m	66 kV	Pole	20m	Less than 66 kV	Pole	20m
Voltage of transmission line	Tower or Pole	Minimum horizontal clearance distance between plantings and transmission lines																				
500 kV	Tower	38m																				
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132 kV	Pole	20m																				
66 kV	Pole	20m																				
Less than 66 kV	Pole	20m																				

Housing Renewal

The Housing Renewal General Development Policies are only applicable to dwellings or residential flat building undertaken by:

- (a) the South Australian Housing Trust either individually or jointly with other persons or bodies
or
(b) a provider registered under the Community Housing National Law participating in a program relating to the renewal of housing endorsed by the South Australian Housing Trust.

Desired Outcome (DO)

Desired Outcome	
DO 1	Renewed residential environments replace older social housing and provide new social housing infrastructure and other housing options and tenures to enhance the residential amenity of the local area.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
<p>PO 1.1</p> <p>Residential development provides a range of housing choices.</p>	<p>DTS/DPF 1.1</p> <p>Development comprises one or more of the following:</p> <ul style="list-style-type: none"> (a) detached dwellings (b) semi-detached dwellings (c) row dwellings (d) group dwellings (e) residential flat buildings.
<p>PO 1.2</p> <p>Medium-density housing options or higher are located in close proximity to public transit, open space and/or activity centres.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
Building Height	
<p>PO 2.1</p> <p>Buildings generally do not exceed 3 building levels unless in locations close to public transport, centres and/or open space.</p>	<p>DTS/DPF 2.1</p> <p>Building height (excluding garages, carports and outbuildings) does not exceed 3 building levels and 12m and wall height does not exceed 9m (not including a gable end).</p>
<p>PO 2.2</p> <p>Medium or high rise residential flat buildings located within or at the interface with zones which restrict heights to a maximum of 2 building levels transition down in scale and height towards the boundary of that zone, other than where it is a street boundary.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
Primary Street Setback	
<p>PO 3.1</p> <p>Buildings are set back from the primary street boundary to contribute to an attractive streetscape character.</p>	<p>DTS/DPF 3.1</p> <p>Buildings are no closer to the primary street (excluding any balcony, verandah, porch, awning or similar structure) than 3m.</p>
Secondary Street Setback	
<p>PO 4.1</p> <p>Buildings are set back from secondary street boundaries to maintain separation between building walls and public streets and contribute to a suburban streetscape character.</p>	<p>DTS/DPF 4.1</p> <p>Buildings are set back at least 900mm from the boundary of the allotment with a secondary street frontage.</p>

Boundary Walls	
<p>PO 5.1</p> <p>Boundary walls are limited in height and length to manage visual impacts and access to natural light and ventilation.</p>	<p>DTS/DPF 5.1</p> <p>Except where the dwelling is located on a central site within a row dwelling or terrace arrangement, dwellings with side boundary walls are sited on only one side boundary and satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) adjoin or abut a boundary wall of a building on adjoining land for the same length and height (b) do not: <ul style="list-style-type: none"> (i) exceed 3.2m in height from the lower of the natural or finished ground level (ii) exceed 11.5m in length (iii) when combined with other walls on the boundary of the subject development site, a maximum 45% of the length of the boundary (iv) encroach within 3 metres of any other existing or proposed boundary walls on the subject land.
<p>PO 5.2</p> <p>Dwellings in a semi-detached, row or terrace arrangement maintain space between buildings consistent with a suburban streetscape character.</p>	<p>DTS/DPF 5.2</p> <p>Dwellings in a semi-detached or row arrangement are set back 900mm or more from side boundaries shared with allotments outside the development site, except for a carport or garage.</p>
Side Boundary Setback	
<p>PO 6.1</p> <p>Buildings are set back from side boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between dwellings in a way that contributes to a suburban character (b) access to natural light and ventilation for neighbours. 	<p>DTS/DPF 6.1</p> <p>Other than walls located on a side boundary, buildings are set back from side boundaries in accordance with the following:</p> <ul style="list-style-type: none"> (a) where the wall height does not exceed 3m - at least 900mm (b) for a wall that is not south facing and the wall height exceeds 3m - at least 900mm from the boundary of the site plus a distance of 1/3 of the extent to which the height of the wall exceeds 3m from the top of the footings (c) for a wall that is south facing and the wall height exceeds 3m - at least 1.9m from the boundary of the site plus a distance of 1/3 of the extent to which the height of the wall exceeds 3m from the top of the footings.
Rear Boundary Setback	
<p>PO 7.1</p> <p>Buildings are set back from rear boundaries to provide:</p> <ul style="list-style-type: none"> (a) separation between dwellings in a way that contributes to a suburban character (b) access to natural light and ventilation for neighbours (c) private open space (d) space for landscaping and vegetation. 	<p>DTS/DPF 7.1</p> <p>Dwellings are set back from the rear boundary:</p> <ul style="list-style-type: none"> (a) 3m or more for the first building level (b) 5m or more for any subsequent building level.
Buildings elevation design	
<p>PO 8.1</p> <p>Dwelling elevations facing public streets and common driveways make a positive contribution to the streetscape and common driveway areas.</p>	<p>DTS/DPF 8.1</p> <p>Each dwelling includes at least 3 of the following design features within the building elevation facing a primary street, and at least 2 of the following design features within the building elevation facing any other public road (other than a laneway) or a common driveway:</p> <ul style="list-style-type: none"> (a) a minimum of 30% of the building elevation is set back an additional 300mm from the building line (b) a porch or portico projects at least 1m from the building elevation (c) a balcony projects from the building elevation (d) a verandah projects at least 1m from the building elevation (e) eaves of a minimum 400mm width extend along the width of the front elevation (f) a minimum 30% of the width of the upper level projects forward from the lower level primary building line by at least 300mm. (g) a minimum of two different materials or finishes are incorporated on the walls of the building elevation, with a maximum of 80% of the building elevation in a single material or finish.
<p>PO 8.2</p> <p>Dwellings incorporate windows along primary street frontages to encourage passive</p>	<p>DTS/DPF 8.2</p> <p>Each dwelling with a frontage to a public street:</p>

<p>surveillance and make a positive contribution to the streetscape.</p>	<p>(a) includes at least one window facing the primary street from a habitable room that has a minimum internal room dimension of 2.4m</p> <p>(b) has an aggregate window area of at least 2m² facing the primary street</p>															
<p>PO 8.3</p> <p>The visual mass of larger buildings is reduced when viewed from adjoining allotments or public streets.</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>															
<p>PO 8.4</p> <p>Built form considers local context and provides a quality design response through scale, massing, materials, colours and architectural expression.</p>	<p>DTS/DPF 8.4</p> <p>None are applicable.</p>															
<p>PO 8.5</p> <p>Entrances to multi-storey buildings are:</p> <p>(a) oriented towards the street</p> <p>(b) visible and easily identifiable from the street</p> <p>(c) designed to include a common mail box structure.</p>	<p>DTS/DPF 8.5</p> <p>None are applicable.</p>															
<p>Outlook and amenity</p>																
<p>PO 9.1</p> <p>Living rooms have an external outlook to provide a high standard of amenity for occupants.</p>	<p>DTS/DPF 9.1</p> <p>A living room of a dwelling incorporates a window with an external outlook towards the street frontage or private open space.</p>															
<p>PO 9.2</p> <p>Bedrooms are separated or shielded from active communal recreation areas, common access areas and vehicle parking areas and access ways to mitigate noise and artificial light intrusion.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>															
<p>Private Open Space</p>																
<p>PO 10.1</p> <p>Dwellings are provided with suitable sized areas of usable private open space to meet the needs of occupants.</p>	<p>DTS/DPF 10.1</p> <p>Private open space is provided in accordance with the following table:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr style="background-color: #2c3e50; color: white;"> <th style="text-align: left;">Dwelling Type</th> <th style="text-align: left;">Dwelling / Site Configuration</th> <th style="text-align: left;">Minimum Rate</th> </tr> </thead> <tbody> <tr> <td style="vertical-align: top;">Dwelling (at ground level)</td> <td></td> <td>Total area: 24m² located behind the building line Minimum adjacent to a living room: 16m² with a minimum dimension 3m</td> </tr> <tr> <td rowspan="4" style="vertical-align: top;">Dwelling (above ground level)</td> <td style="vertical-align: top;">Studio</td> <td>4m² / minimum dimension 1.8m</td> </tr> <tr> <td style="vertical-align: top;">One bedroom dwelling</td> <td>8m² / minimum dimension 2.1m</td> </tr> <tr> <td style="vertical-align: top;">Two bedroom dwelling</td> <td>11m² / minimum dimension 2.4m</td> </tr> <tr> <td style="vertical-align: top;">Three + bedroom dwelling</td> <td>15 m² / minimum dimension 2.6m</td> </tr> </tbody> </table>	Dwelling Type	Dwelling / Site Configuration	Minimum Rate	Dwelling (at ground level)		Total area: 24m ² located behind the building line Minimum adjacent to a living room: 16m ² with a minimum dimension 3m	Dwelling (above ground level)	Studio	4m ² / minimum dimension 1.8m	One bedroom dwelling	8m ² / minimum dimension 2.1m	Two bedroom dwelling	11m ² / minimum dimension 2.4m	Three + bedroom dwelling	15 m ² / minimum dimension 2.6m
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<p>PO 10.2</p> <p>Private open space positioned to provide convenient access from internal living areas.</p>	<p>DTS/DPF 10.2</p> <p>At least 50% of the required area of private open space is accessible from a habitable room.</p>										
<p>PO 10.3</p> <p>Private open space is positioned and designed to:</p> <p>(a) provide useable outdoor space that suits the needs of occupants;</p> <p>(b) take advantage of desirable orientation and vistas; and</p> <p>(c) adequately define public and private space.</p>	<p>DTS/DPF 10.3</p> <p>None are applicable.</p>										
<p>Visual privacy</p>											
<p>PO 11.1</p> <p>Development mitigates direct overlooking from upper level windows to habitable rooms and private open spaces of adjoining residential uses.</p>	<p>DTS/DPF 11.1</p> <p>Upper level windows facing side or rear boundaries shared with another residential allotment/site satisfy one of the following:</p> <p>(a) are permanently obscured to a height of 1.5m above finished floor level and are fixed or not capable of being opened more than 200mm</p> <p>(b) have sill heights greater than or equal to 1.5m above finished floor level</p> <p>(c) incorporate screening with a maximum of 25% openings, permanently fixed no more than 500mm from the window surface and sited adjacent to any part of the window less than 1.5m above the finished floor.</p>										
<p>PO 11.2</p> <p>Development mitigates direct overlooking from upper level balconies and terraces to habitable rooms and private open space of adjoining residential uses.</p>	<p>DTS/DPF 11.2</p> <p>One of the following is satisfied:</p> <p>(a) the longest side of the balcony or terrace will face a public road, public road reserve or public reserve that is at least 15m wide in all places faced by the balcony or terrace</p> <p>or</p> <p>(b) all sides of balconies or terraces on upper building levels are permanently obscured by screening with a maximum 25% transparency/openings fixed to a minimum height of:</p> <p>(i) 1.5m above finished floor level where the balcony is located at least 15 metres from the nearest habitable window of a dwelling on adjacent land</p> <p>or</p> <p>(ii) 1.7m above finished floor level in all other cases</p>										
<p>Landscaping</p>											
<p>PO 12.1</p> <p>Soft landscaping is incorporated into development to:</p> <p>(a) minimise heat absorption and reflection</p> <p>(b) maximise shade and shelter</p> <p>(c) maximise stormwater infiltration and biodiversity</p> <p>(d) enhance the appearance of land and streetscapes.</p>	<p>DTS/DPF 12.1</p> <p>Residential development incorporates pervious areas for soft landscaping with a minimum dimension of 700mm provided in accordance with (a) and (b):</p> <p>(a) a total area as determined by the following table:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="text-align: left;">Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m²)</th> <th style="text-align: left;">Minimum percentage of site</th> </tr> </thead> <tbody> <tr> <td><150</td> <td>10%</td> </tr> <tr> <td><200</td> <td>15%</td> </tr> <tr> <td>200-450</td> <td>20%</td> </tr> <tr> <td>>450</td> <td>25%</td> </tr> </tbody> </table> <p>(b) at least 30% of land between the road boundary and the building line.</p>	Dwelling site area (or in the case of residential flat building or group dwelling(s), average site area) (m ²)	Minimum percentage of site	<150	10%	<200	15%	200-450	20%	>450	25%
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<p>Water Sensitive Design</p>											
<p>PO 13.1</p> <p>Residential development is designed to capture and use stormwater to:</p> <p>(a) maximise efficient use of water resources</p> <p>(b) manage peak stormwater runoff flows and volume to ensure the carrying capacities of downstream systems are not overloaded</p> <p>(c) manage runoff quality to maintain, as close as practical, pre-development conditions.</p>	<p>DTS/DPF 13.1</p> <p>None are applicable.</p>										
<p>Car Parking</p>											
<p>PO 14.1</p>	<p>DTS/DPF 14.1</p>										

<p>On-site car parking is provided to meet the anticipated demand of residents, with less on-site parking in areas in close proximity to public transport.</p>	<p>On-site car parking is provided at the following rates per dwelling:</p> <ul style="list-style-type: none"> (a) 2 or fewer bedrooms - 1 car parking space (b) 3 or more bedrooms - 2 car parking spaces.
<p>PO 14.2</p> <p>Enclosed car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 14.2</p> <p>Residential parking spaces enclosed by fencing, walls or other obstructions with the following internal dimensions (separate from any waste storage area):</p> <ul style="list-style-type: none"> (a) single parking spaces: <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 3.0m (iii) a minimum garage door width of 2.4m (b) double parking spaces (side by side): <ul style="list-style-type: none"> (i) a minimum length of 5.4m (ii) a minimum width of 5.5m (iii) minimum garage door width of 2.4m per space.
<p>PO 14.3</p> <p>Uncovered car parking spaces are of dimensions to be functional, accessible and convenient.</p>	<p>DTS/DPF 14.3</p> <p>Uncovered car parking spaces have:</p> <ul style="list-style-type: none"> (a) a minimum length of 5.4m (b) a minimum width of 2.4m (c) a minimum width between the centre line of the space and any fence, wall or other obstruction of 1.5m.
<p>PO 14.4</p> <p>Residential flat buildings and group dwelling developments provide sufficient on-site visitor car parking to cater for anticipated demand.</p>	<p>DTS/DPF 14.4</p> <p>Visitor car parking for group and residential flat buildings incorporating 4 or more dwellings is provided on-site at a minimum ratio of 0.25 car parking spaces per dwelling.</p>
<p>PO 14.5</p> <p>Residential flat buildings provide dedicated areas for bicycle parking.</p>	<p>DTS/DPF 14.5</p> <p>Residential flat buildings provide one bicycle parking space per dwelling.</p>
<p>Overshadowing</p>	
<p>PO 15.1</p> <p>Development minimises overshadowing of the private open spaces of adjoining land by ensuring that ground level open space associated with residential buildings receive direct sunlight for a minimum of 2 hours between 9am and 3pm on 21 June.</p>	<p>DTS/DPF 15.1</p> <p>None are applicable.</p>
<p>Waste</p>	
<p>PO 16.1</p> <p>Provision is made for the convenient storage of waste bins in a location screened from public view.</p>	<p>DTS/DPF 16.1</p> <p>A waste bin storage area is provided behind the primary building line that:</p> <ul style="list-style-type: none"> (a) has a minimum area of 2m² with a minimum dimension of 900mm (separate from any designated car parking spaces or private open space).; and (b) has a continuous unobstructed path of travel (excluding moveable objects like gates, vehicles and roller doors) with a minimum width of 800mm between the waste bin storage area and the street.
<p>PO 16.2</p> <p>Residential flat buildings provide a dedicated area for the on-site storage of waste which is:</p> <ul style="list-style-type: none"> (a) easily and safely accessible for residents and for collection vehicles (b) screened from adjoining land and public roads (c) of sufficient dimensions to be able to accommodate the waste storage needs of the development considering the intensity and nature of the development and the frequency of collection. 	<p>DTS/DPF 16.2</p> <p>None are applicable.</p>
<p>Vehicle Access</p>	

<p>PO 17.1</p> <p>Driveways are located and designed to facilitate safe access and egress while maximising land available for street tree planting, landscaped street frontages and on-street parking.</p>	<p>DTS/DPF 17.1</p> <p>None are applicable.</p>
<p>PO 17.2</p> <p>Vehicle access is safe, convenient, minimises interruption to the operation of public roads and does not interfere with street infrastructure or street trees.</p>	<p>DTS/DPF 17.2</p> <p>Vehicle access to designated car parking spaces satisfy (a) or (b):</p> <ul style="list-style-type: none"> (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: <ul style="list-style-type: none"> (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
<p>PO 17.3</p> <p>Driveways are designed to enable safe and convenient vehicle movements from the public road to on-site parking spaces.</p>	<p>DTS/DPF 17.3</p> <p>Driveways are designed and sited so that:</p> <ul style="list-style-type: none"> (a) the gradient of the driveway does not exceed a grade of 1 in 4 and includes transitions to ensure a maximum grade change of 12.5% (1 in 8) for summit changes, and 15% (1 in 6.7) for sag changes, in accordance with AS 2890.1:2004 to prevent vehicles bottoming or scraping (b) the centreline of the driveway has an angle of no less than 70 degrees and no more than 110 degrees from the street boundary to which it takes its access as shown in the following diagram: <div style="text-align: center; margin: 10px 0;"> <p style="text-align: center;">CENTRE LINE OF DRIVEWAY TO BE BETWEEN 70° TO 110° OFF THE STREET BOUNDARY</p> </div> <ul style="list-style-type: none"> (c) if located to provide access from an alley, lane or right of way - the alley, land or right of way is at least 6.2m wide along the boundary of the allotment / site.
<p>PO 17.4</p> <p>Driveways and access points are designed and distributed to optimise the provision of on-street parking.</p>	<p>DTS/DPF 17.4</p> <p>Where on-street parking is available abutting the site's street frontage, on-street parking is retained in accordance with the following requirements:</p> <ul style="list-style-type: none"> (a) minimum 0.33 on-street spaces per dwelling on the site (rounded up to the nearest whole number) (b) minimum car park length of 5.4m where a vehicle can enter or exit a space directly (c) minimum carpark length of 6m for an intermediate space located between two other parking spaces or to an end obstruction where the parking is indented.
<p>PO 17.5</p>	<p>DTS/DPF 17.5</p>

Residential driveways that service more than one dwelling of a dimension to allow safe and convenient movement.	Driveways that service more than 1 dwelling or a dwelling on a battle-axe site: (a) have a minimum width of 3m (b) for driveways servicing more than 3 dwellings: (i) have a width of 5.5m or more and a length of 6m or more at the kerb of the primary street (ii) where the driveway length exceeds 30m, incorporate a passing point at least every 30 metres with a minimum width of 5.5m and a minimum length of 6m.
PO 17.6 Residential driveways that service more than one dwelling are designed to allow passenger vehicles to enter and exit the site and manoeuvre within the site in a safe and convenient manner.	DTS/DPF 17.6 Driveways providing access to more than one dwelling, or a dwelling on a battle-axe site, allow a B85 passenger vehicle to enter and exit the garages or parking spaces in no more than a three-point turn manoeuvre
PO 17.7 Dwellings are adequately separated from common driveways and manoeuvring areas.	DTS/DPF 17.7 Dwelling walls with entry doors or ground level habitable room windows are set back at least 1.5m from any driveway or area designated for the movement and manoeuvring of vehicles.
Storage	
PO 18.1 Dwellings are provided with sufficient and accessible space for storage to meet likely occupant needs.	DTS/DPF 18.1 Dwellings are provided with storage at the following rates and 50% or more of the storage volume is provided within the dwelling: (a) studio: not less than 6m ³ (b) 1 bedroom dwelling / apartment: not less than 8m ³ (c) 2 bedroom dwelling / apartment: not less than 10m ³ (d) 3+ bedroom dwelling / apartment: not less than 12m ³ .
Earthworks	
PO 19.1 Development, including any associated driveways and access tracks, minimises the need for earthworks to limit disturbance to natural topography.	DTS/DPF 19.1 The development does not involve: (a) excavation exceeding a vertical height of 1m or (b) filling exceeding a vertical height of 1m or (c) a total combined excavation and filling vertical height exceeding 2m.
Service connections and infrastructure	
PO 20.1 Dwellings are provided with appropriate service connections and infrastructure.	DTS/DPF 20.1 The site and building: (a) have the ability to be connected to a permanent potable water supply (b) have the ability to be connected to a sewerage system, or a wastewater system approved under the <i>South Australian Public Health Act 2011</i> (c) have the ability to be connected to electricity supply (d) have the ability to be connected to an adequate water supply (and pressure) for fire-fighting purposes (e) would not be contrary to the Regulations prescribed for the purposes of Section 86 of the <i>Electricity Act 1996</i> .
Site contamination	
PO 21.1 Land that is suitable for sensitive land uses to provide a safe environment.	DTS/DPF 21.1 Development satisfies (a), (b), (c) or (d): (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a <u>more sensitive use</u> (c) involves a change in the use of land to a <u>more sensitive use</u> on land at which <u>site contamination</u> does not exist (as demonstrated in a <u>site contamination declaration form</u>) (d) involves a change in the use of land to a <u>more sensitive use</u> on land at which <u>site contamination</u> exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: (i) a <u>site contamination audit report</u> has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that

	<p>A. <u>site contamination</u> does not exist (or no longer exists) at the land or</p> <p>B. the land is suitable for the proposed use or range of uses (without the need for any further <u>remediation</u>) or</p> <p>C. where <u>remediation</u> is, or remains, necessary for the proposed use (or range of uses), <u>remediation work</u> has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development)</p> <p>and</p> <p>(ii) no other <u>class 1 activity</u> or <u>class 2 activity</u> has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a <u>site contamination declaration form</u>).</p>
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Infrastructure and Renewable Energy Facilities

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Efficient provision of infrastructure networks and services, renewable energy facilities and ancillary development in a manner that minimises hazard, is environmentally and culturally sensitive and manages adverse visual impacts on natural and rural landscapes and residential amenity.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
General	
<p>PO 1.1</p> <p>Development is located and designed to minimise hazard or nuisance to adjacent development and land uses.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
Visual Amenity	
<p>PO 2.1</p> <p>The visual impact of above-ground infrastructure networks and services (excluding high voltage transmission lines), renewable energy facilities (excluding wind farms), energy storage facilities and ancillary development is minimised from townships, scenic routes and public roads by:</p> <ul style="list-style-type: none"> (a) utilising features of the natural landscape to obscure views where practicable (b) siting development below ridgelines where practicable (c) avoiding visually sensitive and significant landscapes (d) using materials and finishes with low-reflectivity and colours that complement the surroundings (e) using existing vegetation to screen buildings (f) incorporating landscaping or landscaped mounding around the perimeter of a site and between adjacent allotments accommodating or zoned to primarily accommodate sensitive receivers. 	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Pumping stations, battery storage facilities, maintenance sheds and other ancillary structures incorporate vegetation buffers to reduce adverse visual impacts on adjacent land.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p>	<p>DTS/DPF 2.3</p>

Surfaces exposed by earthworks associated with the installation of storage facilities, pipework, penstock, substations and other ancillary plant are reinstated and revegetated to reduce adverse visual impacts on adjacent land.	None are applicable.
Rehabilitation	
PO 3.1 Progressive rehabilitation (incorporating revegetation) of disturbed areas, ahead of or upon decommissioning of areas used for renewable energy facilities and transmission corridors.	DTS/DPF 3.1 None are applicable.
Hazard Management	
PO 4.1 Infrastructure and renewable energy facilities and ancillary development located and operated to not adversely impact maritime or air transport safety, including the operation of ports, airfields and landing strips.	DTS/DPF 4.1 None are applicable.
PO 4.2 Facilities for energy generation, power storage and transmission are separated as far as practicable from dwellings, tourist accommodation and frequently visited public places (such as viewing platforms / lookouts) to reduce risks to public safety from fire or equipment malfunction.	DTS/DPF 4.2 None are applicable.
PO 4.3 Bushfire hazard risk is minimised for renewable energy facilities by providing appropriate access tracks, safety equipment and water tanks and establishing cleared areas around substations, battery storage and operations compounds.	DTS/DPF 4.3 None are applicable.
Electricity Infrastructure and Battery Storage Facilities	
PO 5.1 Electricity infrastructure is located to minimise visual impacts through techniques including: (a) siting utilities and services: (i) on areas already cleared of native vegetation (ii) where there is minimal interference or disturbance to existing native vegetation or biodiversity (b) grouping utility buildings and structures with non-residential development, where practicable.	DTS/DPF 5.1 None are applicable.
PO 5.2 Electricity supply (excluding transmission lines) serving new development in urban areas and townships installed underground, excluding lines having a capacity exceeding or equal to 33kV.	DTS/DPF 5.2 None are applicable.
PO 5.3 Battery storage facilities are co-located with substation infrastructure where practicable to minimise the development footprint and reduce environmental impacts.	DTS/DPF 5.3 None are applicable.
Telecommunication Facilities	
PO 6.1 The proliferation of telecommunications facilities in the form of towers/monopoles in any one locality is managed, where technically feasible, by co-locating a facility with other communications facilities to mitigate impacts from clutter on visual amenity.	DTS/DPF 6.1 None are applicable.

<p>PO 6.2</p> <p>Telecommunications antennae are located as close as practicable to support structures to manage overall bulk and mitigate impacts on visual amenity.</p>	<p>DTS/DPF 6.2</p> <p>None are applicable.</p>
<p>PO 6.3</p> <p>Telecommunications facilities, particularly towers/monopoles, are located and sized to mitigate visual impacts by the following methods:</p> <p>(a) where technically feasible, incorporating the facility within an existing structure that may serve another purpose</p> <p>or all of the following:</p> <p>(b) using existing buildings and landscape features to obscure or interrupt views of a facility from nearby public roads, residential areas and places of high public amenity to the extent practical without unduly hindering the effective provision of telecommunications services</p> <p>(c) using materials and finishes that complement the environment</p> <p>(d) screening using landscaping and vegetation, particularly for equipment shelters and huts.</p>	<p>DTS/DPF 6.3</p> <p>None are applicable.</p>
Renewable Energy Facilities	
<p>PO 7.1</p> <p>Renewable energy facilities are located as close as practicable to existing transmission infrastructure to facilitate connections and minimise environmental impacts as a result of extending transmission infrastructure.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
Renewable Energy Facilities (Wind Farm)	
<p>PO 8.1</p> <p>Visual impact of wind turbine generators on the amenity of residential and tourist development is reduced through appropriate separation.</p>	<p>DTS/DPF 8.1</p> <p>Wind turbine generators are:</p> <p>(a) set back at least 2000m from the base of a turbine to any of the following zones:</p> <ul style="list-style-type: none"> (i) Rural Settlement Zone (ii) Township Zone (iii) Rural Living Zone (iv) Rural Neighbourhood Zone <p>with an additional 10m setback per additional metre over 150m overall turbine height (measured from the base of the turbine).</p> <p>(b) set back at least 1500m from the base of the turbine to non-associated (non-stakeholder) dwellings and tourist accommodation</p>
<p>PO 8.2</p> <p>The visual impact of wind turbine generators on natural landscapes is managed by:</p> <p>(a) designing wind turbine generators to be uniform in colour, size and shape</p> <p>(b) coordinating blade rotation and direction</p> <p>(c) mounting wind turbine generators on tubular towers as opposed to lattice towers.</p>	<p>DTS/DPF 8.2</p> <p>None are applicable.</p>
<p>PO 8.3</p> <p>Wind turbine generators and ancillary development minimise potential for bird and bat strike.</p>	<p>DTS/DPF 8.3</p> <p>None are applicable.</p>
<p>PO 8.4</p> <p>Wind turbine generators incorporate recognition systems or physical markers to minimise the risk to aircraft operations.</p>	<p>DTS/DPF 8.4</p> <p>No Commonwealth air safety (CASA / ASA) or Defence requirement is applicable.</p>
<p>PO 8.5</p> <p>Meteorological masts and guidewires are identifiable to aircraft through the use of colour bands, marker balls, high visibility sleeves or flashing strobes.</p>	<p>DTS/DPF 8.5</p> <p>None are applicable.</p>
Renewable Energy Facilities (Solar Power)	

<p>PO 9.1</p> <p>Ground mounted solar power facilities generating 5MW or more are not located on land requiring the clearance of areas of intact native vegetation or on land of high environmental, scenic or cultural value.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>																																			
<p>PO 9.2</p> <p>Ground mounted solar power facilities allow for movement of wildlife by:</p> <p>(a) incorporating wildlife corridors and habitat refuges (b) avoiding the use of extensive security or perimeter fencing or incorporating fencing that enables the passage of small animals without unreasonably compromising the security of the facility.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>																																			
<p>PO 9.3</p> <p>Amenity impacts of solar power facilities are minimised through separation from conservation areas and sensitive receivers in other ownership.</p>	<p>DTS/DPF 9.3</p> <p>Ground mounted solar power facilities are set back from land boundaries, conservation areas and relevant zones in accordance with the following criteria:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="text-align: center;">Generation Capacity</th> <th style="text-align: center;">Approximate size of array</th> <th style="text-align: center;">Setback from adjoining land boundary</th> <th style="text-align: center;">Setback from conservation areas</th> <th style="text-align: center;">Setback from Township, Rural Settlement, Rural Neighbourhood and Rural Living Zones¹</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">50MW></td> <td style="text-align: center;">80ha+</td> <td style="text-align: center;">30m</td> <td style="text-align: center;">500m</td> <td style="text-align: center;">2km</td> </tr> <tr> <td style="text-align: center;">10MW<50MW</td> <td style="text-align: center;">16ha<80ha</td> <td style="text-align: center;">25m</td> <td style="text-align: center;">500m</td> <td style="text-align: center;">1.5km</td> </tr> <tr> <td style="text-align: center;">5MW<10MW</td> <td style="text-align: center;">8ha to <16ha</td> <td style="text-align: center;">20m</td> <td style="text-align: center;">500m</td> <td style="text-align: center;">1km</td> </tr> <tr> <td style="text-align: center;">1MW<5MW</td> <td style="text-align: center;">1.6ha to <8ha</td> <td style="text-align: center;">15m</td> <td style="text-align: center;">500m</td> <td style="text-align: center;">500m</td> </tr> <tr> <td style="text-align: center;">100kW<1MW</td> <td style="text-align: center;">0.5ha<1.6ha</td> <td style="text-align: center;">10m</td> <td style="text-align: center;">500m</td> <td style="text-align: center;">100m</td> </tr> <tr> <td style="text-align: center;"><100kW</td> <td style="text-align: center;"><0.5ha</td> <td style="text-align: center;">5m</td> <td style="text-align: center;">500m</td> <td style="text-align: center;">25m</td> </tr> </tbody> </table> <p>Notes:</p> <p>1. Does not apply when the site of the proposed ground mounted solar power facility is located within one of these zones.</p>	Generation Capacity	Approximate size of array	Setback from adjoining land boundary	Setback from conservation areas	Setback from Township, Rural Settlement, Rural Neighbourhood and Rural Living Zones ¹	50MW>	80ha+	30m	500m	2km	10MW<50MW	16ha<80ha	25m	500m	1.5km	5MW<10MW	8ha to <16ha	20m	500m	1km	1MW<5MW	1.6ha to <8ha	15m	500m	500m	100kW<1MW	0.5ha<1.6ha	10m	500m	100m	<100kW	<0.5ha	5m	500m	25m
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<p>PO 9.4</p> <p>Ground mounted solar power facilities incorporate landscaping within setbacks from adjacent road frontages and boundaries of adjacent allotments accommodating non-host dwellings, where balanced with infrastructure access and bushfire safety considerations.</p>	<p>DTS/DPF 9.4</p> <p>None are applicable.</p>																																			
<p><small>Hydropower / Pumped Hydropower Facilities</small></p>																																				
<p>PO 10.1</p> <p>Hydropower / pumped hydropower facility storage is designed and operated to minimise the risk of storage dam failure.</p>	<p>DTS/DPF 10.1</p> <p>None are applicable.</p>																																			
<p>PO 10.2</p> <p>Hydropower / pumped hydropower facility storage is designed and operated to minimise water loss through increased evaporation or system leakage, with the incorporation of appropriate liners, dam covers, operational measures or detection systems.</p>	<p>DTS/DPF 10.2</p> <p>None are applicable.</p>																																			
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Hydropower / pumped hydropower facilities on existing or former mine sites minimise environmental impacts from site contamination, including from mine operations or water sources subject to such processes, now or in the future.	None are applicable.
Water Supply	
<p>PO 11.1</p> <p>Development is connected to an appropriate water supply to meet the ongoing requirements of the intended use.</p>	<p>DTS/DPF 11.1</p> <p>Development is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the on-going requirements of the development.</p>
<p>PO 11.2</p> <p>Dwellings are connected to a reticulated water scheme or mains water supply with the capacity to meet the requirements of the intended use. Where this is not available an appropriate rainwater tank or storage system for domestic use is provided.</p>	<p>DTS/DPF 11.2</p> <p>A dwelling is connected, or will be connected, to a reticulated water scheme or mains water supply with the capacity to meet the requirements of the development. Where this is not available it is serviced by a rainwater tank or tanks capable of holding at least 50,000 litres of water which is:</p> <p>(a) exclusively for domestic use (b) connected to the roof drainage system of the dwelling.</p>
Wastewater Services	
<p>PO 12.1</p> <p>Development is connected to an approved common wastewater disposal service with the capacity to meet the requirements of the intended use. Where this is not available an appropriate on-site service is provided to meet the ongoing requirements of the intended use in accordance with the following:</p> <p>(a) it is wholly located and contained within the allotment of the development it will service (b) in areas where there is a high risk of contamination of surface, ground, or marine water resources from on-site disposal of liquid wastes, disposal systems are included to minimise the risk of pollution to those water resources (c) septic tank effluent drainage fields and other wastewater disposal areas are located away from watercourses and flood prone, sloping, saline or poorly drained land to minimise environmental harm.</p>	<p>DTS/DPF 12.1</p> <p>Development is connected, or will be connected, to an approved common wastewater disposal service with the capacity to meet the requirements of the development. Where this is not available it is instead capable of being serviced by an on-site waste water treatment system in accordance with the following:</p> <p>(a) the system is wholly located and contained within the allotment of development it will service; and (b) the system will comply with the requirements of the South Australian Public Health Act 2011.</p>
<p>PO 12.2</p> <p>Effluent drainage fields and other wastewater disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.</p>	<p>DTS/DPF 12.2</p> <p>Development is not built on, or encroaches within, an area that is, or will be, required for a sewerage system or waste control system.</p>
Temporary Facilities	
<p>PO 13.1</p> <p>In rural and remote locations, development that is likely to generate significant waste material during construction, including packaging waste, makes provision for a temporary on-site waste storage enclosure to minimise the incidence of wind-blown litter.</p>	<p>DTS/DPF 13.1</p> <p>A waste collection and disposal service is used to dispose of the volume of waste at the rate it is generated.</p>
<p>PO 13.2</p> <p>Temporary facilities to support the establishment of renewable energy facilities (including borrow pits, concrete batching plants, laydown, storage, access roads and worker amenity areas) are sited and operated to minimise environmental impact.</p>	<p>DTS/DPF 13.2</p> <p>None are applicable.</p>

Intensive Animal Husbandry and Dairies

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development of intensive animal husbandry and dairies in locations that are protected from encroachment by sensitive receivers and in a manner that minimises their adverse effects on amenity and the environment.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting and Design	
<p>PO 1.1</p> <p>Intensive animal husbandry, dairies and associated activities are sited, designed, constructed and managed to not unreasonably impact on the environment or amenity of the locality.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Intensive animal husbandry, dairies and associated activities are sited, designed, constructed and managed to prevent the potential transmission of disease to other operations where animals are kept.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Intensive animal husbandry and associated activities such as wastewater lagoons and liquid/solid waste disposal areas are sited, designed, constructed and managed to not unreasonably impact on sensitive receivers in other ownership in terms of noise and air emissions.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>Dairies and associated activities such as wastewater lagoons and liquid/solid waste disposal areas are sited, designed, constructed and managed to not unreasonably impact on sensitive receivers in other ownership in terms of noise and air emissions.</p>	<p>DTS/DPF 1.4</p> <p>Dairies, associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities are located 500m or more from the nearest sensitive receiver in other ownership.</p>
<p>PO 1.5</p> <p>Lagoons for the storage or treatment of milking shed effluent is adequately separated from roads to minimise impacts from odour on the general public.</p>	<p>DTS/DPF 1.5</p> <p>Lagoons for the storage or treatment of milking shed effluent are set back 20m or more from public roads.</p>
Waste	
<p>PO 2.1</p> <p>Storage of manure, used litter and other wastes (other than waste water lagoons) is sited, designed, constructed and managed to:</p> <p>(a) avoid attracting and harbouring vermin (b) avoid polluting water resources (c) be located outside 1% AEP flood event areas.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
Soil and Water Protection	
<p>PO 3.1</p> <p>To avoid environmental harm and adverse effects on water resources, intensive animal husbandry operations are appropriately set back from:</p>	<p>DTS/DPF 3.1</p> <p>Intensive animal husbandry operations are set back:</p> <p>(a) 800m or more from a public water supply reservoir</p>

<p>(a) public water supply reservoirs (b) major watercourses (third order or higher stream) (c) any other watercourse, bore or well used for domestic or stock water supplies.</p>	<p>(b) 200m or more from a major watercourse (third order or higher stream) (c) 100m or more from any other watercourse, bore or well used for domestic or stock water supplies.</p>
<p>PO 3.2</p> <p>Intensive animal husbandry operations and dairies incorporate appropriately designed effluent and run-off facilities that:</p> <p>(a) have sufficient capacity to hold effluent and runoff from the operations on site (b) ensure effluent does not infiltrate and pollute groundwater, soil or other water resources.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>

Interface between Land Uses

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Development is located and designed to mitigate adverse effects on or from neighbouring and proximate land uses.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature								
<small>General Land Use Compatibility</small>									
<p>PO 1.1</p> <p>Sensitive receivers are designed and sited to protect residents and occupants from adverse impacts generated by lawfully existing land uses (or lawfully approved land uses) and land uses desired in the zone.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>								
<p>PO 1.2</p> <p>Development adjacent to a site containing a sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers is designed to minimise adverse impacts.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>								
<small>Hours of Operation</small>									
<p>PO 2.1</p> <p>Non-residential development does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) or an adjacent zone primarily for sensitive receivers through its hours of operation having regard to:</p> <p>(a) the nature of the development (b) measures to mitigate off-site impacts (c) the extent to which the development is desired in the zone (d) measures that might be taken in an adjacent zone primarily for sensitive receivers that mitigate adverse impacts without unreasonably compromising the intended use of that land.</p>	<p>DTS/DPF 2.1</p> <p>Development operating within the following hours:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="width: 50%; text-align: center;">Class of Development</th> <th style="width: 50%; text-align: center;">Hours of operation</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;">Consulting room</td> <td style="padding: 5px;">7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td style="padding: 5px;">Office</td> <td style="padding: 5px;">7am to 9pm, Monday to Friday 8am to 5pm, Saturday</td> </tr> <tr> <td style="padding: 5px;">Shop, other than any one or</td> <td style="padding: 5px;">7am to 9pm, Monday to Friday</td> </tr> </tbody> </table>	Class of Development	Hours of operation	Consulting room	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Office	7am to 9pm, Monday to Friday 8am to 5pm, Saturday	Shop, other than any one or	7am to 9pm, Monday to Friday
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	<p>combination of the following:</p> <p>(a) restaurant</p> <p>(b) cellar door in the Productive Rural Landscape Zone, Rural Zone or Rural Horticulture Zone</p>	<p>8am to 5pm, Saturday and Sunday</p>	
Overshadowing			
<p>PO 3.1</p> <p>Overshadowing of habitable room windows of adjacent residential land uses in:</p> <p>a. a neighbourhood-type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>DTS/DPF 3.1</p> <p>North-facing windows of habitable rooms of adjacent residential land uses in a neighbourhood-type zone receive at least 3 hours of direct sunlight between 9.00am and 3.00pm on 21 June.</p>		
<p>PO 3.2</p> <p>Overshadowing of the primary area of private open space or communal open space of adjacent residential land uses in:</p> <p>a. a neighbourhood type zone is minimised to maintain access to direct winter sunlight</p> <p>b. other zones is managed to enable access to direct winter sunlight.</p>	<p>DTS/DPF 3.2</p> <p>Development maintains 2 hours of direct sunlight between 9.00 am and 3.00 pm on 21 June to adjacent residential land uses in a neighbourhood-type zone in accordance with the following:</p> <p>a. for ground level private open space, the smaller of the following:</p> <p>i. half the existing ground level open space</p> <p>or</p> <p>ii. 35m² of the existing ground level open space (with at least one of the area's dimensions measuring 2.5m)</p> <p>b. for ground level communal open space, at least half of the existing ground level open space.</p>		
<p>PO 3.3</p> <p>Development does not unduly reduce the generating capacity of adjacent rooftop solar energy facilities taking into account:</p> <p>(a) the form of development contemplated in the zone</p> <p>(b) the orientation of the solar energy facilities</p> <p>(c) the extent to which the solar energy facilities are already overshadowed.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>		
<p>PO 3.4</p> <p>Development that incorporates moving parts, including windmills and wind farms, are located and operated to not cause unreasonable nuisance to nearby dwellings and tourist accommodation caused by shadow flicker.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>		
Activities Generating Noise or Vibration			
<p>PO 4.1</p> <p>Development that emits noise (other than music) does not unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.1</p> <p>Noise that affects sensitive receivers achieves the relevant Environment Protection (Noise) Policy criteria.</p>		
<p>PO 4.2</p> <p>Areas for the on-site manoeuvring of service and delivery vehicles, plant and equipment, outdoor work spaces (and the like) are designed and sited to not unreasonably impact the amenity of adjacent sensitive receivers (or lawfully approved sensitive receivers) and zones primarily intended to accommodate sensitive receivers due to noise and vibration by adopting techniques including:</p> <p>(a) locating openings of buildings and associated services away from the interface with the adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</p> <p>(b) when sited outdoors, locating such areas as far as practicable from adjacent sensitive receivers and zones primarily intended to accommodate sensitive receivers</p> <p>(c) housing plant and equipment within an enclosed structure or acoustic enclosure</p> <p>(d) providing a suitable acoustic barrier between the plant and / or equipment and the adjacent sensitive receiver boundary or zone.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>		

<p>PO 4.3</p> <p>Fixed plant and equipment in the form of pumps and/or filtration systems for a swimming pool or spa are positioned and/or housed to not cause unreasonable noise nuisance to adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.3</p> <p>The pump and/or filtration system ancillary to a dwelling erected on the same site is:</p> <p>(a) enclosed in a solid acoustic structure located at least 5m from the nearest habitable room located on an adjoining allotment or (b) located at least 12m from the nearest habitable room located on an adjoining allotment.</p>				
<p>PO 4.4</p> <p>External noise into bedrooms is minimised by separating or shielding these rooms from service equipment areas and fixed noise sources located on the same or an adjoining allotment.</p>	<p>DTS/DPF 4.4</p> <p>Adjacent land is used for residential purposes.</p>				
<p>PO 4.5</p> <p>Outdoor areas associated with licensed premises (such as beer gardens or dining areas) are designed and/or sited to not cause unreasonable noise impact on existing adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 4.5</p> <p>None are applicable.</p>				
<p>PO 4.6</p> <p>Development incorporating music achieves suitable acoustic amenity when measured at the boundary of an adjacent sensitive receiver (or lawfully approved sensitive receiver) or zone primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 4.6</p> <p>Development incorporating music includes noise attenuation measures that will achieve the following noise levels:</p> <table border="1" style="width: 100%; border-collapse: collapse; margin-top: 10px;"> <thead> <tr> <th style="width: 50%; text-align: center;">Assessment location</th> <th style="width: 50%; text-align: center;">Music noise level</th> </tr> </thead> <tbody> <tr> <td style="padding: 5px;">Externally at the nearest existing or envisaged noise sensitive location</td> <td style="padding: 5px;">Less than 8dB above the level of background noise (L_{90,15min}) in any octave band of the sound spectrum (LOCT10,15 < LOCT90,15 + 8dB)</td> </tr> </tbody> </table>	Assessment location	Music noise level	Externally at the nearest existing or envisaged noise sensitive location	Less than 8dB above the level of background noise (L _{90,15min}) in any octave band of the sound spectrum (LOCT10,15 < LOCT90,15 + 8dB)
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<p><small>Air Quality</small></p>					
<p>PO 5.1</p> <p>Development with the potential to emit harmful or nuisance-generating air pollution incorporates air pollution control measures to prevent harm to human health or unreasonably impact the amenity of sensitive receivers (or lawfully approved sensitive receivers) within the locality and zones primarily intended to accommodate sensitive receivers.</p>	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>				
<p>PO 5.2</p> <p>Development that includes chimneys or exhaust flues (including cafes, restaurants and fast food outlets) is designed to minimise nuisance or adverse health impacts to sensitive receivers (or lawfully approved sensitive receivers) by:</p> <p>(a) incorporating appropriate treatment technology before exhaust emissions are released (b) locating and designing chimneys or exhaust flues to maximise the dispersion of exhaust emissions, taking into account the location of sensitive receivers.</p>	<p>DTS/DPF 5.2</p> <p>None are applicable.</p>				
<p><small>Light Spill</small></p>					
<p>PO 6.1</p> <p>External lighting is positioned and designed to not cause unreasonable light spill impact on adjacent sensitive receivers (or lawfully approved sensitive receivers).</p>	<p>DTS/DPF 6.1</p> <p>None are applicable.</p>				
<p>PO 6.2</p> <p>External lighting is not hazardous to motorists and cyclists.</p>	<p>DTS/DPF 6.2</p> <p>None are applicable.</p>				
<p><small>Solar Reflectivity / Glare</small></p>					

<p>PO 7.1</p> <p>Development is designed and comprised of materials and finishes that do not unreasonably cause a distraction to adjacent road users and pedestrian areas or unreasonably cause heat loading and micro-climatic impacts on adjacent buildings and land uses as a result of reflective solar glare.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
Electrical Interference	
<p>PO 8.1</p> <p>Development in rural and remote areas does not unreasonably diminish or result in the loss of existing communication services due to electrical interference.</p>	<p>DTS/DPF 8.1</p> <p>The building or structure:</p> <p>(a) is no greater than 10m in height, measured from existing ground level or</p> <p>(b) is not within a line of sight between a fixed transmitter and fixed receiver (antenna) other than where an alternative service is available via a different fixed transmitter or cable.</p>
Interface with Rural Activities	
<p>PO 9.1</p> <p>Sensitive receivers are located and designed to mitigate impacts from lawfully existing horticultural and farming activities (or lawfully approved horticultural and farming activities), including spray drift and noise and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>
<p>PO 9.2</p> <p>Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing intensive animal husbandry activities and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>
<p>PO 9.3</p> <p>Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing land-based aquaculture activities and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.3</p> <p>Sensitive receivers are located at least 200m from the boundary of a site used for land-based aquaculture and associated components in other ownership.</p>
<p>PO 9.4</p> <p>Sensitive receivers are located and designed to mitigate potential impacts from lawfully existing dairies including associated wastewater lagoons and liquid/solid waste storage and disposal facilities and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.4</p> <p>Sensitive receivers are sited at least 500m from the boundary of a site used for a dairy and associated wastewater lagoon(s) and liquid/solid waste storage and disposal facilities in other ownership.</p>
<p>PO 9.5</p> <p>Sensitive receivers are located and designed to mitigate the potential impacts from lawfully existing facilities used for the handling, transportation and storage of bulk commodities (recognising the potential for extended hours of operation) and do not prejudice the continued operation of these activities.</p>	<p>DTS/DPF 9.5</p> <p>Sensitive receivers are located away from the boundary of a site used for the handling, transportation and/or storage of bulk commodities in other ownership in accordance with the following:</p> <p>(a) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals to or from any commercial storage facility</p> <p>(b) 300m or more, where it involves the handling of agricultural crop products, rock, ores, minerals, petroleum products or chemicals at a wharf or wharf side facility (including sea-port grain terminals) where the handling of these materials into or from vessels does not exceed 100 tonnes per day</p> <p>(c) 500m or more, where it involves the storage of bulk petroleum in individual containers with a capacity up to 200 litres and a total on-site storage capacity not exceeding 1000 cubic metres</p> <p>(d) 500m or more, where it involves the handling of coal with a capacity up to 1 tonne per day or a storage capacity up to 50 tonnes</p> <p>(e) 1000m or more, where it involves the handling of coal with a capacity exceeding 1 tonne per day but not exceeding 100 tonnes per day or a storage capacity exceeding 50 tonnes but not exceeding 5000 tonnes.</p>
<p>PO 9.6</p> <p>Setbacks and vegetation plantings along allotment boundaries should be incorporated to mitigate the potential impacts of spray drift and other impacts associated with agricultural and horticultural activities.</p>	<p>DTS/DPF 9.6</p> <p>None are applicable.</p>

<p>PO 9.7</p> <p>Urban development does not prejudice existing agricultural and horticultural activities through appropriate separation and design techniques.</p>	<p>DTS/DPF 9.7</p> <p>None are applicable.</p>
<p>Interface with Mines and Quarries (Rural and Remote Areas)</p>	
<p>PO 10.1</p> <p>Sensitive receivers are separated from existing mines to minimise the adverse impacts from noise, dust and vibration.</p>	<p>DTS/DPF 10.1</p> <p>Sensitive receivers are located no closer than 500m from the boundary of a Mining Production Tenement under the <i>Mining Act 1971</i>.</p>

Land Division

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
<p>DO 1</p>	<p>Land division:</p> <ul style="list-style-type: none"> (a) creates allotments with the appropriate dimensions and shape for their intended use (b) allows efficient provision of new infrastructure and the optimum use of underutilised infrastructure (c) integrates and allocates adequate and suitable land for the preservation of site features of value, including significant vegetation, watercourses, water bodies and other environmental features (d) facilitates solar access through allotment orientation (e) creates a compact urban form that supports active travel, walkability and the use of public transport (f) avoids areas of high natural hazard risk.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>All land division</p>	
<p>Allotment configuration</p>	
<p>PO 1.1</p> <p>Land division creates allotments suitable for their intended use.</p>	<p>DTS/DPF 1.1</p> <p>Division of land satisfies (a) or (b):</p> <ul style="list-style-type: none"> (a) reflects the site boundaries illustrated and approved in an operative or existing development authorisation for residential development under the <i>Development Act 1993</i> or <i>Planning, Development and Infrastructure Act 2016</i> where the allotments are used or are proposed to be used solely for residential purposes (b) is proposed as part of a combined land division application with deemed-to-satisfy dwellings on the proposed allotments.
<p>PO 1.2</p> <p>Land division considers the physical characteristics of the land, preservation of environmental and cultural features of value and the prevailing context of the locality.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>Design and Layout</p>	
<p>PO 2.1</p> <p>Land division results in a pattern of development that minimises the likelihood of future earthworks and retaining walls.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>

PO 2.2 Land division enables the appropriate management of interface impacts between potentially conflicting land uses and/or zones.	DTS/DPF 2.2 None are applicable.
PO 2.3 Land division maximises the number of allotments that face public open space and public streets.	DTS/DPF 2.3 None are applicable.
PO 2.4 Land division is integrated with site features, adjacent land uses, the existing transport network and available infrastructure.	DTS/DPF 2.4 None are applicable.
PO 2.5 Development and infrastructure is provided and staged in a manner that supports an orderly and economic provision of land, infrastructure and services.	DTS/DPF 2.5 None are applicable.
PO 2.6 Land division results in watercourses being retained within open space and development taking place on land not subject to flooding.	DTS/DPF 2.6 None are applicable.
PO 2.7 Land division results in legible street patterns connected to the surrounding street network.	DTS/DPF 2.7 None are applicable.
PO 2.8 Land division is designed to preserve existing vegetation of value including native vegetation and regulated and significant trees.	DTS/DPF 2.8 None are applicable.
Roads and Access	
PO 3.1 Land division provides allotments with access to an all-weather public road.	DTS/DPF 3.1 None are applicable.
PO 3.2 Street patterns and intersections are designed to enable the safe and efficient movement of pedestrian, cycle and vehicular traffic.	DTS/DPF 3.2 None are applicable.
PO 3.3 Land division does not impede access to publicly owned open space and/or recreation facilities.	DTS/DPF 3.3 None are applicable.
PO 3.4 Road reserves provide for safe and convenient movement and parking of projected volumes of vehicles and allow for the efficient movement of service and emergency vehicles.	DTS/DPF 3.4 None are applicable.
PO 3.5 Road reserves are designed to accommodate pedestrian and cycling infrastructure, street tree planting, landscaping and street furniture.	DTS/DPF 3.5 None are applicable.
PO 3.6	DTS/DPF 3.6

Road reserves accommodate stormwater drainage and public utilities.	None are applicable.
PO 3.7 Road reserves provide unobstructed vehicular access and egress to and from individual allotments and sites.	DTS/DPF 3.7 None are applicable.
PO 3.8 Roads, open space and thoroughfares provide safe and convenient linkages to the surrounding open space and transport network.	DTS/DPF 3.8 None are applicable.
PO 3.9 Public streets are designed to enable tree planting to provide shade and enhance the amenity of streetscapes.	DTS/DPF 3.9 None are applicable.
PO 3.10 Local streets are designed to create low-speed environments that are safe for cyclists and pedestrians.	DTS/DPF 3.10 None are applicable.
Infrastructure	
PO 4.1 Land division incorporates public utility services within road reserves or dedicated easements.	DTS/DPF 4.1 None are applicable.
PO 4.2 Waste water, sewage and other effluent is capable of being disposed of from each allotment without risk to public health or the environment.	DTS/DPF 4.2 Each allotment can be connected to: (a) a waste water treatment plant that has the hydraulic volume and pollutant load treatment and disposal capacity for the maximum predicted wastewater volume generated by subsequent development of the proposed allotment or (b) a form of on-site waste water treatment and disposal that meets relevant public health and environmental standards.
PO 4.3 Septic tank effluent drainage fields and other waste water disposal areas are maintained to ensure the effective operation of waste systems and minimise risks to human health and the environment.	DTS/DPF 4.3 Development is not built on, or encroaches within, an area that is or will be, required for a sewerage system or waste control system.
PO 4.4 Constructed wetland systems, including associated detention and retention basins, are sited and designed to ensure public health and safety is protected, including by minimising potential public health risks arising from the breeding of mosquitoes.	DTS/DPF 4.4 None are applicable.
PO 4.5 Constructed wetland systems, including associated detention and retention basins, are sited and designed to allow sediments to settle prior to discharge into watercourses or the marine environment.	DTS/DPF 4.5 None are applicable.
PO 4.6 Constructed wetland systems, including associated detention and retention basins, are sited and designed to function as a landscape feature.	DTS/DPF 4.6 None are applicable.
Minor Land Division (Under 20 Allotments)	
Open Space	

<p>PO 5.1</p> <p>Land division proposing an additional allotment under 1 hectare provides or supports the provision of open space.</p>	<p>DTS/DPF 5.1</p> <p>None are applicable.</p>
Solar Orientation	
<p>PO 6.1</p> <p>Land division for residential purposes facilitates solar access through allotment orientation.</p>	<p>DTS/DPF 6.1</p> <p>None are applicable.</p>
Water Sensitive Design	
<p>PO 7.1</p> <p>Land division creating a new road or common driveway includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
<p>PO 7.2</p> <p>Land division designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.</p>	<p>DTS/DPF 7.2</p> <p>None are applicable.</p>
Battle-Axe Development	
<p>PO 8.1</p> <p>Battle-axe development appropriately responds to the existing neighbourhood context.</p>	<p>DTS/DPF 8.1</p> <p>Allotments are not in the form of a battle-axe arrangement.</p>
<p>PO 8.2</p> <p>Battle-axe development designed to allow safe and convenient movement.</p>	<p>DTS/DPF 8.2</p> <p>The handle of a battle-axe development:</p> <ul style="list-style-type: none"> (a) has a minimum width of 4m or (b) where more than 3 allotments are proposed, a minimum width of 5.5m.
<p>PO 8.3</p> <p>Battle-axe allotments and/or common land are of a suitable size and dimension to allow passenger vehicles to enter and exit and manoeuvre within the site in a safe and convenient manner.</p>	<p>DTS/DPF 8.3</p> <p>Battle-axe development allows a B85 passenger vehicle to enter and exit parking spaces in no more than a three-point turn manoeuvre.</p>
<p>PO 8.4</p> <p>Battle-axe or common driveways incorporate landscaping and permeability to improve appearance and assist in stormwater management.</p>	<p>DTS/DPF 8.4</p> <p>Battle-axe or common driveways satisfy (a) and (b):</p> <ul style="list-style-type: none"> (a) are constructed of a minimum of 50% permeable or porous material (b) where the driveway is located directly adjacent the side or rear boundary of the site, soft landscaping with a minimum dimension of 1m is provided between the driveway and site boundary (excluding along the perimeter of a passing point).
Major Land Division (20+ Allotments)	
Open Space	
<p>PO 9.1</p> <p>Land division allocates or retains evenly distributed, high quality areas of open space to improve residential amenity and provide urban heat amelioration.</p>	<p>DTS/DPF 9.1</p> <p>None are applicable.</p>
<p>PO 9.2</p> <p>Land allocated for open space is suitable for its intended active and passive recreational use considering gradient and potential for inundation.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>

PO 9.3 Land allocated for active recreation has dimensions capable of accommodating a range of active recreational activities.	DTS/DPF 9.3 None are applicable.
Water Sensitive Design	
PO 10.1 Land division creating 20 or more allotments includes a stormwater management system designed to mitigate peak flows and manage the rate and duration of stormwater discharges from the site to ensure that the development does not increase the peak flows in downstream systems.	DTS/DPF 10.1 None are applicable.
PO 10.2 Land division creating 20 or more allotments includes stormwater management systems that minimise the discharge of sediment, suspended solids, organic matter, nutrients, bacteria, litter and other contaminants to the stormwater system, watercourses or other water bodies.	DTS/DPF 10.2 None are applicable.
Solar Orientation	
PO 11.1 Land division creating 20 or more allotments for residential purposes facilitates solar access through allotment orientation and allotment dimensions.	DTS/DPF 11.1 None are applicable.

Marinas and On-Water Structures

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Marinas and on-water structures are located and designed to minimise the impairment of commercial, recreational and navigational activities and adverse impacts on the environment.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Navigation and Safety	
PO 1.1 Safe public access is provided or maintained to the waterfront, public infrastructure and recreation areas.	DTS/DPF 1.1 None are applicable.
PO 1.2 The operation of wharves is not impaired by marinas and on-water structures.	DTS/DPF 1.2 None are applicable.
PO 1.3 Navigation and access channels are not impaired by marinas and on-water	DTS/DPF 1.3 None are applicable.

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structures.	
PO 1.4 Commercial shipping lanes are not impaired by marinas and on-water structures.	DTS/DPF 1.4 Marinas and on-water structures are set back 250m or more from commercial shipping lanes.
PO 1.5 Marinas and on-water structures are located to avoid interfering with the operation or function of a water supply pumping station.	DTS/DPF 1.5 On-water structures are set back: (a) 3km or more from upstream water supply pumping station take-off points (b) 500m or more from downstream water supply pumping station take-off points.
PO 1.6 Maintenance of on-water infrastructure, including revetment walls, is not impaired by marinas and on-water structures.	DTS/DPF 1.6 None are applicable.
Environmental Protection	
PO 2.1 Development is sited and designed to facilitate water circulation and exchange.	DTS/DPF 2.1 None are applicable.

Open Space and Recreation

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Pleasant, functional and accessible open space and recreation facilities are provided at State, regional, district, neighbourhood and local levels for active and passive recreation, biodiversity, community health, urban cooling, tree canopy cover, visual amenity, gathering spaces, wildlife and waterway corridors, and a range of other functions and at a range of sizes that reflect the purpose of that open space.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Land Use and Intensity	
PO 1.1 Recreation facilities are compatible with surrounding land uses and activities.	DTS/DPF 1.1 None are applicable.
PO 1.2 Open space areas include natural or landscaped areas using locally indigenous plant species and large trees.	DTS/DPF 1.2 None are applicable.
Design and Siting	
PO 2.1	DTS/DPF 2.1

Open space and recreation facilities address adjacent public roads to optimise pedestrian access and visibility.	None are applicable.
PO 2.2 Open space and recreation facilities incorporate park furniture, shaded areas and resting places.	DTS/DPF 2.2 None are applicable.
PO 2.3 Open space and recreation facilities link habitats, wildlife corridors and existing open spaces and recreation facilities.	DTS/DPF 2.3 None are applicable.
Pedestrians and Cyclists	
PO 3.1 Open space incorporates: (a) pedestrian and cycle linkages to other open spaces, centres, schools and public transport nodes; (b) safe crossing points where pedestrian routes intersect the road network; (c) easily identified access points.	DTS/DPF 3.1 None are applicable.
Usability	
PO 4.1 Land allocated for open space is suitable for its intended active and passive recreational use taking into consideration its gradient and potential for inundation.	DTS/DPF 4.1 None are applicable.
Safety and Security	
PO 5.1 Open space is overlooked by housing, commercial or other development to provide casual surveillance where possible.	DTS/DPF 5.1 None are applicable.
PO 5.2 Play equipment is located to maximise opportunities for passive surveillance.	DTS/DPF 5.2 None are applicable.
PO 5.3 Landscaping provided in open space and recreation facilities maximises opportunities for casual surveillance throughout the park.	DTS/DPF 5.3 None are applicable.
PO 5.4 Fenced parks and playgrounds have more than one entrance or exit to minimise potential entrapment.	DTS/DPF 5.4 None are applicable.
PO 5.5 Adequate lighting is provided around toilets, telephones, seating, litter bins, bicycle storage, car parks and other such facilities.	DTS/DPF 5.5 None are applicable.
PO 5.6 Pedestrian and bicycle movement after dark is focused along clearly defined, adequately lit routes with observable entries and exits.	DTS/DPF 5.6 None are applicable.
Signage	
PO 6.1 Signage is provided at entrances to and within the open space and recreation	DTS/DPF 6.1 None are applicable.

facilities to provide clear orientation to major points of interest such as the location of public toilets, telephones, safe routes, park activities and the like.	
Buildings and Structures	
PO 7.1 Buildings and car parking areas in open space areas are designed, located and of a scale to be unobtrusive.	DTS/DPF 7.1 None are applicable.
PO 7.2 Buildings and structures in open space areas are clustered where practical to ensure that the majority of the site remains open.	DTS/DPF 7.2 None are applicable.
PO 7.3 Development in open space is constructed to minimise the extent of impervious surfaces.	DTS/DPF 7.3 None are applicable.
PO 7.4 Development that abuts or includes a coastal reserve or Crown land used for scenic, conservation or recreational purposes is located and designed to have regard to the purpose, management and amenity of the reserve.	DTS/DPF 7.4 None are applicable.
Landscaping	
PO 8.1 Open space and recreation facilities provide for the planting and retention of large trees and vegetation.	DTS/DPF 8.1 None are applicable.
PO 8.2 Landscaping in open space and recreation facilities provides shade and windbreaks: (a) along cyclist and pedestrian routes; (b) around picnic and barbecue areas; (c) in car parking areas.	DTS/DPF 8.2 None are applicable.
PO 8.3 Landscaping in open space facilitates habitat for local fauna and facilitates biodiversity.	DTS/DPF 8.3 None are applicable.
PO 8.4 Landscaping including trees and other vegetation passively watered with local rainfall run-off, where practicable.	DTS/DPF 8.4 None are applicable.

Out of Activity Centre Development

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO1	The role of Activity Centres in contributing to the form and pattern of development and enabling equitable and convenient access to a range of shopping, administrative, cultural, entertainment and other facilities in a single trip is maintained and reinforced.

Performance Outcomes and Deemed to Satisfy / Designated Performance Outcome Criteria

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Non-residential development outside Activity Centres of a scale and type that does not diminish the role of Activity Centres:</p> <p>(a) as primary locations for shopping, administrative, cultural, entertainment and community services</p> <p>(b) as a focus for regular social and business gatherings</p> <p>(c) in contributing to or maintaining a pattern of development that supports equitable community access to services and facilities.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Out-of-activity centre non-residential development complements Activity Centres through the provision of services and facilities:</p> <p>(a) that support the needs of local residents and workers, particularly in underserved locations</p> <p>(b) at the edge of Activities Centres where they cannot readily be accommodated within an existing Activity Centre to expand the range of services on offer and support the role of the Activity Centre.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>

Resource Extraction

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Resource extraction activities are developed in a manner that minimises human and environmental impacts.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<small>Land Use and Intensity</small>	
<p>PO 1.1</p> <p>Resource extraction activities minimise landscape damage outside of those areas unavoidably disturbed to access and exploit a resource and provide for the progressive reclamation and betterment of disturbed areas.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Resource extraction activities avoid damage to cultural sites or artefacts.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<small>Water Quality</small>	
<p>PO 2.1</p> <p>Stormwater and/or wastewater from resource extraction activities is diverted into appropriately sized treatment and retention systems to enable reuse on site.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<small>Separation Treatments, Buffers and Landscaping</small>	

<p>PO 3.1</p> <p>Resource extraction activities minimise adverse impacts upon sensitive receivers through incorporation of separation distances and/or mounding/vegetation.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Resource extraction activities are screened from view from adjacent land by perimeter landscaping and/or mounding.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>

Site Contamination

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Ensure land is suitable for the proposed use in circumstances where it is, or may have been, subject to site contamination.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Ensure land is suitable for use when land use changes to a more sensitive use.</p>	<p>DTS/DPF 1.1</p> <p>Development satisfies (a), (b), (c) or (d):</p> <ul style="list-style-type: none"> (a) does not involve a change in the use of land (b) involves a change in the use of land that does not constitute a change to a more sensitive use (c) involves a change in the use of land to a more sensitive use on land at which site contamination is unlikely to exist (as demonstrated in a site contamination declaration form) (d) involves a change in the use of land to a more sensitive use on land at which site contamination exists, or may exist (as demonstrated in a site contamination declaration form), and satisfies both of the following: <ul style="list-style-type: none"> (i) a site contamination audit report has been prepared under Part 10A of the <i>Environment Protection Act 1993</i> in relation to the land within the previous 5 years which states that- <ul style="list-style-type: none"> A. site contamination does not exist (or no longer exists) at the land or B. the land is suitable for the proposed use or range of uses (without the need for any further remediation) or C. where remediation is, or remains, necessary for the proposed use (or range of uses), remediation work has been carried out or will be carried out (and the applicant has provided a written undertaking that the remediation works will be implemented in association with the development) and (ii) no other class 1 activity or class 2 activity has taken place at the land since the preparation of the site contamination audit report (as demonstrated in a site contamination declaration form).

Tourism Development

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Tourism development is built in locations that cater to the needs of visitors and positively contributes to South Australia's visitor economy.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<small>General</small>	
<p>PO 1.1</p> <p>Tourism development complements and contributes to local, natural, cultural or historical context where:</p> <p>(a) it supports immersive natural experiences (b) it showcases South Australia's landscapes and produce (c) its events and functions are connected to local food, wine and nature.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Tourism development comprising multiple accommodation units (including any facilities and activities for use by guests and visitors) is clustered to minimise environmental and contextual impact.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<small>Caravan and Tourist Parks</small>	
<p>PO 2.1</p> <p>Potential conflicts between long-term residents and short-term tourists are minimised through suitable siting and design measures.</p>	<p>DTS/DPF 2.1</p> <p>None are applicable.</p>
<p>PO 2.2</p> <p>Occupants are provided privacy and amenity through landscaping and fencing.</p>	<p>DTS/DPF 2.2</p> <p>None are applicable.</p>
<p>PO 2.3</p> <p>Communal open space and centrally located recreation facilities are provided for guests and visitors.</p>	<p>DTS/DPF 2.3</p> <p>12.5% or more of a caravan park comprises clearly defined communal open space, landscaped areas and areas for recreation.</p>
<p>PO 2.4</p> <p>Perimeter landscaping is used to enhance the amenity of the locality.</p>	<p>DTS/DPF 2.4</p> <p>None are applicable.</p>
<p>PO 2.5</p> <p>Amenity blocks (showers, toilets, laundry and kitchen facilities) are sufficient to serve the full occupancy of the development.</p>	<p>DTS/DPF 2.5</p> <p>None are applicable.</p>
<p>PO 2.6</p> <p>Long-term occupation does not displace tourist accommodation, particularly in important tourist destinations such as coastal and riverine locations.</p>	<p>DTS/DPF 2.6</p> <p>None are applicable.</p>
<small>Tourist accommodation in areas constituted under the National Parks and Wildlife Act 1972</small>	
<p>PO 3.1</p> <p>Tourist accommodation avoids delicate or environmentally sensitive areas such as sand dunes, cliff tops, estuaries, wetlands or substantially intact strata of native vegetation (including regenerated areas of native vegetation lost through bushfire).</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p>	<p>DTS/DPF 3.2</p>

<p>Tourist accommodation is sited and designed in a manner that is subservient to the natural environment and where adverse impacts on natural features, landscapes, habitats and cultural assets are avoided.</p>	<p>None are applicable.</p>
<p>PO 3.3</p> <p>Tourist accommodation and recreational facilities, including associated access ways and ancillary structures, are located on cleared (other than where cleared as a result of bushfire) or degraded areas or where environmental improvements can be achieved.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Tourist accommodation is designed to prevent conversion to private dwellings through:</p> <ul style="list-style-type: none"> (a) comprising a minimum of 10 accommodation units (b) clustering separated individual accommodation units (c) being of a size unsuitable for a private dwelling (d) ensuring functional areas that are generally associated with a private dwelling such as kitchens and laundries are excluded from, or physically separated from individual accommodation units, or are of a size unsuitable for a private dwelling. 	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>

Transport, Access and Parking

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	A comprehensive, integrated and connected transport system that is safe, sustainable, efficient, convenient and accessible to all users.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Movement Systems	
<p>PO 1.1</p> <p>Development is integrated with the existing transport system and designed to minimise its potential impact on the functional performance of the transport system.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Development is designed to discourage commercial and industrial vehicle movements through residential streets and adjacent other sensitive receivers.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Industrial, commercial and service vehicle movements, loading areas and designated parking spaces are separated from passenger vehicle car parking areas to ensure efficient and safe movement and minimise potential conflict.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>Development is sited and designed so that loading, unloading and turning of all</p>	<p>DTS/DPF 1.4</p> <p>All vehicle manoeuvring occurs onsite.</p>

traffic avoids interrupting the operation of and queuing on public roads and pedestrian paths.	
Sightlines	
PO 2.1 Sightlines at intersections, pedestrian and cycle crossings, and crossovers to allotments for motorists, cyclists and pedestrians are maintained or enhanced to ensure safety for all road users and pedestrians.	DTS/DPF 2.1 None are applicable.
PO 2.2 Walls, fencing and landscaping adjacent to driveways and corner sites are designed to provide adequate sightlines between vehicles and pedestrians.	DTS/DPF 2.2 None are applicable.
Vehicle Access	
PO 3.1 Safe and convenient access minimises impact or interruption on the operation of public roads.	DTS/DPF 3.1 The access is: (a) provided via a lawfully existing or authorised driveway or access point or an access point for which consent has been granted as part of an application for the division of land or (b) not located within 6m of an intersection of 2 or more roads or a pedestrian activated crossing.
PO 3.2 Development incorporating vehicular access ramps ensures vehicles can enter and exit a site safely and without creating a hazard to pedestrians and other vehicular traffic.	DTS/DPF 3.2 None are applicable.
PO 3.3 Access points are sited and designed to accommodate the type and volume of traffic likely to be generated by the development or land use.	DTS/DPF 3.3 None are applicable.
PO 3.4 Access points are sited and designed to minimise any adverse impacts on neighbouring properties.	DTS/DPF 3.4 None are applicable.
PO 3.5 Access points are located so as not to interfere with street trees, existing street furniture (including directional signs, lighting, seating and weather shelters) or infrastructure services to maintain the appearance of the streetscape, preserve local amenity and minimise disruption to utility infrastructure assets.	DTS/DPF 3.5 Vehicle access to designated car parking spaces satisfy (a) or (b): (a) is provided via a lawfully existing or authorised access point or an access point for which consent has been granted as part of an application for the division of land (b) where newly proposed, is set back: (i) 0.5m or more from any street furniture, street pole, infrastructure services pit, or other stormwater or utility infrastructure unless consent is provided from the asset owner (ii) 2m or more from the base of the trunk of a street tree unless consent is provided from the tree owner for a lesser distance (iii) 6m or more from the tangent point of an intersection of 2 or more roads (iv) outside of the marked lines or infrastructure dedicating a pedestrian crossing.
PO 3.6 Driveways and access points are separated and minimised in number to optimise the provision of on-street visitor parking (where on-street parking is appropriate).	DTS/DPF 3.6 Driveways and access points: (a) for sites with a frontage to a public road of 20m or less, one access point no greater than 3.5m in width is provided (b) for sites with a frontage to a public road greater than 20m: (i) a single access point no greater than 6m in width is provided or (ii) not more than two access points with a width of 3.5m each are provided.
PO 3.7	DTS/DPF 3.7

<p>Access points are appropriately separated from level crossings to avoid interference and ensure their safe ongoing operation.</p>	<p>Development does not involve a new or modified access or cause an increase in traffic through an existing access that is located within the following distance from a railway crossing:</p> <ul style="list-style-type: none"> (a) 80 km/h road - 110m (b) 70 km/h road - 90m (c) 60 km/h road - 70m (d) 50km/h or less road - 50m.
<p>PO 3.8</p> <p>Driveways, access points, access tracks and parking areas are designed and constructed to allow adequate movement and manoeuvrability having regard to the types of vehicles that are reasonably anticipated.</p>	<p>DTS/DPF 3.8</p> <p>None are applicable.</p>
<p>PO 3.9</p> <p>Development is designed to ensure vehicle circulation between activity areas occurs within the site without the need to use public roads.</p>	<p>DTS/DPF 3.9</p> <p>None are applicable.</p>
<p>Access for People with Disabilities</p>	
<p>PO 4.1</p> <p>Development is sited and designed to provide safe, dignified and convenient access for people with a disability.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>Vehicle Parking Rates</p>	
<p>PO 5.1</p> <p>Sufficient on-site vehicle parking and specifically marked accessible car parking places are provided to meet the needs of the development or land use having regard to factors that may support a reduced on-site rate such as:</p> <ul style="list-style-type: none"> (a) availability of on-street car parking (b) shared use of other parking areas (c) in relation to a mixed-use development, where the hours of operation of commercial activities complement the residential use of the site, the provision of vehicle parking may be shared (d) the adaptive reuse of a State or Local Heritage Place. 	<p>DTS/DPF 5.1</p> <p>Development provides a number of car parking spaces on-site at a rate no less than the amount calculated using one of the following, whichever is relevant:</p> <ul style="list-style-type: none"> (a) Transport, Access and Parking Table 2 - Off-Street Vehicle Parking Requirements in Designated Areas if the development is a class of development listed in Table 2 and the site is in a Designated Area (b) Transport, Access and Parking Table 1 - General Off-Street Car Parking Requirements where (a) does not apply (c) if located in an area where a lawfully established carparking fund operates, the number of spaces calculated under (a) or (b) less the number of spaces offset by contribution to the fund.
<p>Vehicle Parking Areas</p>	
<p>PO 6.1</p> <p>Vehicle parking areas are sited and designed to minimise impact on the operation of public roads by avoiding the use of public roads when moving from one part of a parking area to another.</p>	<p>DTS/DPF 6.1</p> <p>Movement between vehicle parking areas within the site can occur without the need to use a public road.</p>
<p>PO 6.2</p> <p>Vehicle parking areas are appropriately located, designed and constructed to minimise impacts on adjacent sensitive receivers through measures such as ensuring they are attractively developed and landscaped, screen fenced, and the like.</p>	<p>DTS/DPF 6.2</p> <p>None are applicable.</p>
<p>PO 6.3</p> <p>Vehicle parking areas are designed to provide opportunity for integration and shared-use of adjacent car parking areas to reduce the total extent of vehicle parking areas and access points.</p>	<p>DTS/DPF 6.3</p> <p>None are applicable.</p>
<p>PO 6.4</p> <p>Pedestrian linkages between parking areas and the development are provided and are safe and convenient.</p>	<p>DTS/DPF 6.4</p> <p>None are applicable.</p>

<p>PO 6.5</p> <p>Vehicle parking areas that are likely to be used during non-daylight hours are provided with sufficient lighting to entry and exit points to ensure clear visibility to users.</p>	<p>DTS/DPF 6.5</p> <p>None are applicable.</p>
<p>PO 6.6</p> <p>Loading areas and designated parking spaces for service vehicles are provided within the boundary of the site.</p>	<p>DTS/DPF 6.6</p> <p>Loading areas and designated parking spaces are wholly located within the site.</p>
<p>PO 6.7</p> <p>On-site visitor parking spaces are sited and designed to be accessible to all visitors at all times.</p>	<p>DTS/DPF 6.7</p> <p>None are applicable.</p>
<p>Undercroft and Below Ground Garaging and Parking of Vehicles</p>	
<p>PO 7.1</p> <p>Undercroft and below ground garaging of vehicles is designed to enable safe entry and exit from the site without compromising pedestrian or cyclist safety or causing conflict with other vehicles.</p>	<p>DTS/DPF 7.1</p> <p>None are applicable.</p>
<p>Internal Roads and Parking Areas in Residential Parks and Caravan and Tourist Parks</p>	
<p>PO 8.1</p> <p>Internal road and vehicle parking areas are surfaced to prevent dust becoming a nuisance to park residents and occupants.</p>	<p>DTS/DPF 8.1</p> <p>None are applicable.</p>
<p>PO 8.2</p> <p>Traffic circulation and movement within the park is pedestrian friendly and promotes low speed vehicle movement.</p>	<p>DTS/DPF 8.2</p> <p>None are applicable.</p>
<p>Bicycle Parking in Designated Areas</p>	
<p>PO 9.1</p> <p>The provision of adequately sized on-site bicycle parking facilities encourages cycling as an active transport mode.</p>	<p>DTS/DPF 9.1</p> <p>Areas and / or fixtures are provided for the parking and storage of bicycles at a rate not less than the amount calculated using Transport, Access and Parking Table 3 - Off Street Bicycle Parking Requirements.</p>
<p>PO 9.2</p> <p>Bicycle parking facilities provide for the secure storage and tethering of bicycles in a place where casual surveillance is possible, is well lit and signed for the safety and convenience of cyclists and deters property theft.</p>	<p>DTS/DPF 9.2</p> <p>None are applicable.</p>
<p>PO 9.3</p> <p>Non-residential development incorporates end-of-journey facilities for employees such as showers, changing facilities and secure lockers, and signage indicating the location of the facilities to encourage cycling as a mode of journey-to-work transport.</p>	<p>DTS/DPF 9.3</p> <p>None are applicable.</p>
<p>Corner Cut-Offs</p>	
<p>PO 10.1</p> <p>Development is located and designed to ensure drivers can safely turn into and out of public road junctions.</p>	<p>DTS/DPF 10.1</p> <p>Development does not involve building work, or building work is located wholly outside the land shown as Corner Cut-Off Area in the following diagram:</p>

	<p>The diagram illustrates a corner cut-off area. A dashed line represents the 'Allotment Boundary'. A solid line represents the 'Road Reserve'. A shaded triangular area is shown at the corner, with a 4.5M setback from the allotment boundary and a 4.5M setback from the road reserve.</p>
Heavy Vehicle Parking	
<p>PO 11.1</p> <p>Heavy vehicle parking and access is designed and sited so that the activity does not result in nuisance to adjoining neighbours as a result of dust, fumes, vibration, odour or potentially hazardous loads.</p>	<p>DTS/DPF 11.1</p> <p>Heavy vehicle parking occurs in accordance with the following:</p> <ul style="list-style-type: none"> (a) the site is not located within a Neighbourhood-type zone (except a Rural Living Zone) (b) the site is a minimum of 0.4 ha (c) where the site is 2 ha or more, no more than 2 vehicles exceeding 3,000 kilograms each (and trailers) are to be parked on the allotment at any time (d) where the site is between 0.4 ha and 2 ha, only one vehicle exceeding 3,000 kilograms (and one trailer) are to be parking on the allotment at any time (e) the vehicle parking area achieves the following setbacks: <ul style="list-style-type: none"> (i) behind the building line or 30m, whichever is greater (ii) 20m from the secondary street if it is a State Maintained Road (iii) 10m from the secondary street if it is a local road (iv) 10m from side and rear boundaries (f) parking and access areas (including internal driveways) should be sealed or have a surface that can be treated and maintained to minimise dust and mud nuisance (g) does not include refrigerated trailers or vehicles (h) vehicles only enter and exit the property in accordance with the following hours: <ul style="list-style-type: none"> (i) Monday to Saturday 6:00am and 9:30pm (ii) Sunday and public holidays between 9:30 am and 7:00 pm (i) the handling or trans-shipment of freight is not carried out on the property.
<p>PO 11.2</p> <p>Heavy vehicle parking ensures that vehicles can enter and exit a site safely and without creating a hazard to pedestrians and other vehicular traffic.</p>	<p>DTS/DPF 11.2</p> <p>Heavy vehicles:</p> <ul style="list-style-type: none"> (a) can enter and exit the site in a forward direction; and (b) operate within the statutory mass and dimension limited for General Access Vehicles (as prescribed by the National Heavy Vehicle Regulator).
<p>PO 11.3</p> <p>Heavy vehicle parking is screened through siting behind buildings, screening, landscaping or the like to obscure views from adjoining properties and public roads.</p>	<p>DTS/DPF 11.3</p> <p>None are applicable.</p>

Table 1 - General Off-Street Car Parking Requirements

The following parking rates apply and if located in an area where a lawfully established carparking fund operates, the number of spaces is reduced by an amount equal to the number of spaces offset by contribution to the fund.

Class of Development	Car Parking Rate (unless varied by Table 2 onwards)
<p>Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.</p>	
Residential Development	
Detached Dwelling	<p>Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>
Group Dwelling	<p>Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling.</p> <p>Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.</p>

	0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings.
Residential Flat Building	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Row Dwelling where vehicle access is from the primary street	0.33 spaces per dwelling for visitor parking where development involves 3 or more dwellings. Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Row Dwelling where vehicle access is not from the primary street (i.e. rear-loaded)	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Semi-Detached Dwelling	Dwelling with 1 bedroom (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 2 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling, 1 of which is to be covered.
Aged / Supported Accommodation	
Retirement facility	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling. 0.2 spaces per dwelling for visitor parking.
Supported accommodation	0.3 spaces per bed.
Residential Development (Other)	
Ancillary accommodation	No additional requirements beyond those associated with the main dwelling.
Residential park	Dwelling with 1 or 2 bedrooms (including rooms capable of being used as a bedroom) - 1 space per dwelling. Dwelling with 3 or more bedrooms (including rooms capable of being used as a bedroom) - 2 spaces per dwelling. 0.2 spaces per dwelling for visitor parking.
Student accommodation	0.3 spaces per bed.
Workers' accommodation	0.5 spaces per bed plus 0.2 spaces per bed for visitor parking.
Tourist	
Caravan and tourist park	Parks with 100 sites or less - a minimum of 1 space per 10 sites to be used for accommodation. Parks with more than 100 sites - a minimum of 1 space per 15 sites used for accommodation. A minimum of 1 space for every caravan (permanently fixed to the ground) or cabin.
Tourist accommodation other than a caravan and tourist park	1 car parking space per accommodation unit / guest room.
Commercial Uses	
Auction room/ depot	1 space per 100m ² of building floor area plus an additional 2 spaces.
Automotive collision repair	3 spaces per service bay.
Motor repair station	3 spaces per service bay.
Office	For a call centre, 8 spaces per 100m ² of gross leasable floor area In all other cases, 4 spaces per 100m ² of gross leasable floor area.
Retail fuel outlet	3 spaces per 100m ² gross leasable floor area.
Service trade premises	2.5 spaces per 100m ² of gross leasable floor area 1 space per 100m ² of outdoor area used for display purposes.
Shop (no commercial kitchen)	5.5 spaces per 100m ² of gross leasable floor area where not located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared. 5 spaces per 100m ² of gross leasable floor area where located in an integrated complex containing two or more tenancies (and which may comprise more than one building) where facilities for off-street vehicle parking, vehicle loading and unloading, and the storage and collection of refuse are shared.
Shop (in the form of a bulky goods outlet)	2.5 spaces per 100m ² of gross leasable floor area.
Shop (in the form of a restaurant or involving a commercial kitchen)	Premises with a dine-in service only (which may include a take-away component with no drive-through) - 0.4 spaces per seat. Premises with take-away service but with no seats - 12 spaces per 100m ² of total floor area plus a drive-through queue capacity of ten vehicles measured from the pick-up point. Premises with a dine-in and drive-through take-away service - 0.3 spaces per seat plus a drive through queue capacity of 10 vehicles measured from the pick-up point.
Community and Civic Uses	
Community facility	For a library, 4 spaces per 100m ² of total floor area. For a hall/meeting hall, 0.2 spaces per seat. In all other cases, 10 spaces per 100m ² of total floor area.
Educational facility	For a primary school - 1.1 space per full time equivalent employee plus 0.25 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site. For a secondary school - 1.1 per full time equivalent employee plus 0.1 spaces per student for a pickup/set down area either on-site or on the public realm within 300m of the site. For a tertiary institution - 0.4 per student based on the maximum number of students on the site at any

	time.
Place of worship	1 space for every 3 visitor seats.
Child care facility	For a child care centre, 0.25 spaces per child In all other cases, 1 per employee plus 0.25 per child (drop off/pick up bays).
Health Related Uses	
Consulting room	4 spaces per consulting room excluding ancillary facilities.
Hospital	4.5 spaces per bed for a public hospital. 1.5 spaces per bed for a private hospital.
Recreational and Entertainment Uses	
Cinema complex	0.2 spaces per seat.
Concert hall / theatre	0.2 spaces per seat.
Hotel	1 space for every 2m ² of total floor area in a public bar plus 1 space for every 6m ² of total floor area available to the public in a lounge, beer garden plus 1 space per 2 gaming machines, plus 1 space per 3 seats in a restaurant.
Indoor recreation facility	6.5 spaces per 100m ² of total floor area for a Fitness Centre 4.5 spaces per 100m ² of total floor area for all other Indoor recreation facilities.
Industry/Employment Uses	
Fuel depot	1.5 spaces per 100m ² total floor area 1 spaces per 100m ² of outdoor area used for fuel depot activity purposes.
Industry	1.5 spaces per 100m ² of total floor area.
Store	0.5 spaces per 100m ² of total floor area.
Timber yard	1.5 spaces per 100m ² of total floor area 1 space per 100m ² of outdoor area used for display purposes.
Warehouse	0.5 spaces per 100m ² total floor area.
Other Uses	
Funeral Parlour	1 space per 5 seats in the chapel plus 1 space for each vehicle operated by the parlour.
Radio or Television Station	5 spaces per 100m ² of total building floor area.

Table 2 - Off-Street Car Parking Requirements in Designated Areas

The following parking rates apply in any zone, subzone or other area described in the 'Designated Areas' column.

Class of Development	Car Parking Rate		Designated Areas
	Minimum number of spaces	Maximum number of spaces	
<p>Where a development comprises more than one development type, then the overall car parking rate will be taken to be the sum of the car parking rates for each development type.</p>			
Development generally			
All classes of development	No minimum.	<p>No maximum except in the Primary Pedestrian Area identified in the Primary Pedestrian Area Concept Plan, where the maximum is:</p> <p>1 space for each dwelling with a total floor area less than 75 square metres</p> <p>2 spaces for each dwelling with a total floor area between 75 square metres and 150 square metres</p> <p>3 spaces for each dwelling with a total floor area greater than 150 square metres.</p> <p>Residential flat building or Residential component of a multi-storey building: 1 visitor space for each 6 dwellings.</p>	<p>Capital City Zone</p> <p>City Main Street Zone</p> <p>City Riverbank Zone</p> <p>Adelaide Park Lands Zone</p> <p>Business Neighbourhood Zone (within the City of Adelaide)</p> <p>The St Andrews Hospital Precinct Subzone and Women's and Children's Hospital Precinct Subzone of the Community Facilities Zone</p>
Non-residential development			
Non-residential development excluding tourist accommodation	3 spaces per 100m ² of gross leasable floor area.	5 spaces per 100m ² of gross leasable floor area.	<p>City Living Zone</p> <p>Urban Corridor (Boulevard) Zone</p> <p>Urban Corridor (Business) Zone</p>

			<p>Urban Corridor (Living) Zone</p> <p>Urban Corridor (Main Street) Zone</p> <p>Urban Neighbourhood Zone (except for Bowden)</p>
Non-residential development excluding tourist accommodation	3 spaces per 100m2 of gross leasable floor area.	6 spaces per 100m2 of gross leasable floor area.	<p>Strategic Innovation Zone in the City of Burnside, City of Marion or City of Mitcham</p> <p>Strategic Innovation Zone outside the City of Burnside, City of Marion or City of Mitcham when the site is also in a high frequency public transit area</p> <p>Suburban Activity Centre Zone when the site is also in a high frequency public transit area</p> <p>Suburban Business Zone when the site is also in a high frequency public transit area</p> <p>Business Neighbourhood Zone outside of the City of Adelaide when the site is also in a high frequency public transit area</p> <p>Suburban Main Street Zone when the site is also in a high frequency public transit area</p> <p>Urban Activity Centre Zone</p>
Non-residential development excluding tourist accommodation	<p>3 spaces per 100 square metres of gross leasable floor area</p> <p>1.5 spaces per 100 square metres of gross leasable floor area above ground floor level other than for a shop</p>	3 spaces per 100 square metres of gross leasable floor area	Urban Neighbourhood Zone in Bowden
Tourist accommodation	1 space for every 4 bedrooms up to 100 bedrooms plus 1 space for every 5 bedrooms over 100 bedrooms	1 space per 2 bedrooms up to 100 bedrooms and 1 space per 4 bedrooms over 100 bedrooms	<p>City Living Zone</p> <p>Urban Activity Centre Zone when the site is also in a high frequency public transit area</p> <p>Urban Corridor (Boulevard) Zone</p> <p>Urban Corridor (Business) Zone</p> <p>Urban Corridor (Living) Zone</p> <p>Urban Corridor (Main Street) Zone</p> <p>Urban Neighbourhood Zone (except for Bowden)</p>
Residential development			
Residential component of a multi-storey building	<p>Dwelling with no separate bedroom -0.25 spaces per dwelling</p> <p>1 bedroom dwelling - 0.75 spaces per dwelling</p> <p>2 bedroom dwelling - 1 space per dwelling</p> <p>3 or more bedroom dwelling - 1.25 spaces per dwelling</p> <p>0.25 spaces per dwelling for visitor parking.</p>	None specified.	<p>City Living Zone</p> <p>Strategic Innovation Zone in the City of Burnside, City of Marion or City of Mitcham</p> <p>Strategic Innovation Zone outside the City of Burnside, City of Marion or City of Mitcham when the site is also in a high frequency public transit area</p> <p>Urban Activity Centre Zone when the site is also in a high frequency public transit area</p> <p>Urban Corridor (Boulevard) Zone</p>

			Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone (except for Bowden)
Residential component of a multi-storey building	0.75 per dwelling	None specified	Urban Neighbourhood Zone in Bowden
Residential flat building	Dwelling with no separate bedroom -0.25 spaces per dwelling 1 bedroom dwelling - 0.75 spaces per dwelling 2 bedroom dwelling - 1 space per dwelling 3 or more bedroom dwelling - 1.25 spaces per dwelling 0.25 spaces per dwelling for visitor parking.	None specified.	City Living Zone Urban Activity Centre Zone when the site is also in a high frequency public transit area Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone (except for Bowden)
Residential flat building	0.75 per dwelling	None specified	Urban Neighbourhood Zone in Bowden
Detached dwelling	0.75 per dwelling	None specified	Urban Neighbourhood Zone in Bowden
Row dwelling	0.75 per dwelling	None specified	Urban Neighbourhood Zone in Bowden
Semi-detached dwelling	0.75 per dwelling	None specified	Urban Neighbourhood Zone in Bowden

Table 3 - Off-Street Bicycle Parking Requirements

The bicycle parking rates apply within designated areas located within parts of the State identified in the Schedule to Table 3.

Class of Development	Bicycle Parking Rate	
	Where a development comprises more than one development type, then the overall bicycle parking rate will be taken to be the sum of the bicycle parking rates for each development type.	
Consulting room	1 space per 20 employees plus 1 space per 20 consulting rooms for customers.	
Educational facility	For a secondary school - 1 space per 20 full-time time employees plus 10 percent of the total number of employee spaces for visitors. For tertiary education - 1 space per 20 employees plus 1 space per 10 full time students.	
Hospital	1 space per 15 beds plus 1 space per 30 beds for visitors.	
Indoor recreation facility	1 space per 4 employees plus 1 space per 200m ² of gross leasable floor area for visitors.	
Licensed Premises	1 per 20 employees, plus 1 per 60 square metres total floor area, plus 1 per 40 square metres of bar floor area, plus 1 per 120 square metres lounge and beer garden floor area, plus 1 per 60 square metres dining floor area, plus 1 per 40 square metres gaming room floor area.	
Office	1 space for every 200m ² of gross leasable floor area plus 2 spaces plus 1 space per 1000m ² of gross leasable floor area for visitors.	
Child care facility	1 space per 20 full time employees plus 1 space per 40 full time children.	
Recreation area	1 per 1500 spectator seats for employees plus 1 per 250 visitor and customers.	
Residential flat building	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 for every 10 dwellings for visitors.	
Residential component of a multi-storey building	Within the City of Adelaide 1 for every dwelling for residents with a total floor area less than 150 square metres, 2 for every dwelling for residents with a total floor area greater than 150 square metres, plus 1 for every 10 dwellings for visitors, and in all other cases 1 space for every 4 dwellings for residents plus 1 space for every 10 dwellings for visitors.	
Shop	1 space for every 300m ² of gross leasable floor area plus 1 space for every 600m ² of gross leasable floor area for customers.	
Tourist accommodation	1 space for every 20 employees plus 2 for the first 40 rooms and 1 for every additional 40 rooms for visitors.	
Schedule to Table 3	Designated Area	Relevant part of the State The bicycle parking rate applies to a designated area located in a relevant part of the State described below.
	All zones	City of Adelaide

	Business Neighbourhood Zone Strategic Innovation Zone Suburban Activity Centre Zone Suburban Business Zone Suburban Main Street Zone Urban Activity Centre Zone Urban Corridor (Boulevard) Zone Urban Corridor (Business) Zone Urban Corridor (Living) Zone Urban Corridor (Main Street) Zone Urban Neighbourhood Zone	Metropolitan Adelaide
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Waste Treatment and Management Facilities

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Mitigation of the potential environmental and amenity impacts of waste treatment and management facilities.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
Siting	
PO 1.1 Waste treatment and management facilities incorporate separation distances and attenuation measures within the site between waste operations areas (including all closed, operating and future cells) and sensitive receivers and sensitive environmental features to mitigate off-site impacts from noise, air and dust emissions.	DTS/DPF 1.1 None are applicable.
Soil and Water Protection	
PO 2.1 Soil, groundwater and surface water are protected from contamination from waste treatment and management facilities through measures such as: <ul style="list-style-type: none"> (a) containing potential groundwater and surface water contaminants within waste operations areas (b) diverting clean stormwater away from waste operations areas and potentially contaminated areas (c) providing a leachate barrier between waste operations areas and underlying soil and groundwater. 	DTS/DPF 2.1 None are applicable.
PO 2.2	DTS/DPF 2.2

Wastewater lagoons are set back from watercourses to minimise environmental harm and adverse effects on water resources.	Wastewater lagoons are set back 50m or more from watercourse banks.
<p>PO 2.3</p> <p>Wastewater lagoons are designed and sited to:</p> <p>(a) avoid intersecting underground waters; (b) avoid inundation by flood waters; (c) ensure lagoon contents do not overflow; (d) include a liner designed to prevent leakage.</p>	<p>DTS/DPF 2.3</p> <p>None are applicable.</p>
<p>PO 2.4</p> <p>Waste operations areas of landfills and organic waste processing facilities are set back from watercourses to minimise adverse impacts on water resources.</p>	<p>DTS/DPF 2.4</p> <p>Waste operations areas are set back 100m or more from watercourse banks.</p>
Amenity	
<p>PO 3.1</p> <p>Waste treatment and management facilities are screened, located and designed to minimise adverse visual impacts on amenity.</p>	<p>DTS/DPF 3.1</p> <p>None are applicable.</p>
<p>PO 3.2</p> <p>Access routes to waste treatment and management facilities via residential streets is avoided.</p>	<p>DTS/DPF 3.2</p> <p>None are applicable.</p>
<p>PO 3.3</p> <p>Litter control measures minimise the incidence of windblown litter.</p>	<p>DTS/DPF 3.3</p> <p>None are applicable.</p>
<p>PO 3.4</p> <p>Waste treatment and management facilities are designed to minimise adverse impacts on both the site and surrounding areas from weed and vermin infestation.</p>	<p>DTS/DPF 3.4</p> <p>None are applicable.</p>
Access	
<p>PO 4.1</p> <p>Traffic circulation movements within any waste treatment or management site are designed to enable vehicles to enter and exit the site in a forward direction.</p>	<p>DTS/DPF 4.1</p> <p>None are applicable.</p>
<p>PO 4.2</p> <p>Suitable access for emergency vehicles is provided to and within waste treatment or management sites.</p>	<p>DTS/DPF 4.2</p> <p>None are applicable.</p>
Fencing and Security	
<p>PO 5.1</p> <p>Security fencing provided around waste treatment and management facilities prevents unauthorised access to operations and potential hazard to the public.</p>	<p>DTS/DPF 5.1</p> <p>Chain wire mesh or pre-coated painted metal fencing 2m or more in height is erected along the perimeter of the waste treatment or waste management facility site.</p>
Landfill	
<p>PO 6.1</p> <p>Landfill gas emissions are managed in an environmentally acceptable manner.</p>	<p>DTS/DPF 6.1</p> <p>None are applicable.</p>

<p>PO 6.2</p> <p>Landfill facilities are separated from areas of environmental significance and land used for public recreation and enjoyment.</p>	<p>DTS/DPF 6.2</p> <p>Landfill facilities are set back 250m or more from a public open space reserve, forest reserve, national park or Conservation Zone.</p>
<p>PO 6.3</p> <p>Landfill facilities are located on land that is not subject to land slip.</p>	<p>DTS/DPF 6.3</p> <p>None are applicable.</p>
<p>PO 6.4</p> <p>Landfill facilities are separated from areas subject to flooding.</p>	<p>DTS/DPF 6.4</p> <p>Landfill facilities are set back 500m or more from land inundated in a 1% AEP flood event.</p>
<p>Organic Waste Processing Facilities</p>	
<p>PO 7.1</p> <p>Organic waste processing facilities are separated from the coast to avoid potential environment harm.</p>	<p>DTS/DPF 7.1</p> <p>Organic waste processing facilities are set back 500m or more from the coastal high water mark.</p>
<p>PO 7.2</p> <p>Organic waste processing facilities are located on land where the engineered liner and underlying seasonal water table cannot intersect.</p>	<p>DTS/DPF 7.2</p> <p>None are applicable.</p>
<p>PO 7.3</p> <p>Organic waste processing facilities are sited away from areas of environmental significance and land used for public recreation and enjoyment.</p>	<p>DTS/DPF 7.3</p> <p>Organic waste processing facilities are set back 250m or more from a public open space reserve, forest reserve, national park or a Conservation Zone.</p>
<p>PO 7.4</p> <p>Organic waste processing facilities are located on land that is not subject to land slip.</p>	<p>DTS/DPF 7.4</p> <p>None are applicable.</p>
<p>PO 7.5</p> <p>Organic waste processing facilities separated from areas subject to flooding.</p>	<p>DTS/DPF 7.5</p> <p>Organic waste processing facilities are set back 500m or more from land inundated in a 1% AEP flood event.</p>
<p>Major Wastewater Treatment Facilities</p>	
<p>PO 8.1</p> <p>Major wastewater treatment and disposal systems, including lagoons, are designed to minimise potential adverse odour impacts on sensitive receivers, minimise public and environmental health risks and protect water quality.</p>	<p>DTS/DPF 8.1</p> <p>None are applicable.</p>
<p>PO 8.2</p> <p>Artificial wetland systems for the storage of treated wastewater are designed and sited to minimise potential public health risks arising from the breeding of mosquitoes.</p>	<p>DTS/DPF 8.2</p> <p>None are applicable.</p>

Workers' accommodation and Settlements

Assessment Provisions (AP)

Desired Outcome (DO)

Desired Outcome	
DO 1	Appropriately designed and located accommodation for seasonal and short-term workers in rural areas that minimises environmental and social impacts.

Performance Outcomes (PO) and Deemed-to-Satisfy (DTS) Criteria / Designated Performance Feature (DPF)

Performance Outcome	Deemed-to-Satisfy Criteria / Designated Performance Feature
<p>PO 1.1</p> <p>Workers' accommodation and settlements are obscured from scenic routes, tourist destinations and areas of conservation significance or otherwise designed to complement the surrounding landscape.</p>	<p>DTS/DPF 1.1</p> <p>None are applicable.</p>
<p>PO 1.2</p> <p>Workers' accommodation and settlements are sited and designed to minimise nuisance impacts on the amenity of adjacent users of land.</p>	<p>DTS/DPF 1.2</p> <p>None are applicable.</p>
<p>PO 1.3</p> <p>Workers' accommodation and settlements are built with materials and colours that blend with the landscape.</p>	<p>DTS/DPF 1.3</p> <p>None are applicable.</p>
<p>PO 1.4</p> <p>Workers' accommodation and settlements are supplied with service infrastructure such as power, water and effluent disposal sufficient to satisfy the living requirements of workers.</p>	<p>DTS/DPF 1.4</p> <p>None are applicable.</p>

Admin - No criteria applies to this land use

No criteria applies to this land use. Please check the definition of the land use for further detail.

09.08.23

Change of Use Application

Proposed Use:

ERN MALLEY will be a 1940's style literary bistro that will serve simple meals, coffee, local beer and wine and host small events to promote literature and the fine arts in Adelaide. We will encourage book clubs, art groups, students, and anyone interested in the history and practice of literature and art to join like minded folk for a hearty conversation over good food and wine. We also hope that Ern Malley will grow to be a popular spot for the local residents (of which I myself am), both in a casual day to day way but also for their own functions, classes and or group get-togethers. We plan to be open for coffee and cake in the morning, from 7am-3pm and then again for dinner and wine from 6pm onwards.

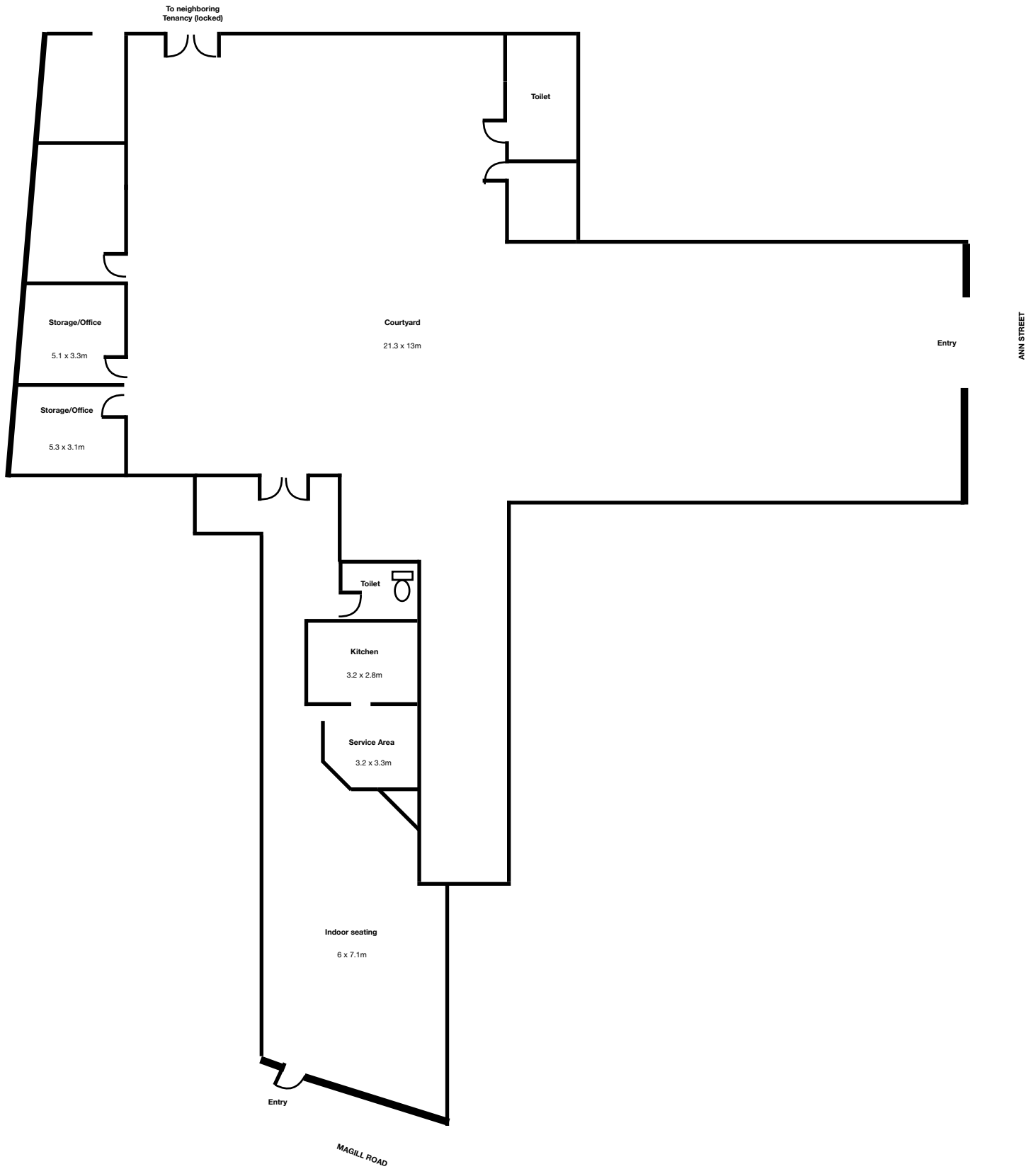
We have access to an additional bathroom in the courtyard and are requesting an increase in the venue capacity to 96.

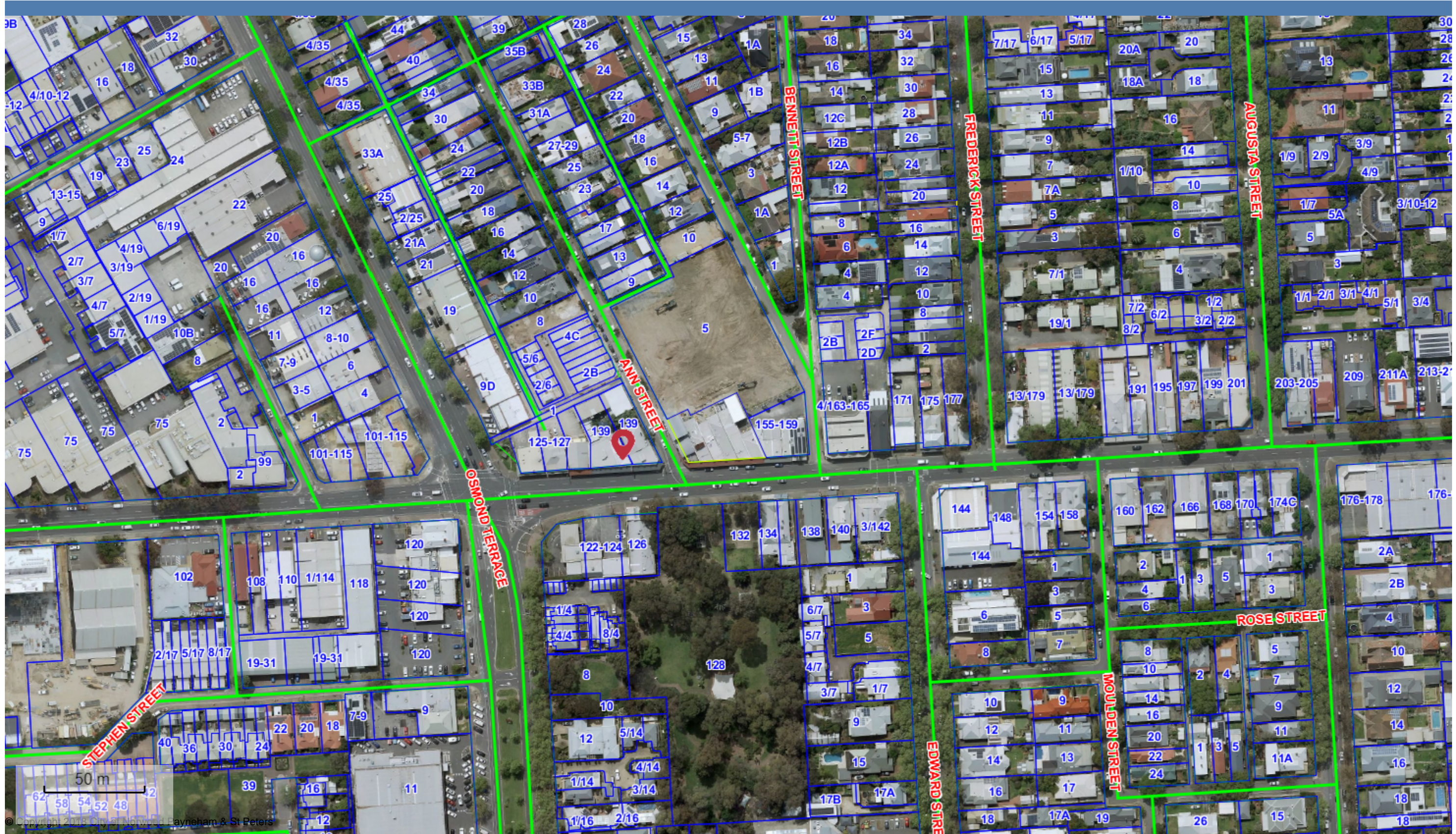
Please reach out for any further clarification on Ern Malley and our plans.

Cheers,

Paul Gallasch
Owner - Ern Malley

137 Magill Rd (Tenancy 2, 133-139 Magill Rd), Stepney 5069





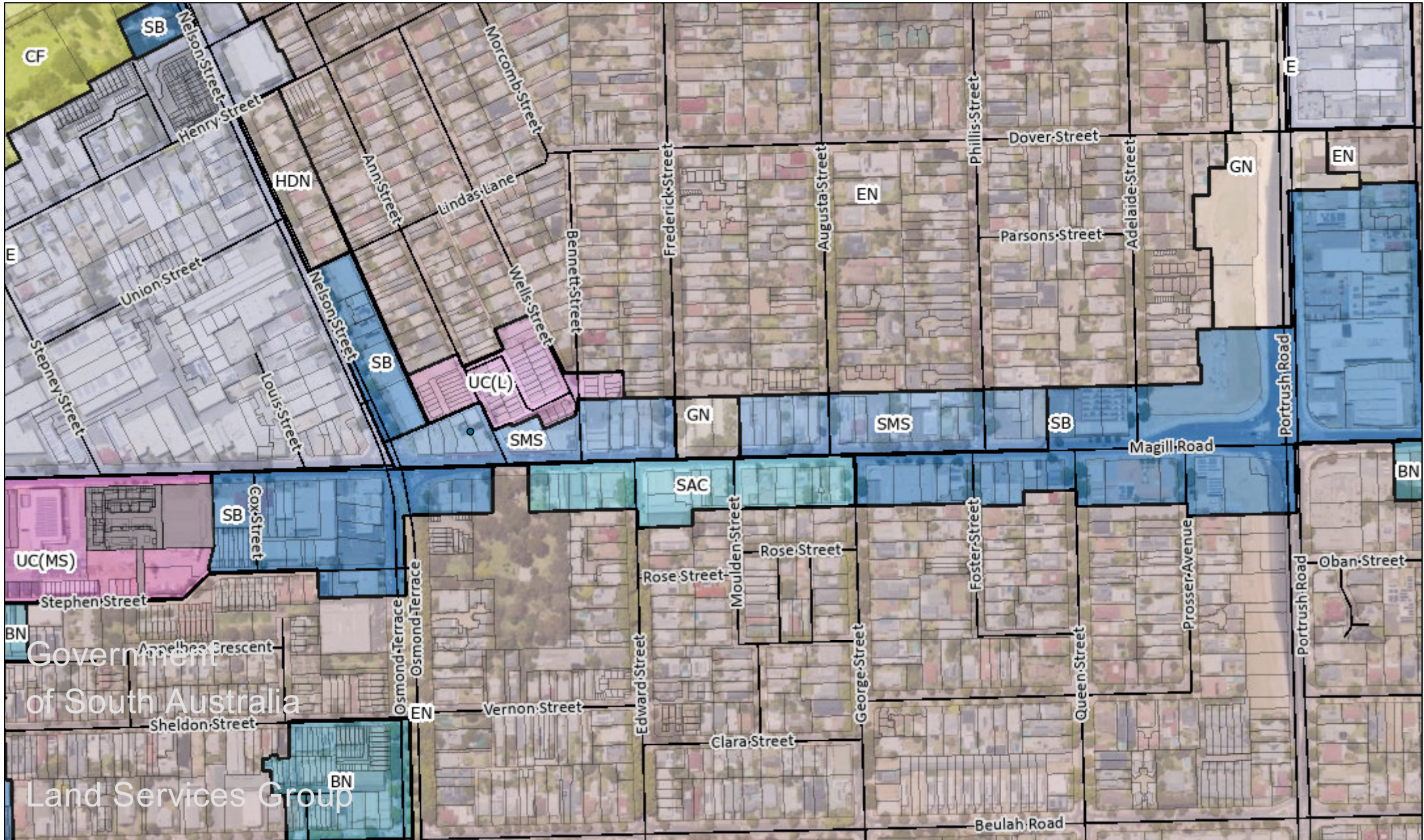
Contact Details

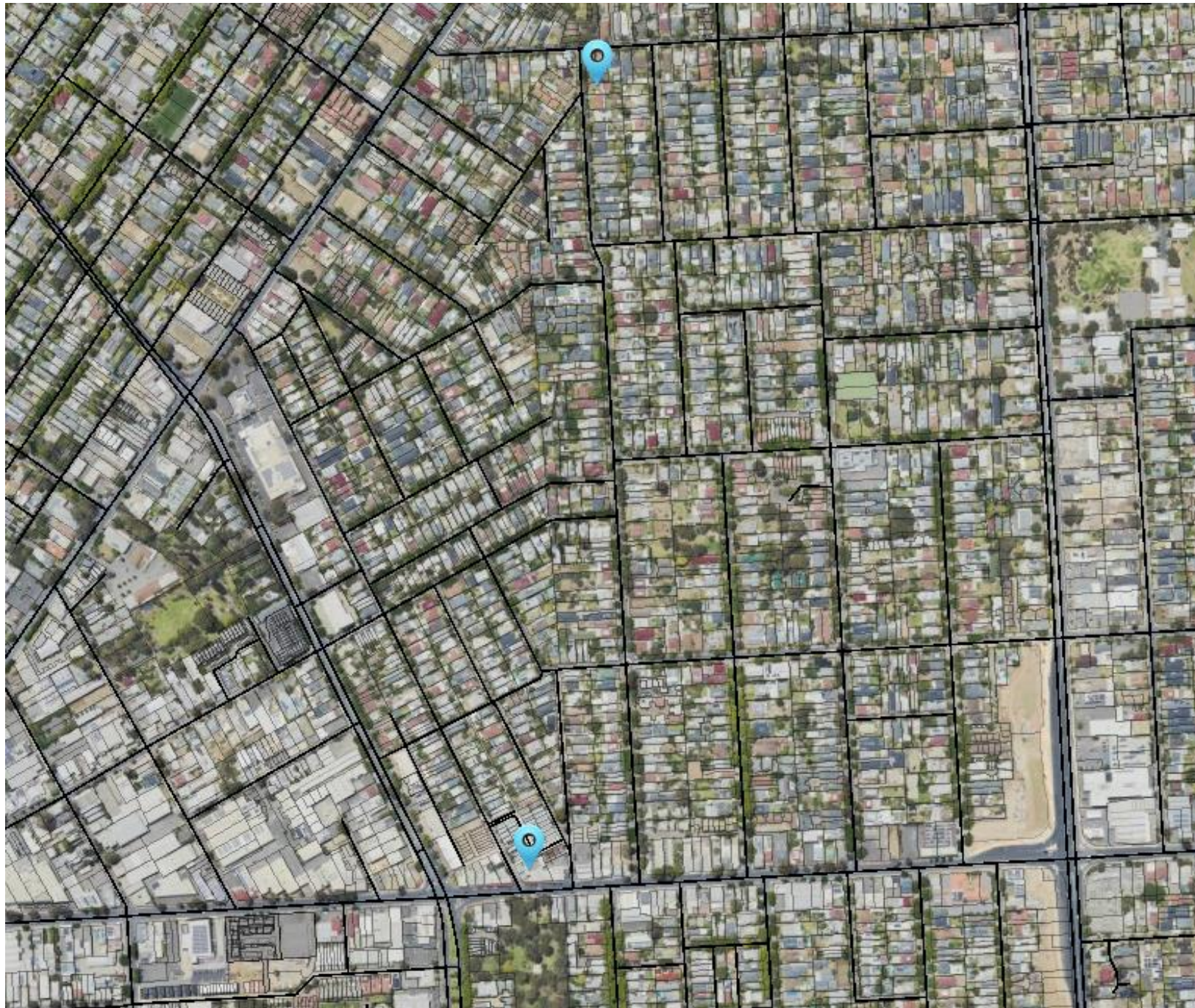
175 The Parade, Norwood
South Australia 5067
P: 08 8366 4555 F: 08 8332 6338
E: townhall@nps.sa.gov.au

Disclaimer

This map is a representation of the information current held by The City of Norwood, Payneham & St Peters. While every effort has been made to ensure the accuracy of the product, Council accepts no responsibility for any errors or omissions. Any feedback on omissions or errors would be appreciated. Data Acknowledgement: Property, Road & Administrator Boundaries - Supplied by Department Environment & Heritage (DEH)

Zoning Map





Application Summary

Application ID	23023331
Proposal	Change of use from restaurant to bistro/bar
Location	133-139 MAGILL RD STEPNEY SA 5069

Representations

Representor 1 - Stuart Williams

Name	Stuart Williams
Address	131 Fredrick street EVANDALE SA, 5069 Australia
Submission Date	10/09/2023 03:18 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

I do believe there is too many coffee shops and bakery's in the area to support a new day time venue parking is already a struggle for the poor cafes there now and home ware shops. If it was to be a venue after the hours of 4pm when home ware shops are closed and bakery's and cafes I feel it would be okay as Magill is quiet after those hours but I feel generally loyal to my current coffee shop & bakery to say it shouldn't support a 7am-11pm venue and it should be a night time bar bistro which is what I understand the venue is going to be. I would totally support this place if it was an afternoon night time venue but if it's before that or after that I believe It's really flooding the neighbour hood.

Attached Documents

Representor 2 - Nabeel Najjar

Name	Nabeel Najjar
Address	12 Treloar Avenue KENSINGTON PARK SA, 5068 Australia
Submission Date	13/09/2023 03:09 PM
Submission Source	Online
Late Submission	No
Would you like to talk to your representation at the decision-making hearing for this development?	No
My position is	I oppose the development

Reasons

I represent the property holdings 141 to 161 Magill Road, Stepney. as well as the 3 storey building 3 Ann Street, Stepney. The proposed development appears to duplicate that of The Republic Norwood by providing Bar/Bistro dining with on tap beer. The concerns are regarding carparking for such a large venture. Its questionable limits will be set at 50 persons given the size of the venture. The Republic provides some off street parking for their business. It would appear the whole of the internal building as well as the external area is to be licensed. This proposed area could accommodate well in excess of 50 persons, lets be realistic. The development of the apartments along Ann street has already removed valuable parking and further congestion is anticipated once residence move in. They are only one carpark apartments with further residence wrestling over left over space for a second vehicle. Where as we have maximized onsite parking and purchased some lands off of the Ottos Developers to create further off street parking it appears other land holders are more interested in leasing outside space instead of creating carparking spaces on their land. We would have been more inclined to support the development if there was the creation of carparks on the area they deemed to be 'courtyard'. This is external land and valuable parking. As owners of the property 141-161 Magill Road it would be absurd for us to remove the rear lands as parking and and simply imply its a courtyard, and do the same. I am happy to discuss further, but cannot see any merit in the blatant lack of inclusion of off street parking when the site has the obvious ability to accommodate. I understand it is also access for adjoining tenancies. If this is to go ahead in its present form it will set a precedent for others to do the same. We will welcome the opportunity to do the same with our sites, remove parking and lease outside area to create a 'courtyard' for further patrons. Again happy to discuss further if required.

Attached Documents

Representation #1:

I represent the property holdings 141 to 161 Magill Road, Stepney. as well as the 3 storey building 3 Ann Street, Stepney.

The proposed development appears to duplicate that of The Republic Norwood by providing Bar/ Bistro dining with on tap beer. The concerns are regarding carparking for such a large venture. Its questionable limits will be set at 50 persons given the size of the venture. The Republic provides some off street parking for their business.

It would appear the whole of the internal building as well as the external area is to be licensed. This proposed area could accommodate well in excess of 50 persons, lets be realistic. The development of the apartments along Ann street has already removed valuable parking and further congestion is anticipated once residence move in. They are only one carpark apartments with further residence wrestling over left over space for a second vehicle.

Where as we have maximized onsite parking and purchased some lands off of the Ottos Developers to create further off street parking it appears other land holders are more interested in leasing outside space instead of creating carparking spaces on their land. We would have been more inclined to support the development if there was the creation of carparks on the area they deemed to be "courtyard";. This is external land and valuable parking. As owners of the property 141-161 Magill Road it would be absurd for us to remove the rear lands as parking and simply imply its a courtyard, and do the same. I am happy to discuss further, but cannot see any merit in the blatant lack of inclusion of off street parking when the site has the obvious ability to accommodate. I understand it is also access for adjoining tenancies.

If this is to go ahead in its present form it will set a precedent for others to do the same. We will welcome the opportunity to do the same with our sites, remove parking and lease outside area to create a "courtyard" for further patrons. Again happy to discuss further if required.

Response:

In response to the first concern regarding Ern Malley being a similar venture to The Republic, the planning code is not in the business of regulating commercial competition and if it were, Ern Malley would most definitely not be providing a similar offering to that of The Republic and would not expect much, if any, crossover in patronage.

In response to the second and main point of the submission, that of capacity and in turn a concern about car parking, firstly, and most importantly, the planning code says that Ern Malley does not need to provide any additional car parking in this case. A fact of which the operators are very grateful for as the courtyard is a very attractive and peaceful place to spend an afternoon and would be much less so if it were in fact a carpark. Also, Ern Malley intends to become a neighborhood establishment and will encourage patrons to walk and ride bicycles to the venue, thus lessening the burden on car parking relative to when this venue was operating recently as a Spanish restaurant. It seems that the respondent, in the end, would like to see less car parking in area, and turn his own back area into a courtyard. I support his mission to see the code changed to allow him and other business owners to do this.

We strongly believe that this representation does not raise any issues which are sufficient to refuse Planning Consent.

Representation #2: Stuart Williams

I do believe there is too many coffee shops and bakery's in the area to support a new day time venue parking is already a struggle for the poor cafes there now and home ware shops. If it was to be a venue after the hours of 4pm when home ware shops are closed and bakery's and cafes I feel it would be okay as Magill is quiet after those hours but I feel generally loyal to my current coffee shop/bakery to say it shouldn't support a 7am-11pm venue and it should be a night time bar bistro which is what I understand the venue is going to be. I would totally support this place if it was an afternoon night time venue but if it's before that or after that I believe It's really flooding the neighbour hood

Response:

As mentioned in our response to representation #1, the planning code is not in the business of determining how many of a certain type of shop can be supported in a particular area. And thus we believe that this representation does not raise any issues which are sufficient to refuse Planning Consent. It also just so happens that we will be focussed on trading in the afternoons and evenings, which Stuart Williams supports.

Request for removal/alteration of conditions.

The council has added conditions to our DA such that no live music is allowed without the prior approval of the Assessment Manager in each and every case. We humbly request that the panel consider removing or altering that condition to allow us some leeway to provide live music at our establishment. We would ideally like the freedom to host small jazz groups, comedians (some of whom play funny songs as part of their set) and un-amplified piano. We have discussed the possibility with a prominent local jazz musician, Django Rowe, and he has advised that in our venue, considering its size, jazz groups would play acoustic with the possible amplification only of the singer. The point being that we have no intention of booking any live music that would disrupt our neighbors, in fact all of the nearby residents that we have talked to thoroughly welcome the idea of having occasional live jazz down the road.

We wonder if the panel would consider, if not removing the condition, at least altering it.

We promise that a solution may be to allow live music only:

Indoors: Sun-Thurs until 9pm, Fri-Sat until 12am

Outdoors: Sun-Thurs until 8pm, Fri-Sat until 10pm.

Or

Live music at a level consistent with the allowance provided for amplified music.

Attachment 6

As residents of the immediate neighborhood ourselves (and after talking with many other residents), we strongly believe that the City of Norwood, Payneham and St Peters would be greatly improved by the addition of our establishment with occasional live music. We would be very happy to accept any conditions that the panel deems necessary to allow that to happen.

6. **DEVELOPMENT APPLICATIONS – DEVELOPMENT ACT**
7. **REVIEW OF ASSESSMENT MANAGER DECISIONS**
8. **ERD COURT APPEALS**
9. **OTHER BUSINESS**
(Of an urgent nature only)
10. **CONFIDENTIAL REPORTS**
11. **CLOSURE**